



HILLINGDON  
LONDON



# Borough Planning Committee

## To Councillors on the Committee

Councillor Henry Higgins (Chairman)  
Councillor Steve Tuckwell (Vice-Chairman)  
Councillor Farhad Choubedar  
Councillor Ekta Gohil  
Councillor Gursharan Mand  
Councillor Raju Sansarpuri  
Councillor Jagjit Singh

**Date:** THURSDAY 8 DECEMBER  
2022

**Time:** 7.00 PM

**Venue:** COMMITTEE ROOM 5,  
CIVIC CENTRE, HIGH  
STREET, UXBRIDGE

**Meeting  
Details:** Members of the Public and  
Media are welcome to attend.  
This meeting may also be  
broadcast live.

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Putting our residents first

Lloyd White  
Head of Democratic Services  
London Borough of Hillingdon,  
Phase II, Civic Centre, High Street, Uxbridge, UB8 1UW

# Useful information for residents and visitors

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It is recommended to give advance notice of filming to ensure any particular requirements can be met. The Council will provide seating areas for residents/public, high speed WiFi access to all attending and an area for the media to report. The officer shown on the front of this agenda should be contacted for further information and will be available to assist.

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# A useful guide for those attending Planning Committees

## Petitions, Speaking and Councillors

**Petitions** – Those who have organised a petition of 20 or more people who live in the Borough, can speak at a Planning Committee in support of or against an application. Petitions must be submitted in writing to the Council in advance of the meeting. Where there is a petition opposing a planning application there is also the right for the applicant or their agent to address the meeting for up to 5 minutes. The Chairman may vary speaking rights if there are multiple petitions

**Ward Councillors** – There is a right for local councillors to speak at Planning Committees about applications in their Ward.

**Committee Members** – The planning committee is made up of the experienced Councillors who meet in public every three weeks to make decisions on applications.

## How the meeting works

The Planning Committees consider the more complex or controversial proposals for development and also enforcement action.

Applications for smaller developments such as householder extensions are generally dealt with by the Council's planning officers under delegated powers.

An agenda is prepared for each meeting, which comprises reports on each application

Reports with petitions will normally be taken at the beginning of the meeting.

The procedure will be as follows:-

1. The Chairman will announce the report;
2. The Planning Officer will introduce it; with a presentation of plans and photographs;
3. If there is a petition(s), the petition organiser will speak, followed by the agent/applicant followed by any Ward Councillors;
4. The Committee may ask questions of the petition organiser or of the agent/applicant;
5. The Committee discuss the item and may seek clarification from officers;
6. The Committee will vote on the recommendation in the report, or on an alternative recommendation put forward by a Member of the Committee, which has been seconded.

## How the Committee makes decisions

The Committee must make its decisions by having regard to legislation, policies laid down by National Government, by the Greater London Authority – under 'The London Plan' and Hillingdon's own planning policies. The Committee must also make its decision based on material planning considerations and case law and material presented to it at the meeting in the officer's report and any representations received.

Guidance on how Members of the Committee must conduct themselves when dealing with planning matters and when making their decisions is contained in the 'Planning Code of Conduct', which is part of the Council's Constitution.

When making their decision, the Committee cannot take into account issues which are not planning considerations such as the effect of a development upon the value of surrounding properties, nor the loss of a view (which in itself is not sufficient ground for refusal of permission), nor a subjective opinion relating to the design of the property. When making a decision to refuse an application, the Committee will be asked to provide detailed reasons for refusal based on material planning considerations.

If a decision is made to refuse an application, the applicant has the right of appeal against the decision. A Planning Inspector appointed by the Government will then consider the appeal. There is no third party right of appeal, although a third party can apply to the High Court for Judicial Review, which must be done within 3 months of the date of the decision.

# Agenda

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## Chairman's Announcements

- 1 Apologies for Absence
- 2 Declarations of Interest in matters coming before this meeting
- 3 Minutes of the previous meeting 1 - 6
- 4 Matters that have been notified in advance or urgent
- 5 To confirm that the items of business marked Part I will be considered in Public and the Items marked Part II will be considered in Private

## PART I - Members, Public and the Press

### Applications with a Petition

	Address	Ward	Description & Recommendation	Page
6	42 Pield Heath Road 17611/APP/2022/993	Colham & Cowley	Demolition of a two-storey bed and breakfast, and the erection of a three-storey residential development comprising of seven flats, alterations to existing drop kerb, all associated external works and landscaping.  <b>Recommendations: Refusal</b>	7 – 32 176 – 188
7	10 St Luke Close 36391/APP/2022/2104	Colham & Cowley	Erection of a two-storey side/rear extension and a single storey rear extension. Roof light located on rear roof slope with the demolition of outbuildings.  <b>Recommendations: Approval</b>	33 – 50 189 – 199
8	R/O 25-31 Warren Road 77265/APP/2022/2845	Ickenham & South Harefield	Erection of 4 no. two storey, detached houses, with habitable accommodation in roof space, garage and associated landscaping, parking and installation of vehicular crossover.  <b>Recommendations: Approval</b>	51 – 86 200 – 215

9	Land at Longford Close  77102/APP/2022/647	Yeading	Erection of a two storey dwelling comprising two 1-bedroom flats  <b>Recommendations: Approval</b>	87 – 112  216 – 221
10	14 High Street  47647/APP/2022/2552	Harefield Village	Application for planning permission for the permanent retention of the existing outdoor seating area at the rear of the coffee shop and replacing existing fence and floor, previously approved under planning application reference number 47647/APP/2020/2017  <b>Recommendations: Approval</b>	113 – 128  222 – 228

### Applications without a Petition

	Address	Ward	Description & Recommendation	Page
11	Axis House, 242 Bath Road  43794/APP/2022/2500	Harefield Village	Change of use of parking spaces from residential use to a paying public car park.  <b>Recommendations: Refusal</b>	129 – 148  229 – 233
12	6 Glebe Avenue  2015/APP/2022/1894	Ickenham & South Harefield	Subdivision of premises into two to provide one barber shop (Class E) and one beauty salon (Sui Generis), and alterations to shopfronts including the installation of a canopy (retrospective application)  <b>Recommendations: Approval</b>	149 – 160  234 – 237
13	57 Tudor Way  20951/APP/2022/1228	Hillingdon East	Erection of a single storey side extension and a part single, part double storey rear extension.  <b>Recommendations: Approval</b>	161 – 174  238 – 248

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# Agenda Item 3

## Minutes

### BOROUGH PLANNING COMMITTEE

1 November 2022

Meeting held at Committee Room 5 - Civic Centre



HILLINGDON  
LONDON

	<p><b>Committee Members Present:</b> Councillors Henry Higgins (Chairman) Steve Tuckwell (Vice-Chairman) Farhad Choubedar Ekta Gohil Gursharan Mand Jagjit Singh</p> <p><b>LBH Officers Present:</b> Katie Crosbie, Planning Team Leader Salleh Jobbi, Legal Advisor Roz Johnson, Planning Services Manager Liz Penny, Democratic Services Officer Fiona Rae, Planning Team Leader Haydon Richardson, Principal Planning Officer Alan Tilly, Transport Planning and Development Team Manager</p>
52.	<p><b>APOLOGIES FOR ABSENCE</b> (<i>Agenda Item 1</i>)</p> <p>Apologies for absence were received from Councillor Raju Sansarpuri.</p>
53.	<p><b>DECLARATIONS OF INTEREST IN MATTERS COMING BEFORE THIS MEETING</b> (<i>Agenda Item 2</i>)</p> <p>There were no declarations of interest.</p>
54.	<p><b>TO SIGN AND RECEIVE THE MINUTES OF THE PREVIOUS MEETING</b> (<i>Agenda Item 3</i>)</p> <p><b>RESOLVED:</b> That the minutes of the meeting dated 4 October 2022 were agreed as an accurate record.</p>
55.	<p><b>MATTERS THAT HAVE BEEN NOTIFIED IN ADVANCE OR URGENT</b> (<i>Agenda Item 4</i>)</p> <p>None.</p>
56.	<p><b>TO CONFIRM THAT THE ITEMS OF BUSINESS MARKED PART I WILL BE CONSIDERED IN PUBLIC AND THE ITEMS MARKED PART II WILL BE CONSIDERED IN PRIVATE</b> (<i>Agenda Item 5</i>)</p> <p>It was confirmed that all items of business were in Part I and would be considered in public.</p>
57.	<p><b>PAVEMENT OUTSIDE 8 CLIVE PARADE, GREEN LANE - 76740/APP/2022/1701</b> (<i>Agenda Item 6</i>)</p>



**Installation of a multi-functional Communication Hub including advertisement display (Application for Planning Permission)**

Officers introduced the application and outlined the information in the addendum. It was noted that items 6 and 10 related to the same site and would be considered together. Members heard that the proposed Hub would be located within the Green Lane Northwood Town Centre and the Northwood Town Centre, Green Lane Conservation Area. The site was also within the setting of the Grade II listed Northwood War Memorial. The location was used for community events including the annual Remembrance Day wreath laying. The application was recommended for refusal as the proposal was considered harmful to the street scene and the Conservation Area and it was felt it would compromise use of the public realm.

A petitioner, representing Northwood Residents' Association, addressed the Committee in objection to the application. Key points highlighted included:

- The proposed Hub would have a damaging impact in a high-profile position within Northwood Conservation Area and was considered disrespectful given its proximity to the War Memorial;
- The Hub would impede flow on a narrow pavement;
- The advertisement display was designed to be eye-catching which would be dangerous on such a busy corner.

Members supported the concept of Communication Hubs and acknowledged that they were beneficial. However, it was felt that this was the wrong location for such a proposal. The Committee enquired whether the highways impact could be included as an additional reason for refusal. It was confirmed that this was not possible since the proposal complied with all guidance. If the application had been recommended for approval, conditions would have been added in relation to the use of the sign to ensure it was not distracting to drivers.

Given the benefits of Community Hubs, Members enquired whether alternative sites were being earmarked for these. The Committee was informed that a number of other Hubs had been agreed under officers' delegated powers.

Members agreed that the benefits of the proposed Hub did not outweigh the disadvantages in this instance. The officer's recommendation was moved, seconded and, when put to a vote, unanimously agreed.

**RESOLVED: That the application be refused.**

58. **139 BELMONT ROAD, UXBRIDGE - 17175/APP/2022/1989** (*Agenda Item 7*)

**Redevelopment of existing two storey dwelling and the erection of 3 no. dwellings with associated private amenity space, car parking, bike and refuse storage.**

Officers introduced the application. It was noted that the area was characterised by detached two-storey properties. No flats or terraced properties were currently located in the vicinity of the application site. Officers felt the proposal would fail to integrate with the established character and appearance of the area due to its size and scale and would be unduly prominent. It was recommended that the application be refused.

A petitioner addressed the Committee in objection to the application. Key points highlighted included:

- The proposed development would be excessively large dwarfing existing houses;
- There were no other terraced houses in the road at present;
- Such a development could set a precedent for the future;
- The proposal would increase pressure on local GP surgeries, schools and other amenities;
- An application proposing 7 flats at the site had been rejected 14 years previously;
- One parking space per house would be insufficient as parking was already at a premium in the area. The development would increase pressure on parking, particularly given the proximity of a roundabout and bus stop;
- Concerns had been raised by neighbours in relation to noise, lack of privacy and loss of light.

The agent for the application was in attendance and addressed the Committee informing Members that the site was extremely large (1000 square metres - 22 metres wide and 44 metres deep). It was centrally located with excellent transport links. The existing building varied from the norm as it was the only bungalow on the street - the proposal was to replace it with an alternative unusual structure and any other future development on the site was likely to take a similar form. Three large, good quality, comfortable, sustainable family homes were proposed (333 square metres each). It was confirmed that the design closely resembled other buildings along the street in terms of its roof pitch, materials, window proportions, eaves etc. The original form of the current building would be retained with the addition of one extra floor. It was claimed that concerns regarding overlooking were unfounded.

In response to questions from the Committee, the agent confirmed that there were existing flats on Belmont Road and other individual properties which varied from the norm. The scheme respected the existing in terms of its proposed form and appearance. It would be a sustainable development on a town centre site.

Committee Members observed that the proposal would fail to integrate with the area and would be harmful to the visual amenity of the area. Concerns were raised in relation to parking although it was recognised that the number of spaces proposed was compliant with the requirements of the London Plan.

Councillors acknowledged the need for additional family homes in the Borough but supported the officer's recommendation and agreed that the development was excessive for the site.

The officer's recommendation was moved, seconded and, when put to a vote, unanimously agreed.

**RESOLVED: That the application be refused.**

59. **12 CHERRY ORCHARD, WEST DRAYTON - 28519/APP/2022/1668** (*Agenda Item 8*)

**This application was withdrawn by the applicant prior to the meeting.**

**Erection of a two-storey side/rear extension and a single storey rear extension. Roof light located on rear roof slope with the demolition of outbuildings.**

Officers introduced the application noting that the site plan / ground floor plan included in the agenda pack was a previous version which had referenced the wrong house number for an outbuilding – an updated plan had been included in the officer's presentation; this had had no impact on the officer's recommendation. The proposal was for a large extension and was recommended for approval. A previous application for planning permission to build a 3-bedroom detached house at the site had been refused in May 2022.

A petitioner addressed the Committee in objection to the application. Key points highlighted included:

- Plans for a 3-bed house at the site had previously been rejected;
- Concerns were raised regarding potential use as an HMO given that the development would incorporate 5 bedrooms and 4 bathrooms;
- A large extension at the other end of the terrace group (no.15) had been built 39 years ago hence should not be considered relevant to this application. Moreover, the extension at no.15 had been intended for family use and had only one bathroom and one toilet;
- Residents were concerned the extension could be converted into a 7-bedroom HMO at a future stage;
- Parking stress would be exacerbated if the planned development were to go ahead;
- House numbers 10 and 12 shared a water supply. Water pressure was already at the legal limit hence the impact of the development was a matter of concern;
- The view from residents' gardens would be compromised if the application were agreed;
- The new property would look like a 'bolt on' and would not be in keeping with the current building line.

A written statement on behalf of the applicant was read out to the Committee. Members heard that the planning application had been made in good faith to increase the size of a family home. The existing house was situated on a large plot of land providing a substantial total of 366 square metres of garden and parking amenities. The development would only utilise a small part of this otherwise unused space (50 square metres). The extension had been carefully planned in line with planning policy and was respectful of the neighbour's amenities. The works would bring the house in line with current building regulations and make it more sustainable.

Ward Councillor Roy Chamdal addressed the Committee in objection to the application. He noted that the application conflicted with current policy as it exceeded half the size of the original property. Councillor Chamdal felt it was not a good development and was not required. He commented that the reference to a previous application on the site was not relevant to this case. Moreover, planning permission for the large extension at the other end of the terrace had been granted many years previously and did not make the current application acceptable.

Members expressed concern that the proposal did not align with current policy and requested further clarification regarding the size of the development and the potential for HMO use. It was confirmed that the proposed width was 4.3m – to comply with

	<p>policy this would need to be reduced by 1.3m. With regard to the potential use as an HMO, the Committee was advised that the area was subject to an Article 4 restriction hence planning permission would be required for any change of use. At this stage it was important that Members focussed only on the application before them.</p> <p>Members were informed that the existence of a large extension at the other end of the terrace was a material consideration, which would also carry considerable weight if a decision were to be appealed. An inspector would be required to consider whether the addition of something symmetrical would be harmful. It was noted that officers had proposed a number of conditions to protect the amenity of neighbours; if the matter were to go to appeal, it was possible that the inspector would not allow these.</p> <p>In response to further questions from the Committee, it was confirmed that the previous application at the site had proposed a materially different scheme for a new house, which had been refused. Parking provision was now considered acceptable because this scheme was for an extension, not a new dwelling. Concerns regarding water pressure could not be taken into account as this was not a material planning consideration.</p> <p>A site visit was proposed and unanimously agreed. It was confirmed that, following the visit, the application would come back to the Borough Planning Committee for further consideration. Should petitioners wish to speak again, a new petition would be required.</p> <p><b>RESOLVED: That the application be deferred pending a site visit.</b></p>
61.	<p><b>PAVEMENT OUTSIDE 8 CLIVE PARADE, GREEN LANE - 76740/ADV/2022/48</b> <i>(Agenda Item 10)</i></p> <p><b>Installation of 1 no. internally illuminated free standing multi-functional Communication Hub, including advertisement display (Application for Advertisement Consent).</b></p> <p>This application was considered in conjunction with agenda item 6.</p> <p>The officer's recommendation was moved, seconded and, when put to a vote, unanimously agreed.</p> <p><b>RESOLVED: That the application be refused.</b></p>
62.	<p><b>HAYDON HOUSE, EASTCOTE - 51321/APP/2022/1861</b> <i>(Agenda Item 11)</i></p> <p><b>Change of use from Class E (Office) to Class C3 (6 no. self-contained flats – (2 x Studio, 2 x 1 Bed 2 People, 2 x 2 Bed 3 People) (Application for Prior Approval under Schedule 2, Part 3, Class MA of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended))).</b></p> <p>Officers introduced the application It was noted that the application was for Prior Approval. It had been concluded that the proposal would not have any unacceptable transport or noise impacts, contamination risks or flooding risks in relation to the application building. Adequate natural light would be provided in all habitable rooms. The application was recommended for approval.</p> <p>Members welcomed the fact that environmental factors had been taken into account</p>

	<p>and that the proposal would bring a disused building back into use. In response to their requests for clarification, the Committee was informed that a condition had been included in relation to parking allocation. No further concerns were raised.</p> <p>The officer's recommendation was moved, seconded and, when put to a vote, unanimously agreed.</p> <p><b>RESOLVED: That the application be approved.</b></p>
	<p>The meeting, which commenced at 7.00 pm, closed at 8.35 pm.</p>

These are the minutes of the above meeting. For more information on any of the resolutions please contact Liz Penny on . Circulation of these minutes is to Councillors, Officers, the Press and Members of the Public.



**Item No.**            **Report of the Interim Director of Planning, Regeneration & Public Realm**

**Address**            42 PIELD HEATH ROAD HILLINGDON

**Development:**    Demolition of a two-storey bed and breakfast, and the erection of a three-storey residential development comprising of seven flats, alterations to existing drop kerb, all associated external works and landscaping.

**LBH Ref Nos:**     17611/APP/2022/993

**Drawing Nos:**    555/B/09  
555/B/01  
555/B/02  
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555/B/04  
555/B/05  
555/B/06  
555/B/07  
555/B/07  
555/B/08  
555/B/08

**Date Plans Recieved:**    23/03/2022

**Date(s) of Amendment(s):**

**Date Application Valid:**    23/03/2022

## 1. **SUMMARY**

Planning permission is sought to replace an existing two storey building serving a bed and breakfast, with a new building comprising 7no. 2-bed flats. The proposed works also include alterations to the site's existing dropped kerb, associated external works and landscaping.

During the course of the application, concerns were raised regarding the design of the proposal. Revised plans were received in an attempt to overcome the concerns raised. The proposed development is considered to be an improvement in comparison with the scheme originally submitted, however it is still unacceptable as proposed.

There is no objection, in principle, to the creation of additional residential units in this location in land use terms. However, concerns are raised that all of the proposed flats would provide 2no. bedrooms and no family accommodation.

Whilst the principle of residential development may be acceptable, the building is unacceptable as proposed. Due to its scale, bulk, design and backland location, the proposal would fail to integrate with the established character and appearance of the area, subsequently resulting in an incongruous and unduly prominent form of development, which would harm the visual amenities and character of the area. The proposal would also cause harm to the amenities of the occupiers of 1, 2 and 3 Newlyn Close, properties on Pield Heath Road and as far as Micawber Road, by reason of loss of light, outlook and privacy as well as a harmful sense of enclosure.

In addition, there are concerns that in the absence of a satisfactory legal agreement to restrict future residents from applying for a parking permit to join the Council's on street parking management scheme, the proposal would give rise to parking stress. Further, the proposed access alterations would cause harm to the free flow of traffic on the local

highway network and would prejudice pedestrian safety. An objection has been received from the Council's Highways Team in this regard.

For these reasons, the proposal is considered to be contrary to the development plan and is recommended for refusal as set out in section 2 of this Committee Report.

## **2. RECOMMENDATION**

### **REFUSAL for the following reasons:**

#### **1 NON2 Non Standard reason for refusal**

Due to its depth, bulk, scale, massing, design and siting, the proposed building would form an oversized, uncharacteristic and visually incongruous form of development, which would fail to harmonise with the established character and appearance of the street scene and other backland/garden development in the vicinity. The proposal would therefore be detrimental to the visual amenities of the streetscene and character of the surrounding area, all contrary to Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (2012), Policies DMHB 11, and DMHB 12 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), Policies D1 and D3 of the London Plan (2021) and the National Planning Policy Framework (2021).

#### **2 NON2 Non Standard reason for refusal**

Due to its depth, size, siting and design, the proposed building would be overbearing and would lead to a harmful sense of enclosure and loss of outlook from the rear windows and gardens of 1 and 2 Newlyn Close, as well as significant overlooking and a harmful loss of privacy to the occupiers of 1, 2, 3 Newlyn Close, properties on Field Heath Road and 2 Micawber Road. The proposal would therefore be contrary to Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) and paragraph 130 (f) of the NPPF (2021).

#### **3 NON2 Non Standard reason for refusal**

The proposed development fails to provide any three or more bedroom (family sized) units. Robust justification has not been provided to demonstrate that the provision of family sized units would be unsuitable or unviable. The proposal therefore fails to optimise the site and provide a suitable mix of housing to support sustainable, inclusive and mixed communities, contrary to Policy DMH 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policy H10 of the London Plan (2021) and the National Planning Policy Framework (2021).

#### **4 NON2 Non Standard reason for refusal**

No legal agreement is in place to prohibit future residents of the proposed development from applying to join the Council's on street parking management scheme. In the absence of such an agreement, the Council's Parking Management Scheme is likely to be overutilised, leading to roadside parking, congestion and reduced highway safety. The proposal would therefore be contrary to Policy DMT 2 and DMT 6 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) and Policy T4 and T6 of the London Plan (2021).

#### **5 NON2 Non Standard reason for refusal**

The proposed vehicular access would create a hazardous and dangerous environment for pedestrians crossing the site and drivers leaving the site. The proposal would therefore be contrary to Policy DMT 2 and DMT 6 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), Policy T4 and T6 of the London Plan (2021) and The London Borough of Hillingdon Domestic Vehicle Footway Crossover Policy 2022.

## **INFORMATIVES**

**1 152 Compulsory Informative (1)**

The decision to REFUSE planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

**2 153 Compulsory Informative (2)**

The decision to REFUSE planning permission has been taken having regard to the policies and proposals in the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including The London Plan (2021) and national guidance.

DMCI 7	Planning Obligations and Community Infrastructure Levy
DMEI 2	Reducing Carbon Emissions
DMEI 4	Development on the Green Belt or Metropolitan Open Land
DMEI 7	Biodiversity Protection and Enhancement
DMEI 10	Water Management, Efficiency and Quality
DMEI 11	Protection of Ground Water Resources
DMEI 14	Air Quality
DMH 2	Housing Mix
DMH 4	Residential Conversions and Redevelopment
DMHB 11	Design of New Development
DMHB 12	Streets and Public Realm
DMHB 14	Trees and Landscaping
DMHB 15	Planning for Safer Places
DMHB 16	Housing Standards
DMHB 17	Residential Density
DMHB 18	Private Outdoor Amenity Space
DMT 1	Managing Transport Impacts
DMT 2	Highways Impacts
DMT 5	Pedestrians and Cyclists
DMT 6	Vehicle Parking
LPP D1	(2021) London's form, character and capacity for growth
LPP D3	(2021) Optimising site capacity through the design-led approach
LPP D4	(2021) Delivering good design
LPP D5	(2021) Inclusive design
LPP D6	(2021) Housing quality and standards
LPP D7	(2021) Accessible housing
LPP G1	(2021) Green infrastructure
LPP G2	(2021) London's Green Belt
LPP G5	(2021) Urban greening
LPP G6	(2021) Biodiversity and access to nature
LPP H1	(2021) Increasing housing supply
LPP H2	(2021) Small sites
LPP SI12	(2021) Flood risk management
LPP SI13	(2021) Sustainable drainage
LPP SI2	(2021) Minimising greenhouse gas emissions
LPP T5	(2021) Cycling
LPP T6	(2021) Car parking
LPP T6.1	(2021) Residential parking

NPPF2	NPPF 2021 - Achieving sustainable development
NPPF4	NPPF 2021 - Decision-Making
NPPF5	NPPF 2021 - Delivering a sufficient supply of homes
NPPF11	NPPF 2021 - Making effective use of land
NPPF12	NPPF 2021 - Achieving well-designed places
NPPF15	NPPF 2021 - Conserving and enhancing the natural environment

### **3 174 Community Infrastructure Levy (CIL) (Refusing Consent)**

This is a reminder that Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), should an application for appeal be allowed, the proposed development would be deemed as 'chargeable development' and therefore liable to pay the London Borough of Hillingdon Community Infrastructure Levy (CIL) and the Mayor of London's Community Infrastructure Levy (CIL). This would be calculated in accordance with the London Borough of Hillingdon CIL Charging Schedule 2014 and the Mayor of London's CIL Charging Schedule 2012.

For more information on CIL matters please visit the planning portal page at: [www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil](http://www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil)

## **3. CONSIDERATIONS**

### **3.1 Site and Locality**

The development site is located on the north side of Pield Heath Road. The site comprises a two storey building which is currently used as bed and breakfast accommodation. The building has been finished in brick and hung tiles. In terms of style it is low level and resembles an extended chalet bungalow. The building is well set back from the road and its frontage comprises off road parking and some soft landscaping.

To the immediate west of the site is 44 Pield Heath Road, which comprises a single storey, flat roofed, warehouse style building with rear service yard which is occupied by "Jet Clean" (a drainage and services maintenance company). To the east of the site is 40 Pield Heath Road a two storey semi detached property. The remaining area surrounding the site is predominantly residential with the exception of a few commercial uses. To the rear of the site are semi detached dwellings which make up Newlyn Close.

It is worth noting that at present the building which occupies 42 Pield Heath Road is uncharacteristically set back from other buildings on the main road. It does not follow the area's prevailing building line.

The site lies within the Hillingdon Air Quality Management Area and a Critical Drainage Area. It has a Public Transport Accessibility Level (PTAL) of 3.

Land to the south west of the application site (on the opposite side of Pield Heath Road) is designated Green Belt. The application site itself is not within the Green Belt.

### **3.2 Proposed Scheme**

Planning permission is sought to replace an existing two storey building serving as a bed and breakfast, with a new building comprising 7 flats. The proposed works also include alterations to the site's existing dropped kerb, associated external works and landscaping.

### **3.3 Relevant Planning History**

17611/A/86/2181            42 Pield Heath Road Hillingdon  
Householder dev. - two storey side extension

**Decision:** 23-01-1987    Approved

17611/APP/2001/726        42 Pield Heath Road Hillingdon

CHANGE OF USE OF FIRST FLOOR AND ONE ROOM ON GROUND FLOOR TO BED AND BREAKFAST HOTEL.

**Decision:** 13-03-2007    NFA

17611/APP/2021/3834      42 Pield Heath Road Hillingdon

Demolition of a two-storey building with rear dormers used as an existing bed and breakfast, and the erection of two-storey with loft floor residential development comprising of nine flats, alteration to the existing drop kerb and all associated external works and landscaping

**Decision:** 02-02-2022    Withdrawn

17611/D/89/1520            42 Pield Heath Road Hillingdon

Erection of a first floor rear extension to provide 4 additional bedrooms, single storey rear extension and conversion of existing garage to games room and utility room

**Decision:** 17-11-1989    Refused

17611/E/90/0861            42 Pield Heath Road Hillingdon

Erection of a side extension for use as a games room

**Decision:** 04-06-1990    Approved

17611/G/92/0560            42 Pield Heath Road Hillingdon

Change of use of part of residential dwellinghouse to bed and breakfast accommodation (retrospective application)

**Decision:** 19-08-1992    Approved

17611/H/98/1976            42 Pield Heath Road Hillingdon

Removal of condition 4 (personal permission) of planning permission ref.17611G/92/560 dated 19/08/92; Change of use of part of residential dwellinghouse to bed and breakfast accommodation

**Decision:** 23-12-1998    Approved

#### **Comment on Relevant Planning History**

The site's planning history is set out in section 3.3 of this report (above).

Planning permission was sought for 'Demolition of a two-storey building with rear dormers used as an existing bed and breakfast, and the erection of two-storey with loft floor residential development comprising of nine flats, alteration to the existing drop kerb and all associated external works and landscaping' in 2021 under planning application No.17611/APP/2021/3834. The application was withdrawn before a decision was made.

#### **4. Planning Policies and Standards**

Planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.



The Development Plan for the London Borough of Hillingdon currently consists of the following documents:

The Local Plan: Part 1 - Strategic Policies (2012)  
The Local Plan Part 2 - Development Management Policies (2020)  
The Local Plan Part 2 - Site Allocations and Designations (2020)  
The West London Waste Plan (2015)  
The London Plan (2021)

The National Planning Policy Framework (NPPF) (2021) is also a material consideration in planning decisions, as well as relevant supplementary planning documents and guidance.

### **Local Plan Designation and London Plan**

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment  
PT1.H1 (2012) Housing Growth

Part 2 Policies:

DMCI 7 Planning Obligations and Community Infrastructure Levy  
DMEI 2 Reducing Carbon Emissions  
DMEI 4 Development on the Green Belt or Metropolitan Open Land  
DMEI 7 Biodiversity Protection and Enhancement  
DMEI 10 Water Management, Efficiency and Quality  
DMEI 11 Protection of Ground Water Resources  
DMEI 14 Air Quality  
DMH 2 Housing Mix  
DMH 4 Residential Conversions and Redevelopment  
DMHB 11 Design of New Development  
DMHB 12 Streets and Public Realm  
DMHB 14 Trees and Landscaping  
DMHB 15 Planning for Safer Places  
DMHB 16 Housing Standards  
DMHB 17 Residential Density  
DMHB 18 Private Outdoor Amenity Space  
DMT 1 Managing Transport Impacts  
DMT 2 Highways Impacts  
DMT 5 Pedestrians and Cyclists  
DMT 6 Vehicle Parking  
LPP D1 (2021) London's form, character and capacity for growth  
LPP D3 (2021) Optimising site capacity through the design-led approach  
LPP D4 (2021) Delivering good design  
LPP D5 (2021) Inclusive design  
LPP D6 (2021) Housing quality and standards

LPP D7	(2021) Accessible housing
LPP G1	(2021) Green infrastructure
LPP G2	(2021) London's Green Belt
LPP G5	(2021) Urban greening
LPP G6	(2021) Biodiversity and access to nature
LPP H1	(2021) Increasing housing supply
LPP H2	(2021) Small sites
LPP SI12	(2021) Flood risk management
LPP SI13	(2021) Sustainable drainage
LPP SI2	(2021) Minimising greenhouse gas emissions
LPP T5	(2021) Cycling
LPP T6	(2021) Car parking
LPP T6.1	(2021) Residential parking
NPPF2	NPPF 2021 - Achieving sustainable development
NPPF4	NPPF 2021 - Decision-Making
NPPF5	NPPF 2021 - Delivering a sufficient supply of homes
NPPF11	NPPF 2021 - Making effective use of land
NPPF12	NPPF 2021 - Achieving well-designed places
NPPF15	NPPF 2021 - Conserving and enhancing the natural environment

## **5. Advertisement and Site Notice**

**5.1** Advertisement Expiry Date:- Not applicable

**5.2** Site Notice Expiry Date:- Not applicable

## **6. Consultations**

### **External Consultees**

5 neighbouring properties were directly notified of the proposal on 11/04/22. Following the receipt of amended plans 27 neighbouring properties were directly notified on 05/10/22. During the consultation periods 27 letters of objection were received against the proposed development (this figure includes an objection from the lead petitioner and 2 Local Councillors).

Objections raised have been summarised below.

- 1) The proposal would represent an overdevelopment of the site and would be out of keeping with the character of the area;
- 2) The proposal would have an adverse impact on the amenities of neighbours due to light loss, privacy loss, noise and intensification of the site's use;
- 3) The proposal would have an adverse impact on the local highways network and pedestrian safety;
- 4) Insufficient vehicle parking is proposed and the proposed access changes are dangerous;
- 5) The proposal would block/interrupt emergency service routes due to parking over spill on the local highway or vehicles turning in the road;
- 6) The proposal would increase greenhouse gas emissions and worsen air quality;
- 7) The development would cause surface water flooding and drainage issues;
- 8) The proposed flat roof could be used as a garden;
- 9) The development would set a precedent for future harmful development in the area;

- 10) During construction, large vehicles would block the road delaying and halting pedestrian and vehicle movements;
- 11) The same concerns and objections raised to the original scheme apply to the amended scheme;
- 12) There is no refuse storage provision;
- 13) The proposal would put a significant burden on service provisions in the area (gas, water, electricity);
- 14) The proposal does not include solar panels or a green roof as mentioned in the Design and Access statement - some misleading and incorrect information has been included; and
- 15) Concerns that all comments received during the process of the application should be taken into consideration when the application is taken to committee.

In addition to the objections, a petition with 139 signatures has been received against the proposed development.

The petition states:

Objections include (but are not limited to):

- Increased traffic, congestion and air pollution;
- Decreased safety for children, pedestrians, road users of Pield Heath Road and side roads;
- Increased noise and intensity of use;
- Inadequate parking and impact of overflow parking on existing residents;
- Loss of privacy;
- Potential blocking or interruption of an ambulance route;
- Overdevelopment - failure to protect residential nature of local area.

The desired outcome is noted as:

Refusal of planning permission.

Planning Officer Response:

Issues relating to points 1-7 are considered in the main body of this report.

In response to point 8, had the application been recommended for approval a condition could have been recommended restricting the use of the building's roof for amenity purposes.

In response to point 9, any and all future applications would be dealt with on their individual merits at the time of submission.

In relation to point 10, it should be noted that construction works are temporary and therefore so are the associated impacts. The Environmental Protection Act (1990) and Pollution Act (1974) are in place to ensure that construction works are carried out in an appropriate and environmentally friendly manner. Notwithstanding the above, if the recommendation had otherwise been to grant planning permission, a condition requiring a construction logistics/ construction management plan to be agreed with the Council would have been recommended, in order to minimise the impacts of construction on neighbouring residents and the environment as far as practicable.

In response to point 11, the amended scheme has been assessed in the main body of the report and is recommended for refusal for the reasons set out in section 2 of this report.

In response to point 12, a refuse storage area is shown near to the entrance of the site on the proposed plans. Had the application been recommended for approval, it is likely that a condition would have been recommended, requiring full details of the bin storage facilities (dimensions and external finishes).

In response to points 7 and 13, Building Regulations are in place to ensure that the proposal has no undue adverse impact on the area's utility infrastructure. In addition, had the proposal been recommended for approval, a condition could have been included to address drainage provision.

In response to point 14, the proposed floor plans show a green roof and solar panels. However the floor plans do not match the proposed roof plan. Had the application been recommended for approval the discrepancy would have been addressed by requesting amended plans. Additionally had the application been recommended for approval, a condition would have been recommended requiring full details of the green roof and solar panels.

In response to point 15, all comments received during the process of the application have been taken into consideration in this recommendation.

### **Internal Consultees**

#### **ACCESS OFFICER:**

This flatted development proposal comprising 7 units has been reviewed against the requirements of London Plan Policy D7 and H2. All new housing must meet the technical specifications set out in Approved Document M to the Building Regulations 2010 (2015 edition). In view of the above H2 policy dispensation for lift access to the units above ground on small sites, no further accessibility concerns are raised subject to the following conditions attached to any approval.

Prior to any works on site above damp proof course level, details of step free access via all points of entry and exit to all ground floor units shall be submitted to, and approved in writing, by the Local Planning Authority. Such provision shall remain in place for the life of the building.

REASON To ensure housing of an inclusive design is achieved and maintained in accordance with Policies D5 and D7 of the London Plan (2021).

The development hereby approved shall ensure that all ground floor units accord with the requirements of Policy D7 of the London Plan, and shall not be occupied until certification of compliance with the technical specifications for an M4(2) dwelling, as set out in Approved Document M to the Building Regulations (2010) 2015, has been submitted to, and approved in writing, by the Local Planning Authority. All such provisions must remain in place for the life of the building.

REASON To not only allow the Building Control body to require the development to comply with the optional Building Regulations standards, but to also ensure the appropriate quantity and standard of accessible and adaptable housing is constructed and maintained in accordance with policy D7 of the London Plan.

#### **HIGHWAYS OFFICER:**

An application has been received seeking planning permission to demolish the existing two-storey bed and breakfast and erect a stepped back three-storey residential development which will comprise of 7 x 2no. bedroom dwellings with alterations to the existing drop kerb, external works, and landscaping.

The development will provide 5no. on-site parking spaces, on-site cycle storage parking and electric vehicle charging points. The proposal site has a PTAL rating of 3 indicating that its access to public transport is moderate when compared to London as a whole suggesting that there will be some reliance on the private car for trip making. Pield Heath Road contains double yellow lines on either side of the road. Residential streets in close proximity to the site contain parking management scheme. The proposal site will retain the existing vehicle crossover.

42 Pield Heath Road will utilise the existing vehicle crossover, with some changes being made to it. However, the development wishes to introduce a 1-metre-high planting to the east and realign the wall to a new 900mm wall.

This would be too high and would block line of sight for vehicles exiting the site, which presents safety concerns for pedestrians from exiting vehicles. This is in breach of the published London Borough of Hillingdon's Domestic Vehicle Footway Crossover policy June 2022 which states that 'fences, walls and shrubs should be kept to a maximum height of 0.6m' and the published London Plan Policy T4 Assessing and mitigating transport impacts which states that 'development proposals should not increase road danger'.

As mentioned above the development will be on-site cycle storage parking for the residents. However, the submitted site plan dated 10/09/2022 makes no mention of the quantity that will be provided, which makes an accurate assessment difficult to do. For a development of this type to be within policy at least 12no. would need to be provided. Therefore, this proposal contradicts the London Plan 2021 Policy T5 Cycle which requires to developments 'provide the provision of appropriate levels of cycle parking which should be fit for purpose, secure and well-located and be in accordance with the minimum standards'.

There are highways objections to this proposal because it would not be in accordance with the published London Plan 2021 Policies T4 Assessing and mitigating transport impacts, T5 Cycle and the published London Borough of Hillingdon's Domestic Vehicle Footway Crossover policy June 2022.

Planning Officer Response:

The above comments are noted. All comments have been taken into consideration and included in the main body of the report where necessary. The conditions recommended by the Access Officer would have been considered in the event of a recommendation to grant permission.

## **7. MAIN PLANNING ISSUES**

### **7.01 The principle of the development**

#### **PRINCIPLE OF DEVELOPMENT:**

The existing bed and breakfast is considered to constitute a form of serviced visitor accommodation.

The London Plan (2021) defines "Visitor Accommodation" as:

'Leisure and business accommodation that provides temporary overnight accommodation on a commercial basis, including serviced accommodation such as hotels, bed and breakfast, guesthouses, hostels and campus accommodation, and non-serviced accommodation such as self-catering apart-hotels, caravans and camping.'

The London Plan (2021) defines "Serviced Accommodation" as:

'In the context of visitor accommodation this includes hotels, bed & breakfasts, guest houses, and hostels where services such as catering and cleaning are provided to guests.'

There are no Local Plan Policies which seek to protect existing visitor accommodation. However, it is noted that the Hillingdon Local Plan Part 1 recognises at paragraph 5.22 that: 'Tourism is therefore a significant contributor to the borough's economy and meeting the needs of visitors, including provision of hotels, conference facilities and cultural activities, is an important consideration for Hillingdon. Visitors include tourists, business visitors, and friends/family of people living in the area, such as university students. Hillingdon benefits from tourism through employment and training opportunities, wealth creation and support for the local economy and culture.'



London Plan Policy E10 states:

'London's visitor economy and associated employment should be strengthened by enhancing and extending its attractions, inclusive access, legibility, visitor experience and management and supporting infrastructure, particularly to parts of outer London well-connected by public transport, taking into account the needs of business as well as leisure visitors'.

A sufficient supply and range of serviced accommodation should therefore be maintained.

Taking into consideration that other forms of visitor accommodation exist within the Borough, in more sustainable locations where there is a wider range of services and amenity provisions for visitors, the loss of this bed and breakfast is considered to be acceptable in principle. It is also important to give weight to the fact that the provision of new housing within the Borough is supported within both the London Plan and Local Plan, so long as the proposed development complies with all other relevant planning policies and material planning considerations.

Overall it is therefore considered that the principle of replacing the existing bed and breakfast with residential accommodation is acceptable, subject to the proposal being in compliance with relevant planning policies and material planning considerations.

#### HOUSING MIX:

Policy H10 of the London Plan (2021) states that schemes should generally consist of a range of unit sizes and sets out a number of factors which should be considered when determining the appropriate housing mix on a particular scheme. This includes local evidence of need.

Policy DMH 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires the provision of a mix of housing units of different sizes in schemes of residential development to reflect the Council's latest information on housing need. Paragraph 4.6 outlines that there is a substantial borough-wide requirement for larger affordable and private market units, particularly three-bedroom properties.

Family housing, as identified in the Council's Strategic Market Housing Assessment is the type of housing most in need in Hillingdon. Family housing is defined within the glossary of the London Plan (2021) which outlines it must generally be of a size that has three or more bedrooms.

The proposed development includes 7 x 2 bedroom properties. There is no mix of housing proposed within the block. This is of concern, particularly as no justification has been provided to demonstrate that the provision of family sized units would be unsuitable or unviable as part of the redevelopment proposal.

The proposal would therefore not provide a suitable mix of housing to support sustainable, inclusive and mixed communities, contrary to Policy DMH 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policy H10 of the London Plan (2021) and the National Planning Policy Framework (2021).

#### **7.02 Density of the proposed development**

Policy D3 of the London Plan (2021) states that all development must make the best use of land by following a design-led approach that optimises the capacity of sites. Higher density developments should generally be promoted in locations that are well connected to jobs, services, infrastructure and amenities by public transport, walking and cycling. In

other areas, incremental densification should be actively encouraged by Boroughs to achieve a change in densities in the most appropriate way. This should be interpreted in the context of Policy H2 of the London Plan (2021) which states that Boroughs should proactively support well-designed new homes on small sites below 0.25 hectares in size.

Policy DMHB 17 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all new residential development should take account of the Residential Density Matrix contained in Table 5.2.

Numerical density levels are considered to be more appropriate to larger sites and are not typically used in the assessment of schemes of less than 10 units. The key consideration is therefore whether the development would acceptably integrate with the character and appearance of the locality and would respect residential amenity considerations, rather than the consideration of the density of the proposal.

Please refer to the other sections of this Committee Report which assess these planning considerations in further detail.

#### **7.03 Impact on archaeology/CAs/LBs or Areas of Special Character**

Not applicable to the proposed development.

#### **7.04 Airport safeguarding**

Not applicable to the proposed development.

#### **7.05 Impact on the green belt**

As noted, the site itself is not within the Green Belt, however land on the opposite side of Field Heath Road is designated Green Belt. Given the location of the proposed development, in the context of other existing built form, it is not considered that the proposal would cause harm to the character or openness of the Green Belt. However, this is notwithstanding the significant concerns about the impact of the development on the character and appearance of the area more generally as detailed in this report.

#### **7.06 Environmental Impact**

**NOISE FROM THE PROPOSED DEVELOPMENT:**

Policy D14 of the London Plan (2021) states that in order to reduce, manage and mitigate noise to improve health and quality of life, residential and non-aviation development proposals should manage noise by avoiding significant adverse noise impacts on health and quality of life.

The provision of 7 additional residential units, when compared with a 9-bed bed and breakfast is not considered to lead to such a significant change in the local noise environment as to warrant a refusal of planning permission on this ground. This is particularly the case as the site is located in close proximity to a commercial service yard and other commercial uses. The comings and goings may well be reduced when compared to the full occupancy levels of the existing bed and breakfast.

**AIR QUALITY:**

Policy DMEI 14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals should demonstrate appropriate reductions in emissions. It adds that, development proposals should, as a minimum:

- i) be at least "air quality neutral";
- ii) include sufficient mitigation to ensure there is no unacceptable risk from air pollution to sensitive receptors, both existing and new; and
- iii) actively contribute towards the improvement of air quality, especially within the Air Quality Management Area.

The site is designated within an Air Quality Management Area. In the event of an approval, a condition would be secured requiring the submission of an Air Quality Management Assessment to demonstrate how the above requirements have been addressed. Also, a condition would be secured requiring the submission of a Construction Management Plan to minimise air and other emissions caused during the construction phase. Given the recommendation to refuse, this information has not been requested.

#### SUSTAINABLE CONSTRUCTION AND USE OF RESOURCES:

Policy SI 2 of the London Plan (2021) states that residential development should achieve at least a 10% improvement beyond Building Regulations 2013.

Policy DMEI 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires all developments to make the fullest contribution to minimising carbon dioxide emissions in accordance with the London Plan targets (2016 The London Plan).

In the event of an approvable scheme, a condition would be included to ensure details are submitted to confirm the sustainability credentials of the proposed development, including the type of renewable technologies that would be utilised. Also, a condition would be secured requiring the proposed dwellings to achieve as a minimum, a water efficiency standard of no more than 105 litres per person per day, maximum water consumption. This is important, particularly in light of the objections received relating to this issue. More information would also have been requested to clarify any confusion around the provision of solar panels and or green roofs at the site.

Subject to the above conditions, the proposal could be compliant with Policies DMEI 2 and DMEI 10 of the Hillingdon Local Plan: Part 2- Development Management Policies (2020) and Policy SI 2 of the London Plan (2021).

#### **7.07 Impact on the character & appearance of the area**

Paragraph 126 of the NPPF (2021) seeks the creation of high quality, beautiful and sustainable buildings.

Paragraph 130 of the NPPF (2021) states 'Planning policies and decisions should ensure that developments:

- a) will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- b) are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;
- c) are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d) establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;
- e) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
- f) create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.'

Policies D1 and D3 of the London Plan (2021) require development proposals to be of high quality and to enhance the local context by delivering buildings and spaces that

positively respond to local distinctiveness.

Hillingdon Local Plan Part 1: Strategic Policies (2012) Policy BE1 states: 'The Council will require all new development to improve and maintain the quality of the built environment in order to create successful and sustainable neighbourhoods, where people enjoy living and working and that serve the long-term needs of all residents. All new developments should achieve a high quality of design in all new buildings, alterations, extensions and the public realm which enhances the local distinctiveness of the area, contributes to community cohesion and a sense of place.'

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states: 'All development, including extensions, alterations and new buildings will be required to be designed to the highest standards and, incorporate principles of good design including: i) harmonising with the local context by taking into account the surrounding scale of development, height, mass and bulk of adjacent structures; building plot sizes and widths, plot coverage and established street patterns; building lines and setbacks, rooflines, streetscape rhythm, for example, gaps between structures and other streetscape elements, such as degree of enclosure; architectural composition and quality of detailing; local topography, views both from and to the site; and impact on neighbouring open spaces and their environment.'

Policy DMHB 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) re-emphasises the need for new development to be well integrated with the surrounding area and provides design criteria as to how this would be achieved.

The development site is located on the north side of Pield Heath Road. The site comprises a two storey building which is currently used as bed and breakfast accommodation. The building has been finished in brick and hung tiles. In terms of style, the building is low level and resembles an extended chalet bungalow. The bulk of the building is well articulated and broken up as existing and it is well set back from the road. This helps the building to sit comfortably in its plot and context, despite failing to respond to the building line of neighbouring properties.

To the immediate west of the site is 44 Pield Heath Road, the site comprises a single storey, flat roofed, warehouse style building with rear service yard. The building is in commercial use. To the east of the site is 40 Pield Heath Road, a two storey semi detached property. The remaining area surrounding the site is predominantly residential with the exception of a few commercial uses. To the rear of the site are the semi detached dwellings which make up Newlyn Close.

The new building would be approximately 8.8m high, 15.5m wide and 16.8m deep. It would be finished in bricks, clay tiles and upvc window and would have a stepped frontage with triangular windows, rear and side dormers and a very large crown roof. It is also worth noting that a green roof with solar panels is proposed on the submitted second floor plan, however that plan does not match the roof plan. Were the application recommended for approval, this would have been resolved or conditioned, but does not change the outcome of the assessment as a whole.

Due to its oversized footprint and associated crown roof, uncharacteristic design, siting in the plot, increased height and increased overall depth when compared with the existing building, the proposed development would be harmful and excessive in context. The building would appear as an oversized form of backland development which would harm the appearance of the street scene and wider area. As the site does not follow the area's building line, the oversized building would be visually at odds with its neighbours from

multiple public and private viewpoints. Its uncharacteristic size and nature would be exacerbated by the fact that the site rises from the main road to the building, increasing the prominence of the proposed building.

The proposed building would also be out of character when viewed against other backland/garden development in the area, as it is much larger in scale and bulk. The excessive footprint fills the plot and the bulk is unrelieved given the profile and envelope proposed.

The roofscape of the area is characterised by pitched roofs and this is evident in all views on and around the site. The proposed crown roof would therefore be out of character with the established roofscape of the area, adding to the bulk, massing and design concerns.

Together, the bulky crown roof, partially obscure glazed triangular bay windows, squat dormers, stepped and blocky rear elevation are considered to result in a contrived, poorly designed building, which has no similarity to other buildings in the area, making it an uncharacteristic and unsympathetic addition to the area in terms of its design.

For these reasons, the proposal is considered to be harmful to the character and appearance of Pield Heath Road and the surrounding area contrary to Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (2012), Policies DMHB 11 and DMHB 12 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), Policies D1 and D3 of the London Plan (2021) and the National Planning Policy Framework (2021).

## **7.08 Impact on neighbours**

Paragraph 130 (f) of the NPPF (2021) states that new development should seek to 'create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.'

Hillingdon Local Plan Part 2: Development Management Policies (2020) Policy DMHB 11 sets out design guidance for all new development in the borough. Part B of the policy states 'development proposals should not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space.'

Guidance for Policy DMHB 11 states: 'The Council will aim to ensure that there is sufficient privacy for residents and it will resist proposals where there is an unreasonable level of overlooking between habitable rooms of adjacent residential properties, schools or onto private open spaces. A minimum of 21 metres separation distance between windows of habitable rooms will be required to maintain levels of privacy and to prevent the possibility of overlooking. In some locations where there is a significant difference in ground levels between dwellings, a greater separation distance may be necessary.'

Guidance for Policy DMHB 11 also states: 'For the purposes of this policy, outlook is defined as the visual amenity enjoyed by occupants when looking out of their windows or from their garden. The Council will expect new development proposals to carefully consider layout and massing in order to ensure development does not result in an increased sense of enclosure and loss of outlook. Single aspect dwellings should be avoided.'

Due to its height, bulk, depth and close proximity to No.1 Newlyn Close, the proposed development is considered to cause a significant loss of outlook from the rear windows and garden of that property. Furthermore, from the rear garden the building would appear

as an overbearing and visually oppressive structure to the detriment of their amenities. This sense of enclosure and feeling of the building towering over the garden would also impact No.2 Newlyn Close, next door.

The impact of the building on the rear gardens of properties on the Pield Heath Road would also be marked. The building would be closer to the boundary and much higher, with no articulation to the bulk due to the square envelope the building proposes. It is noted that the main amenity areas are set further away, but the change is notable and harmful given the proximity to this boundary. This sense of enclosure would also be unneighbourly and harmful.

The service yard of Jet Clean (44 Pield Heath Road) is located to the west of the site and the development. Whilst the bulk of the building would be particularly evident from this property due to its close siting to the boundary line and excessive size, height and footprint, it would not harm any living conditions and the commercial use could continue to function adjacent to the new building.

It is noted that a day and sunlight impact assessment has been submitted in support of the proposal (Ref: OPP-091175 DL2). The report demonstrates that the proposal would cause no harmful loss of light or overshadowing to neighbouring properties. The results are accepted however, do not overcome the concerns raised regarding outlook, enclosure and overbearing impact. The proposal is therefore considered to be contrary to Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) and paragraph 130 (f) of the NPPF (2021).

Overlooking is also a key issue and forms part of the amenity reason for refusal. The east facing side elevation dormer (serving Flat 7's living room) would provide views into the rear windows and gardens of No.1 and No.2 Newlyn Close at a distance of approximately 19.5m, causing a significant loss of privacy to its residents. This entire relationship is contrived, unacceptable and harmful.

It is accepted that at present, low level first floor dormers serving a single guest bedroom look toward the garden of No.3 Newlyn close at a distance of approximately 21m. This relationship is not considered directly comparable. The proposed development would result in the habitable first floor rear windows of flats 4, 5 and 6 looking toward the garden, as well as the roof level dormer windows of flat 7 (at a shorter distance). The proposal would therefore result in a significant increase in views toward No.3 Newlyn Close, from higher vantage points causing a harmful loss of privacy to the property's garden space and its rear elevation windows. The proposal is therefore considered to be detrimental to the amenities of No.3 Newlyn close, due to overlooking and privacy loss.

Despite attempts to obscure glaze views towards No.40 Pield Heath Road, the first floor front facing windows of Flat 4 (serving bedrooms) would still provide views into the rear garden and windows of No.40 (at distances of 15-16m). It is noted that the existing building provides some views toward the garden and rear windows of the mentioned property, however the views provided from Flat 4 would be more direct due to their position and the design of the windows. The proposed development is therefore considered to cause a harmful loss of privacy to the residents of that property, which would harm their amenities. The side dormer windows serving principal rooms at second floor level would also overlook the rear gardens of other properties on Pield Heath Road, as well as the neighbouring site at 2 Micawber Road.

The impact of the side dormer windows has been carefully considered as the Local Planning Authority could impose obscure glazing conditions to reduce overlooking.

However, obscure glazing these windows would not be appropriate given the rooms they serve and the limited outlook provided to the rear by the smaller rear dormers. The quality of these rooms would be compromised to an unacceptable degree if an obscure glazing condition was used to overcome the harm to rear neighbours. It is therefore concluded that the siting and design of the dormers and associated flats would be unacceptable and harmful and this is a result of the cramped, overly dense and poorly designed building.

#### **7.09 Living conditions for future occupiers**

##### **INTERNAL AMENITY SPACE PROVISION:**

Policy DMHB 15 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development will be required to comprise good design and create inclusive environments whilst improving safety and security by incorporating specific measures, which includes ensuring adequate defensible space is provided.

Policy DMHB 16 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all housing development should have an adequate provision of internal space in order to provide an appropriate living environment. The space standards set out in Table 5.1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) are the same as those found in Table 3.1 of the London Plan (2021).

Policy D6 of the London Plan (2021) sets out the minimum internal floor space standards required for residential developments in order to ensure that there is an adequate level of amenity for future occupants. Table 3.1 of Policy D6 of the London Plan (2021) states that:

- A one storey dwelling with 2 bedrooms, 3 person occupancy should provide a GIA of at least 61 square metres;

and

- A one storey dwelling with 2 bedroom, 4 person occupancy should provide a GIA of at least 70 square metres.

Also of importance is Policy D6 (8) of the London Plan (2021) which states: The minimum floor to ceiling height must be 2.5m for at least 75 per cent of the Gross Internal Area of each dwelling.

With this context in mind, each of the proposed flats would meet or exceed the floorspace requirements set out in Policy D6. Furthermore each unit would have the requisite floor to ceiling height. All habitable spaces would have good access to natural light and outlook.

All ground and first floor side elevation windows could be conditioned to be obscure glazed as they are secondary windows to their respective bedroom/kitchen/living room spaces. This would ensure future residents would have adequate privacy inside these rooms. Defensible space exists to the front of each ground floor front facing habitable room window, meaning that greenery could be erected via condition in order to ensure that front facing windows provide privacy for their occupants. Had the application been recommended for approval, such a condition would have recommended for inclusion on the decision.

However, as discussed above, the second floor dormer windows could not be obscured without compromising the quality of the flats at this level.

For the above reasons, the proposal would provide satisfactory living conditions for future

residents of the block itself, compliant with the policies noted above.

#### EXTERNAL AMENITY SPACE PROVISION:

Policy DMHB 18: 'Private Outdoor Amenity Space' of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that applications for residential development should provide adequate levels of private, well designed and located amenity space. The policy advises that 2 bedroom flats should be provided with 25sqm of private amenity space. The proposal includes 7x 2 bed flats and therefore requires 175sqm of amenity space.

Flats 1, 2 and 3 would be provided with private rear gardens ranging from 17m<sup>2</sup> - 21m<sup>2</sup>. A 178m<sup>2</sup> shared amenity space would also be provided for residents of the scheme. Whilst each unit would not have private amenity space, the shared communal area is of sufficient size to accommodate the external amenity space needs of future residents and therefore the amenity space provisions are considered to be acceptable, on balance.

#### **7.10 Traffic impact, Car/cycle parking, pedestrian safety**

##### PARKING AND TRIP GENERATION:

Policy DMT 2 of the Hillingdon Local Plan Part 2: Development Management Policies (2020) states:

'Development proposals must ensure that:

- i) safe and efficient vehicular access to the highway network is provided to the Council's standards;
- ii) they do not contribute to the deterioration of air quality, noise or local amenity or safety of all road users and residents;
- iii) safe, secure and convenient access and facilities for cyclists and pedestrian are satisfactorily accommodated in the design of highway and traffic management schemes;
- iv) impacts on local amenity and congestion are minimised by routing through traffic by the most direct means to the strategic road network, avoiding local distributor and access roads; and
- v) there are suitable mitigation measures to address any traffic impacts in terms of capacity and functions of existing and committed roads, including along roads or through junctions which are at capacity.'

Policy DMT 5 of the Hillingdon Local Plan Part 2: Development Management Policies (2020) states:

'A) Development proposals will be required to ensure that safe, direct and inclusive access for pedestrians and cyclists is provided on the site connecting it to the wider network, including:

- i) the retention and, where appropriate, enhancement of any existing pedestrian and cycle routes;
- ii) the provision of a high quality and safe public realm or interface with the public realm, which facilitates convenient and direct access to the site for pedestrian and cyclists;
- iii) the provision of well signposted, attractive pedestrian and cycle routes separated from vehicular traffic where possible; and
- iv) the provision of cycle parking and changing facilities in accordance with Appendix C, Table 1 or, in agreement with Council.'

Policy DMT 6 of the Hillingdon Local Plan Part 2: Development Management Policies (2020) states:

'Development proposals must comply with the parking standards outlined in Appendix C Table 1 in order to facilitate sustainable development and address issues relating to congestion and amenity. The Council may agree to vary these requirements when:



- i) the variance would not lead to a deleterious impact on street parking provision, congestion or local amenity; and/or
- ii) a transport appraisal and travel plan has been approved and parking provision is in accordance with its recommendations.

The Mayor of London adopted a new and revised London Plan in March 2021, consequently the car parking standards set out in the London Plan take precedence over those in the Local Development Plan, except where the Local Plan specifies lower local maximum standards.

The development site has a PTAL Rating of 3 and is located in Outer London. The London Plan advises that 1-2 bedroom properties in such areas should be provided with a maximum of 0.75 parking spaces per unit. The proposal includes the provision of 7no.x 2-bedroom flats and therefore should provide a maximum of 5 parking spaces. 5 parking spaces are proposed, as such the development would comply with the maximum parking standards in the London Plan (2021).

Notwithstanding the above, the existing controlled parking zone and the ongoing demand for spaces in this area is a material consideration. Were the development otherwise acceptable, a legal agreement would be required to mitigate any potential impact on this already saturated on-street parking area. The legal agreement would be necessary to restrict future residents from applying for parking permits. To date, no such legal agreement is in place to prohibit new residents from applying for parking permits and as such, this has been included as a recommended refusal reason. The absence of such an agreement could lead to roadside parking, congestion and reduced highway safety, as the Council's managed on street parking spaces could be in further demand by new residents where demand already exceeds the supply of on-street spaces.

Taking into consideration the above, the proposal fails to comply with Policy DMT 2 and DMT 6 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), as well as Policy T4 and T6 of the London Plan (2021).

It should be noted that the refusal reason relating to the completion of a legal agreement could be overcome, and a completed legal agreement would be accepted in the event of an appeal and/or resubmission.

#### ACCESS:

At present a 4.8m wide dropped kerb exists at the site with a crossover. The dropped kerb is slightly misaligned with the site's existing 3m wide vehicular access. The applicant seeks to widen the existing access from 3m to 3.4m. Additionally, the brick walled access would be brought closer to the road to align with the existing crossover. The height of the entrance brick wall would be to match the existing (stated to be 900mm).

The proposed access would be closer to the road and would not provide adequate visibility splays due to the brick walls proposed at the front of the site (which exceed 0.6m in height) and the fact that 1m planting is proposed behind the brick walls. The proposed access would therefore not allow vehicles exiting the site to adequately see people crossing the site. The new access would therefore endanger pedestrians and members of the public crossing the site.

It is noted that the existing access and crossover does not provide adequate visibility splays for those entering and exiting the site due to the excessive height of the brick walls at the site's entrance. However, this is an existing situation, which has served the bed and

breakfast for sometime without causing notable harm to the local highways network. The proposal would result in more people at the site (on a permanent basis) and more vehicle movements in and out of the site (on a permanent basis when compared with the site's existing use). It is therefore considered that together the new access and proposed development would reduce pedestrian and highway safety despite the existing access having substandard visibility splays. As a redevelopment proposal, a higher standard and degree of safety must be secured and is unacceptable as shown.

The proposed access alterations are therefore considered to cause harm to pedestrian safety and the local highways network, contrary to Policy DMT 2 and DMT 6 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020), as well as Policy T4 and T6 of the London Plan (2021).

#### ELECTRIC CHARGING POINTS:

Part G) of Policy T6 and part C) of Policy T6.1 of the London Plan (2021) state that all residential car parking spaces must provide infrastructure for electric or Ultra-Low Emission vehicles. At least 20 per cent of spaces should have active charging facilities, with passive provision for all remaining spaces. Accordingly, one car parking space should be provided with an active electric charging point and the remaining four car parking spaces should be provided with passive electric vehicle charging infrastructure. The submitted plans suggest that all car parking spaces would have EV charging provisions in compliance with policy guidance. If planning permission were to be granted, the provisions would have been secured by condition.

#### ACCESSIBLE PARKING:

In accordance with the Accessible Hillingdon SPD, 10% of car parking spaces must be for blue badge holders (disabled users) which equates to 1 car parking space. If this application had been recommended for approval, a condition would have been secured to ensure that 1 disabled space is secured.

#### BICYCLE PARKING:

Appendix C, Table 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires the provision of cycle parking for new residential developments. The Council's Highway Officer has advised: 'For a development of this type to be within policy at least 12no. [cycle parking spaces] would need to be provided.' A sizeable cycle store is to be located at the front of the site in an accessible location. No details have been provided regarding the external finishes and dimensions of the store, however the structure appears to be large enough to accommodate the requisite amount of cycle parking. Had this application been recommended for approval, full details of the dimensions, external finish and design of the cycle store would have been secured by condition.

#### REFUSE/RECYCLING COLLECTION:

Policy DMHB 11 part (d) of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development proposals should make sufficient provision for well-designed internal and external storage space for general, recycling and organic waste, with suitable access for collection. External bins should be located and screened to avoid nuisance and adverse visual impacts to occupiers and neighbours.

If the development were otherwise acceptable, full details of the proposed refuse store to

be sited near the sites entrance (including its dimensions, external finishes and design) would have been secured by condition.

#### **7.11 Urban design, access and security**

London Plan (2021) Policy D7 states: 'To provide suitable housing and genuine choice for London's diverse population, including disabled people, older people and families with young children, residential development must ensure that:

1) at least 10 per cent of dwellings (which are created via works to which Part M volume 1 of the Building Regulations applies) meet Building Regulation requirement M4(3) 'wheelchair user dwellings'

2) all other dwellings (which are created via works to which Part M volume 1 of the Building Regulations applies) meet Building Regulation requirement M4(2) 'accessible and adaptable dwellings'.

The Borough's Access Officer has reviewed the application and raises no objection, subject to the imposition of planning conditions. If planning permission were to be granted, it would be recommended that suitable conditions be attached to ensure compliance with the above standards.

#### **7.13 Provision of affordable & special needs housing**

Policy H2 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) states that housing provision is expected to include a range of housing to meet the needs of all types of households, and the Council will seek to maximise the delivery of affordable housing from all sites over the period of the Local Plan. For sites with a capacity of 10 or more units, the Council will seek to ensure that the affordable housing mix reflects housing needs in the borough, particularly the need for larger family units. This is supported by Policy DMH 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

The proposal is for less than 10 residential units and does not meet the threshold in order to require affordable housing provision. As such, the proposal is not contrary to Policy H2 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) and Policy DMH 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) in this respect.

#### **7.14 Trees, landscaping and Ecology**

TREES AND LANDSCAPING:

Policy G1 of the London Plan (2021) states that development proposals should incorporate appropriate elements of green infrastructure that are integrated into London's wider green infrastructure network.

Policy DMHB 14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that:

A) All developments will be expected to retain or enhance existing landscaping, trees, biodiversity or other natural features of merit.

B) Development proposals will be required to provide a landscape scheme that includes hard and soft landscaping appropriate to the character of the area, which supports and enhances biodiversity and amenity particularly in areas deficient in green infrastructure.

C) Where space for ground level planting is limited, such as high rise buildings, the inclusion of living walls and roofs will be expected where feasible.

The proposed site layout plan suggests that the proposal would have no adverse impact on significant trees. If planning permission were to be granted, it would be recommended that full landscaping details be secured via condition.

## ECOLOGY:

Policy DMEI 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that if development is proposed on or near to a site considered to have features of ecological or geological value, applicants must submit appropriate surveys and assessments to demonstrate that the proposed development will not have unacceptable effects. The development must provide a positive contribution to the protection and enhancement of the site or feature of ecological value.

The site does not contain any ponds, open woodland or dense scrub and shrubbery. There are no protected sites of ecological interest adjacent to the site. It is therefore considered that the likelihood of protected species being present at the site is low.

In the event of an approval, an informative would be secured advising that should protected species be found at the site, the applicant(s) must fulfill their duties under the Wildlife and Countryside Act 1981 (as amended).

### **7.17 Flooding or Drainage Issues**

Policy SI 12 of the London Plan (2021) states that development proposals should ensure that flood risk is minimised and mitigated, and that residual risk is addressed. Policy SI 13 of the London Plan (2021) states that development proposals should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible.

Policy DMEI 9 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals that fail to make appropriate provision for flood risk mitigation, or which would increase the risk or consequences of flooding, will be refused. Policy DMEI 10 states that development within areas identified at risk from surface water flooding which fail to make adequate provision for the control and reduction of surface water runoff rates will be refused.

The site lies within Flood Zone 1 of the Environment Agency's Flood Risk Map. This means the site is classified as being at low risk and defined as having a less than 1 in 1,000 probability of fluvial and tidal flooding. As such, there are no restrictions on development, including more vulnerable uses such as Use Class C3 (dwellinghouses), in this location, in terms of fluvial and tidal flood risk.

Notwithstanding the above, the site is located within a Critical Drainage Area. Had this application been recommended for approval a SuDS condition would have been recommended for inclusion on the decision notice to ensure that water was adequately and appropriately managed on site.

### **7.19 Comments on Public Consultations**

Comments received in response to the public consultation have been summarised in section 6 (above) and addressed within the relevant sections of the report.

### **7.20 Planning obligations**

Policy DMCI 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that to ensure development is sustainable, planning permission will only be granted for development that clearly demonstrates there will be sufficient infrastructure of all types to support it. Infrastructure requirements will be predominantly addressed through the Council's Community Infrastructure Levy (CIL).

The Council adopted its own Community Infrastructure Levy (CIL) on 1st August 2014. The Hillingdon CIL charge for residential developments is £95 per square metre of additional floor space. This is in addition to the Mayoral CIL charge of £60 per square

metre. CIL rates are index linked. The proposal involves the erection of new dwellings and is therefore CIL liable if planning permission were to be granted.

#### **7.21 Expediency of enforcement action**

Not applicable to the proposed development.

#### **7.22 Other Issues**

Not applicable to the proposed development.

### **8. Observations of the Borough Solicitor**

#### General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

#### Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

#### Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

#### Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be

given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

**9. Observations of the Director of Finance**

Not applicable to the proposed development.

**10. CONCLUSION**

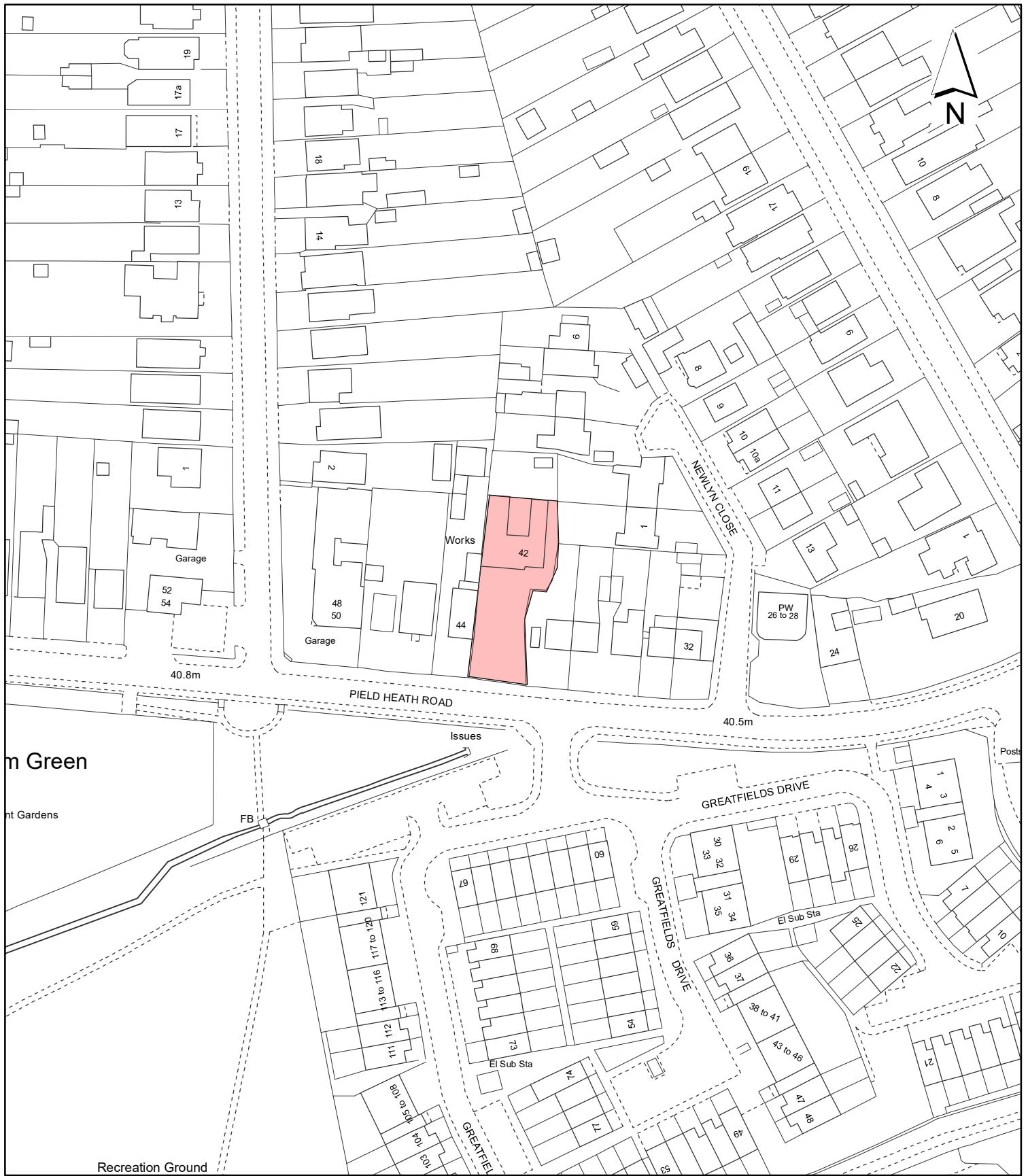
For the reasons set out in this report, it is considered that the proposed development would conflict with national, regional and local planning policies and guidance. The provision of 7 dwellings would not outweigh the identified harm. Consequently, it is recommended that the application be refused for the reasons set out in section 2 of this report.

**11. Reference Documents**

Hillingdon Local Plan: Part One - Strategic Policies (November 2012)  
Hillingdon Local Plan: Part Two - Development Management Policies (January 2020)  
The London Plan (March 2021)  
National Planning Policy Framework (NPPF) (July 2021)  
National Planning Practice Guidance (NPPG)  
Technical Housing Standards - Nationally Described Space Standard (2015) (as amended)

**Contact Officer:** Haydon Richardson

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

For identification purposes only.  
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Site Address:

**42 Pield Heath Road**

**LONDON BOROUGH OF HILLINGDON**  
**Residents Services Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**17611/APP/2022/993**

Scale:

**1:1,250**

Planning Committee:

**BoroughPage 31**

Date:

**December 2022**



**HILLINGDON**  
 LONDON

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A

**Item No.** Report of the Interim Director of Planning, Regeneration & Public Realm

**Address** 10 ST LUKE CLOSE COWLEY UXBRIDGE

**Development:** Erection of a two storey side/rear extension and a single storey rear extension. Roof light located on rear roof slope with the demolition of outbuildings.

**LBH Ref Nos:** 36391/APP/2022/2104

**Drawing Nos:** 3

4

2

5 - dated 20-10-22

8

6

1

7

10

Daylight and Sunlight Assessment by Plantit Received 01-07-22

**Date Plans Received:** 01/07/2022

**Date(s) of Amendment(s):**

**Date Application Valid:** 01/07/2022

Recommendation: APPROVAL, subject to conditions.

**DEFERRED ON 1st November 2022 FOR SITE VISIT ON 9th November 2022**

The application was deferred at the 01/11/22 Borough Planning Committee for members to visit the application site. This visit was undertaken at 10am on 09/11/22. Members visited the frontage of St Luke Close, the rear garden of the application property, the rear garden of No.12 next door and the rear garden of No.15 at the other end of the same terrace group.

During the site visit members asked for the following points to be clarified:

1. The distance from the new flank wall of the proposed side extension to the rear windows of the properties on St Nicholas Close. This was confirmed as circa 25m.
2. The distance retained to the shared boundary with properties on St Nicholas Close. This was confirmed as 2.8m.
3. What width a new two storey side extension would need to be in order to be policy compliant i.e. meeting all the criteria set out in Policy DMHD 1. This was confirmed as a width of 3m, half the width of the existing property. It was noted that the proposed side extension would be 1.3m wider than this, to match the historic side extension at the other end of the terrace.
4. Following this discussion it was noted that the proposal differs from the existing side extension at No.15, despite being the same width, as a two storey rear extension is also

proposed. No.15 does not have a first floor rear extension.

5. Whether or not the first floor rear extension would be compliant with Policy DMHD 1 in its own right having regard to criterion vi-x (detailed in full, below). This was confirmed as the extension is set away from the nearest first floor habitable room window, outside the 45 degree line of sight, there are no windows overlooking neighbours (which are not comparable to existing openings in size and direction), the extension has a pitched roof, the site is not in a conservation area and the building is not listed.

6. Whether the existing outbuilding at No.15, the existing stores to the rear of the terrace and existing conservatory at No.12 would be material considerations at a planning appeal. It was confirmed that all built form on and adjacent to the site should inform the decision, particularly in the context of Policy DMHB 11.

7. The depth of the existing rear conservatory at No.12 was confirmed as approximately 4.5m and this has also been confirmed using the Council's GIS imagery. It was noted that the 3.4m deep extension proposed at No.10 would not meet or exceed this depth.

8. The depth of the existing stores on the site (and serving neighbouring properties) were confirmed as likely to be original to the houses. These existing projections have a depth of 5.5m. It was confirmed that the 3.4m deep extension at the application property would not meet or exceed this depth and the existing store serving the application property would be demolished to facilitate this.

9. It was confirmed that the 45 degree line was met in relation to No.12 next door.

10. It was confirmed that this would be the first rear extension to be sited at first floor level within this particular terrace group.

11. It was confirmed that the splayed side boundary, showing No.12 St Luke Close's garden widening to the rear was accurate.

12. It was confirmed that the two existing first floor rear windows serving No.10 would remain as existing and the rear extension would not cover the existing rear elevation. This was confirmed as the rear extension is predominantly sited behind the new side extension, up to the first floor window. The original, main rear wall would remain largely unextended at first floor level if permission were granted.

13. The parking arrangement as existing was noted. Members noted the presence of numerous cars on street. It was confirmed that the extensions would not require an increased parking provision as there is no change of use, however a new house or a change of use would likely require additional parking. This was a key reason for refusal when a new house was proposed at the site.

14. It was confirmed that the existing garage at the front of the site would be demolished.

15. It was confirmed that the property would remain in use as a single family dwelling

and that planning permission would be required for a change of use to a HMO or to convert the property into flats.

16. Members noted the size and position of the nearest side facing windows serving No.8 (the non-attached neighbour).

17. Finally, a query was raised regarding the ownership of the fenced off parcel of land next to the garage on the site. Whilst land ownership is not a material consideration the Council has checked the information it has available and this appears to be in the ownership of No.8, the non-attached neighbour.

Following the site visit a second petition was received with 27 signatures.

## 1. CONSIDERATIONS

### 1.1 Site and Locality

The application site is located on the east side of St Luke Close, at the end of the cul-de-sac. The site comprises an end of terrace dwelling. The existing dwelling is set back from the street with off street parking and access to a detached garage.

The surrounding area is characterised by two-storey terraced and semi detached properties finished in brick, with tiled roofs. Each property is set back from the main road, allowing for off street parking and/or front gardens with hard and soft landscaping.

The application site is not designated within a Conservation Area, nor an Area of Special Local Character, however the Cowley Church (St. Laurence) Conservation Area is located beyond the east of the application site. The site does not contain any Listed Buildings. There are no trees that are subject to a Tree Preservation Order within the site or on adjoining land. To the rear of the site is the Green Belt and the site is on potentially contaminated land.

### 1.2 Proposed Scheme

The application seeks planning permission for the erection of a two storey side extension and a part single, part two storey rear extension. The proposal also includes the installation of a roof light on the rear roof slope and the demolition of outbuildings.

### 1.3 Relevant Planning History

31969/APP/2015/909      8 St Luke Close Cowley Uxbridge

Two storey, 2-bed, detached dwelling with associated parking and amenity space and installation of vehicular crossover to front

**Decision Date:** 01-09-2015      Refused      **Appeal:**

36391/APP/2022/521      10 St Luke Close Cowley Uxbridge

Erection of 1 x three-bedroom detached dwelling with associated amenity space.

**Decision Date:** 09-05-2022      Refused      **Appeal:**

39000/A/87/0309

15 St Luke Close Cowley Uxbridge

Erection of a two storey flank extension.

**Decision Date:** 06-04-1987

Approved

**Appeal:**

### **Comment on Planning History**

See above for relevant planning history to the site.

On the site itself, permission was refused in May 2022 for the "erection of 1 x three-bedroom detached dwelling with associated amenity space". The application was refused for the following reasons:

1. The proposed dwelling fails to provide adequate parking for its future residents. The shortfall in parking is likely to result in roadside parking which would create a more hazardous environment for road users and pedestrians. The proposal would therefore result in a more dangerous environment for pedestrians and vehicle users, having an adverse impact on the free flow of traffic on the local highways network. The development is therefore contrary to Policies DMT 2 and DMT 6 of the Hillingdon Local Plan: Part Two - Development Management Policies (January 2020), as well as policy T4 of the London Plan (2021).

2. Due to the proposed design, height, irregular width, siting and uncharacteristic frontage, the proposed dwelling would form a harmful and incongruous addition to the streetscene of St Luke Close. The new property would appear unusually narrow and linear when compared with other dwellings and proposes an irregular access which includes only a footpath, as well as a flank in close proximity to the rear gardens of neighbours. The proposed dwelling would therefore be detrimental to the character, appearance and visual amenities of the street and wider area. The proposal would therefore be contrary to Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (November 2012), Policies DMHB 11, DMHB 12 of the Hillingdon Local Plan: Part Two - Development Management Policies (January 2020), as well as policies D3 and D6 of the London Plan and paragraphs 130 (a) and (c) of the NPPF (2021).

3. Due to its size, siting, proximity, height, bulk and massing the proposed dwelling would lead to a loss of outlook from the rear bedroom and kitchen diner windows of No.10 St Luke Close as well as an overbearing sense of enclosure to the rear garden of No.13 St Nicholas Close. For the same reasons the new dwelling would have an enclosing impact on the rear patio of No.10, whilst also leading to overshadowing and a significant loss of light to these adjacent spaces. The proposal would adversely impact on the amenities and living conditions of No.10 and No.12's occupiers as a result and is therefore contrary to Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies and paragraph 130 (f) of the NPPF (2021).

Also of relevance is the historic approval for a two storey side extension at the other end of the terrace, at no.15 St Luke Close.

## **2. Advertisement and Site Notice**

**2.1** Advertisement Expiry Date:- Not applicable

**2.2** Site Notice Expiry Date:- Not applicable

### 3. Comments on Public Consultations

8 neighbouring properties were consulted on the 21-07-22. Two neighbour objections were received during this time raising the following, summarised concerns:

- The proposal would result in a visually intrusive and discordant development, detracting from the visual amenities of St Nicholas Close.
- The proposal would disturb the openness of the view enjoyed by those on St Nicholas Close and nearby house.
- The proposal fails to harmonise with the street, character and proportions of existing houses.
- The proposal fails to comply with the Hillingdon Planning requirements.
- The proposal goes beyond the walls of the original property.
- The proposal would detract from the visual amenities of the streetscene and surrounding nearby houses and gardens.
- The boundary line is incorrect.
- The left hand side outbuilding belongs to no.15 St Nicholas Close, not no.17 as shown.
- The proposed plan shows no space between the outbuilding and boundary line.
- Same comments and objections exist relating to the previous application which was refused.
- There have been various applications for new homes in St Luke Close in recent years and none have been within planning guidelines for such a small cul-de-sac.
- The plans submitted show there is a clear indication that the house will be used as a HMO.
- Concerns about the existing use of the property.
- Concerns about the standard of the building and ongoing maintenance of no.10.
- A HMO would result in increased pressure on parking, as demonstrated by other HMOs on the street. Parking is already difficult and no additional parking would be provided for increased residents.
- Some of the internal passageways of the extensions are too narrow for building standards.
- The extensions would affect light to No.12.
- The extension would alter the symmetry of the 4 property terrace.
- The extension would cause a loss of privacy to no.8.
- The extension would be out of keeping, overdevelopment and detrimental to St Luke Close.

Additionally, a valid petition with 82 signatures within Colham & Cowley Ward has been received in objection to the application.

Case Officer Comment:

The key issues noted from the objections relate to character and appearance of the proposed development, as well as the impact on residential amenity and parking. These issues have been discussed in detail, below and are material to the decision.

Issues relating to the boundary line are noted. The block plans originally submitted incorrectly labelled the outbuildings. Whilst ownership is not a material planning consideration, revised drawings have been sought and received to resolve this issue by labelling the outbuildings as "Existing Outbuildings on Nicolas Close [sic], indicative positions".

The impact on neighbouring properties has been fully assessed during the course of the

application, with both a site visit and aerial imagery to fully understand the relationship.

Concerns raised by residents regarding potential use as an HMO are duly noted. However, it should be noted that the area is subject to an Article 4 restriction which removes permitted development rights normally in place for change of use of a dwellinghouse (C3) to use class C4 (houses in multiple occupation). Consequently, planning permission would be required for such a change of use and the impacts of such a change would therefore be subject to consideration through the planning process.

Comments regarding building standards are duly noted, however these would be addressed through the Building Regulations and consequently are not material to this planning decision.

#### **4. Local Plan Designation and London Plan**

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

Part 2 Policies:

DMHB 1 Heritage Assets

DMHB 4 Conservation Areas

DMHB 11 Design of New Development

DMHB 18 Private Outdoor Amenity Space

DMHD 1 Alterations and Extensions to Residential Dwellings

DMEI 6 Development in Green Edge Locations

DMEI 4 Development on the Green Belt or Metropolitan Open Land

DMT 6 Vehicle Parking

LPP D6 (2021) Housing quality and standards

LPP D3 (2021) Optimising site capacity through the design-led approach

LPP HC1 (2021) Heritage conservation and growth

#### **5. MAIN PLANNING ISSUES**

The main considerations are the design and impact on the character of the existing property, the impact upon the street scene and locality, the impact upon the amenities of adjoining occupiers, the reduction in size of the rear garden and car parking provision. The recent planning history, whilst for a different scheme, also carries some weight in this decision.

Character and Appearance:

The Hillingdon Local Plan: Part One (2012) Strategic Policy BE1 seeks a quality of design in all new development that enhances and contributes to the area in terms of form, scale and materials; is appropriate to the identity and context of the townscape; and would

improve the quality of the public realm and respect local character.

Policy DMHB 11: Design of New Development states that:

A) All development, including extensions, alterations and new buildings will be required to be designed to the highest standards and, incorporate principles of good design including:

- i) harmonising with the local context by taking into account the surrounding:
  - scale of development, considering the height, mass and bulk of adjacent structures;
  - building plot sizes and widths, plot coverage and established street patterns;
  - building lines and setbacks, rooflines, streetscape rhythm, for example, gaps between structures and other streetscape elements, such as degree of enclosure;
  - architectural composition and quality of detailing;
  - local topography, views both from and to the site; and
  - impact on neighbouring open spaces and their environment.
- ii) ensuring the use of high quality building materials and finishes;
- iii) ensuring that the internal design and layout of development maximises sustainability and is adaptable to different activities;
- iv) protecting features of positive value within and adjacent to the site, including the safeguarding of heritage assets, designated and un-designated, and their settings; and
- v) landscaping and tree planting to protect and enhance amenity, biodiversity and green infrastructure.

Policy DMHD 1 requires that alterations and extension of dwellings would not have an adverse cumulative impact on the character and appearance of the street scene, and should appear subordinate to the main dwelling. It also requires that there is no unacceptable loss of outlook to neighbouring occupiers.

With specific regard to rear extensions, Policy DMHD 1 states that:

- i) single storey rear extensions on terraced or semi-detached houses with a plot width of 5 metres or less should not exceed 3.3 metres in depth or 3.6 metres where the plot width is 5 metres or more;
- ii) single storey rear extensions to detached houses with a plot width of 5 metres or more should not exceed 4.0 metres in depth;
- iii) flat roofed single storey extensions should not exceed 3.0 metres in height and any pitched or sloping roofs should not exceed 3.4 metres in height, measured from ground level;
- iv) in Conservation Areas and Areas of Special Local Character, flat roofed single storey extensions will be expected to be finished with a parapet;
- v) balconies or access to flat roofs which result in loss of privacy to nearby dwellings or gardens will not be permitted;
- vi) two storey extensions should not extend into an area provided by a 45-degree line of sight drawn from the centre of the nearest ground or first floor habitable room window of an adjacent property and should not contain windows or other openings that overlook other houses at a distance of less than 21 metres;
- vii) flat roofed two storey extensions will not be acceptable unless the design is in keeping with the particular character of the existing house;
- viii) pitched roofs on extensions should be of a similar pitch and materials to that of the original roof and subordinate to it in design. Large crown roofs on detached houses will not be supported; and
- ix) full width two storey rear extensions are not considered acceptable in designated areas or as extensions to Listed Buildings or Locally Listed Buildings.

With regard to side extensions, Policy DMHD 1 requires:

- i) side extensions should not exceed half the width of the original property;
- ii) extensions to corner plots should ensure that the openness of the area is maintained and the return building line is not exceeded;
- iii) garages should reflect the size guidelines set out in Appendix C Parking standards;
- iv) two storey side extensions should be set in a minimum of 1 metre from the side boundary or in the case of properties in the Copse Wood and Gatehill Estates, at least 1.5 metres, but more if on a wider than average plot, in order to maintain adequate visual separation and views between houses;
- v) two storey side extensions to detached and semi-detached properties should be set back a minimum of 1 metre behind the main front elevation;
- vi) where hip to gable roof extensions exist, a two storey side extension will not be supported; and
- vii) in Conservation Areas, single storey side extensions may be required to be set back.

The design of this proposal is materially different to that of the recent refusal for a new house. The previous proposal was refused on design grounds (reason 2 detailed above), however was for a detached, narrow building which would infill the side garden area of no.10. This new proposal for an extension is markedly different.

The proposed two storey side extension would adjoin the application property. The side extension would not be set back from the front elevation of the property, compliant with policy DMHD 1 as the application property forms the end of a terrace. It would also retain a generous gap to the side boundary of 3m. However, with a proposed width of 4.3m from the flank wall of the existing dwelling, the extension is in direct conflict with criterion i) quoted above, exceeding more than half the width of the original property.

In line with Section 38 of the Planning and Compulsory Purchase Act 2004, decisions on planning applications 'must be made in accordance with the development plan unless other material considerations indicate otherwise'. In this particular instance there is a key material consideration that must weigh heavily in the balance. At the other end of the same terrace group, at no. 15 St Luke Close, there is an existing extension of a very similar width and design to that proposed here. This extension is historic, granted in April 1987, but is nonetheless a notable addition to the terrace group and wider streetscene. The proposed extension would, as a result, help to balance the terrace group by introducing an extension of a comparable width, size and massing and this would be appreciated in both streetscene and longer distance views.

With this in mind, the side extension is considered acceptable. Whilst overly wide in relation to the main house, failing to read as a subordinate addition to no.10 itself, it would have a positive impact on the balanced composition and overall design of the terrace group.

The proposed single storey rear extension would have a depth of 3.4 metres with a pitched roof of 3.3 metres maximum. The two storey element would have a depth of 3.4m also, with a width of 6.2m adjoining onto the two storey side extension. The roof design of the two storey rear element would have a pitched roof and would retain a 0.5m gap from the ridge of the pitched roof to the main dwelling. A new single rooflight is proposed to be inserted to the rear roofslope.



The rear extension is also considered acceptable in terms of its design. Given the size of the proposed side extension and the overall increase to the width of the house, the extension would still read as subordinate. The plot is generous enough in size and overall area to accommodate the extension whilst still retaining a sense of openness to all boundaries. The roof design is also compatible with that of the main house and would read as subordinate as a result.

The rear extension would not be readily visible from the streetscene. Key views would be from the open space (designated conservation area and green belt) behind as well as private views from properties on St Nicholas Close. Given the existing backdrop of built form from all these viewpoints and the distance to boundaries, this is again, acceptable in terms of the visual impact. There would be no harm caused to the setting of the conservation area or the character or openness of the green belt.

As a whole, the extensions fail, in part, to accord with the requirements of Policy DMHD 1. However, other material considerations exist in this particular case to warrant a grant of planning permission. Further, the extensions comply with the requirements of Policy DMHB 11 as the design harmonises with the local context and takes account of the surrounding scale of development, including the bulk of adjacent structures, including the extension at no.15. On balance, the proposal is acceptable and is still considered to comply with the overarching aims of the development plan.

#### Residential Amenity:

Paragraph A1.23 of the Hillingdon Local Plan Part 2 (2020) states no direct overlooking will be permitted. Adequate distance should be maintained to any area from which overlooking may occur. Regard should be given to the character of the area and the distances between buildings but as a guide, the distance should not be less than 21m between facing habitable rooms and windows.

Policy DMHB 11: Design of New Development also states that:

B) Development proposals should not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space.

Policy DMHD 1 (referred to above) also seeks to ensure a satisfactory relationship between dwellings in maintained.

The key properties affected by the development are the properties on St Nicholas Close who share their rear garden boundary with the application site, No. 12 St Luke close which is the attached neighbour and No. 8 St Luke Close which is the non-attached neighbour to the west.

With regard to St Nicholas Close, the proposed side extension would be built closer to these properties. However, there would still be approximately 26 metres retained from the flank wall of the side extension to the rear of the properties on this street. The separation distance to the boundary mitigates any harmful sense of enclosure to these neighbours and there would be no harmful loss of light or outlook.

This relationship is materially different to the scheme recently refused permission for a new house (detailed above) where the refusal reason included an objection to the "overbearing sense of enclosure to the rear garden of No.13 St Nicholas Close". In this

application, the new house was proposed to be sited directly adjacent to the shared boundary with the new two storey building towering over the shared boundary line and associated garden. Side windows at first floor level have been conditioned to be obscure glazed (refer to section 6 of this report below) to prevent any overlooking and a further condition is recommended to ensure any new windows would require planning permission.

With regard to No. 12 St Luke Close, the proposed rear extension would not extend beyond the rear building line of this neighbour at the proposed depth of 3.4m. The two storey rear extension would not extend into the area provided by a 45 degree line of sight and is considered to not create an unduly detrimental impact for this neighbour. The extensions would not cause a harmful loss of light or outlook, or increased sense of enclosure and new windows would not cause a loss of privacy.

No. 8 St Luke Close was not noted in the previous reason for refusal for a new house on the site. Nevertheless, the impact must be carefully considered, due to the close proximity of the two properties. The extension would not be visible from the rear windows of this neighbour and would therefore have no impact, but would be visible from the front windows. Whilst this is the case, the existing building at no.10 already sits within this sight line and the additional width would not result in undue or material harm to this neighbour. This is helped by the orientation of the properties, the boundary line and the separation distances that exist.

With reference to all properties, it is also helpful to note that the Daylight and Sunlight Assessment concludes that the impact to the neighbouring dwellings would be negligible. The findings of this report are accepted and not disputed. Overall, the proposed extensions are not considered to cause harm to neighbouring residential amenity, complying with the requirements of Policy DMHB 11 and DHMD 1 in this regard.

In addition to the above, it is considered that all of the proposed habitable rooms added or altered by the proposal in the host dwelling would have a satisfactory outlook and access to natural light in compliance with Policy D6 of the London Plan (2021).

#### Private Amenity Space:

The proposed development will still retain approximately 200sqm of private amenity space after being built where the minimum standards for a 4+ bedroom house is 100 sqm. It is therefore considered that the proposal would not undermine the provision of external amenity space for the existing occupiers at the site, in compliance with Policies DMHB 11 and DMHB 18 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020).

#### Parking and Highway Safety:

Given its siting to the side, retaining the front driveway of the property, the parking provision would be unaffected by the proposed development. It is noted that two additional bedrooms would be created in the new extension, but this uplift would not be so great as to create a harmful increased demand for street parking. The extension is therefore not considered to prejudice highway safety, in accordance with Policy DMT 6 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020).

The previous refusal of a new house was refused on the lack of parking provided, but

again, this was materially different. Additional occupiers would require their own parking provision, in a different way to the extension of an existing home.

**Other Issues:**

As noted, the area is identified as being potentially contaminated, according to the Council's GIS records. Responsibility for ensuring a safe development rests with the applicant and it would not be proportionate to require submission of a contaminated land assessment report as part of this planning application for a householder extension. Nonetheless, an informative note has been recommended for inclusion on the decision notice.

**Conclusion:**

The main body of the report shows that the proposal complies with the overarching aims and objectives of relevant policies contained within the development plan. The development would not harm the amenities of neighbouring residents to an unacceptable degree and would have an acceptable impact on the character and appearance of the host property and terrace. Planning permission is therefore recommended for approval, subject to conditions.

**6. RECOMMENDATION**

**APPROVAL subject to the following:**

**1 HO1 Time Limit**

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

**REASON**

To comply with Section 91 of the Town and Country Planning Act 1990.

**2 HO2 Accordance with approved**

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers; 5 dated 20-10-22, 6, 7, 8 and 10.

**REASON**

To ensure the development complies with the provisions of the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020), and the London Plan (2021).

**3 HO4 Materials**

The materials to be used in the construction of the external surfaces of the development hereby permitted shall match those used in the existing building and shall thereafter be retained as such.

**REASON**

To safeguard the visual amenities of the area and to ensure that the proposed development does not have an adverse effect upon the appearance of the existing building in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**4** HO5 No additional windows or doors

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (or any order revoking and re-enacting that Order with or without modification), no additional windows, doors or other openings shall be constructed in the walls or roof slopes of the development hereby approved.

**REASON**

To prevent overlooking to adjoining properties in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**5** HO6 Obscure Glazing

The first floor window(s) facing the south towards St Nicholas Close shall be glazed with permanently obscured glass to at least scale 4 on the Pilkington scale and be non-opening below a height of 1.8 metres taken from internal finished floor level for so long as the development remains in existence.

**REASON**

To prevent overlooking to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**INFORMATIVES**

**1** Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance 'The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit ([www.hillingdon.gov.uk/noise](http://www.hillingdon.gov.uk/noise) Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

**2** The Council will recover from the applicant the cost of highway and footway repairs, including damage to grass verges.

Care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering

materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense.

For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

- 3** You are advised there is mapped evidence that the site is situated on land which may be subject to contamination. The applicant is advised to carry out their own investigations and take appropriate precautions as part of the development. For information about contaminated land, please refer to <https://www.gov.uk/contaminated-land/dealing-with-contamination>.

### Standard Informatives

- 1 The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).
- 2 The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including The London Plan - The Spatial Development Strategy for London consolidated with alterations since 2011 (2016) and national guidance.

#### Part 1 Policies:

PT1.BE1 (2012) Built Environment

#### Part 2 Policies:

DMHB 1 Heritage Assets  
DMHB 4 Conservation Areas  
DMHB 11 Design of New Development  
DMHB 18 Private Outdoor Amenity Space  
DMHD 1 Alterations and Extensions to Residential Dwellings  
DMEI 6 Development in Green Edge Locations  
DMEI 4 Development on the Green Belt or Metropolitan Open Land  
DMT 6 Vehicle Parking  
LPP D6 (2021) Housing quality and standards  
LPP D3 (2021) Optimising site capacity through the design-led approach  
LPP HC1 (2021) Heritage conservation and growth

- 3 You are advised this permission is based on the dimensions provided on the approved drawings as numbered above. The development hereby approved must be constructed precisely in accordance with the approved drawings. Any deviation from these drawings requires the written consent of the Local Planning Authority.
- 4 You are advised that if any part of the development hereby permitted encroaches by either its roof, walls, eaves, gutters, or foundations, then a new planning application may have to be submitted. The validity of this planning permission may be challengeable by third parties if the development results in any form of encroachment onto land outside the applicant's control that is considered to cause harm to local amenity.
- 5 Your attention is drawn to the need to comply with the relevant provisions of the Building Regulations, the Building Acts and other related legislation. These cover such works as - the demolition of existing buildings, the erection of a new building or structure, the extension or alteration to a building, change of use of buildings, installation of services, underpinning works, and fire safety/means of escape works. Notice of intention to demolish existing buildings must be given to the Council's Building Control Service at least 6 weeks before work starts. A completed application form together with detailed plans must be submitted for approval before any building work is commenced. For further information and advice, contact - Building Control, 3N/01 Civic Centre, Uxbridge (Telephone 01895 558170).
- 6 You have been granted planning permission to build a residential extension. When undertaking demolition and/or building work, please be considerate to your neighbours and do not undertake work in the early morning or late at night or at any time on Sundays or Bank Holidays. Furthermore, please ensure that all vehicles associated with the construction of the development hereby approved are properly washed and cleaned to prevent the passage of mud and dirt onto the adjoining highway. You are advised that the Council does have formal powers to control noise and nuisance under The Control of Pollution Act 1974, the Clean Air Acts and other relevant legislation. For further information and advice, please contact - Environmental Protection Unit, 4W/04, Civic Centre, High Street, Uxbridge, UB8 1UW (Tel. 01895 250190).
- 7 The Party Wall Act 1996 requires a building owner to notify, and obtain formal agreement from, any adjoining owner, where the building owner proposes to:
- carry out work to an existing party wall;
  - build on the boundary with a neighbouring property;
  - in some circumstances, carry out groundworks within 6 metres of an adjoining building.
- Notification and agreements under this Act are the responsibility of the building owner and are quite separate from Building Regulations, or Planning Controls. The Building Control Service will assume that an applicant has obtained any necessary agreements with the adjoining owner, and nothing said or implied by the Council should be taken as removing the necessity for the building owner to

comply fully with the Party Wall Act. Further information and advice is to be found in "the Party Walls etc. Act 1996 - explanatory booklet" published by the ODPM, available free of charge from the Planning Services Civic Centre, Uxbridge, UB8 1UW.

8 Your attention is drawn to the fact that the planning permission does not override property rights and any ancient rights of light that may exist. This permission does not empower you to enter onto land not in your ownership without the specific consent of the owner. If you require further information or advice, you should consult a solicitor.

9 Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with: -

A) Demolition and construction works should only be carried out between the hours of 08.00 hours and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays Bank and Public Holidays.

B) All noise generated during such works should be controlled in compliance with British Standard Code of Practice BS 5228: 1984.

C) The elimination of the release of dust or odours that could create a public health nuisance.

D) No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit, 3S/02, Civic Centre, High Street, Uxbridge, UB8 1UW (Tel.01895 277401) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

10 You are advised that care should be taken during the building works hereby approved to avoid spillage of mud, soil or related building materials onto the pavement or public highway. You are further advised that failure to take appropriate steps to avoid spillage or adequately clear it away could result in action being taken under the Highways Act.

11 To promote the development of sustainable building design and construction methods, you are encouraged to investigate the use of renewable energy resources which do not produce any extra carbon dioxide (CO2) emissions, including solar, geothermal and fuel cell systems, and use of high quality

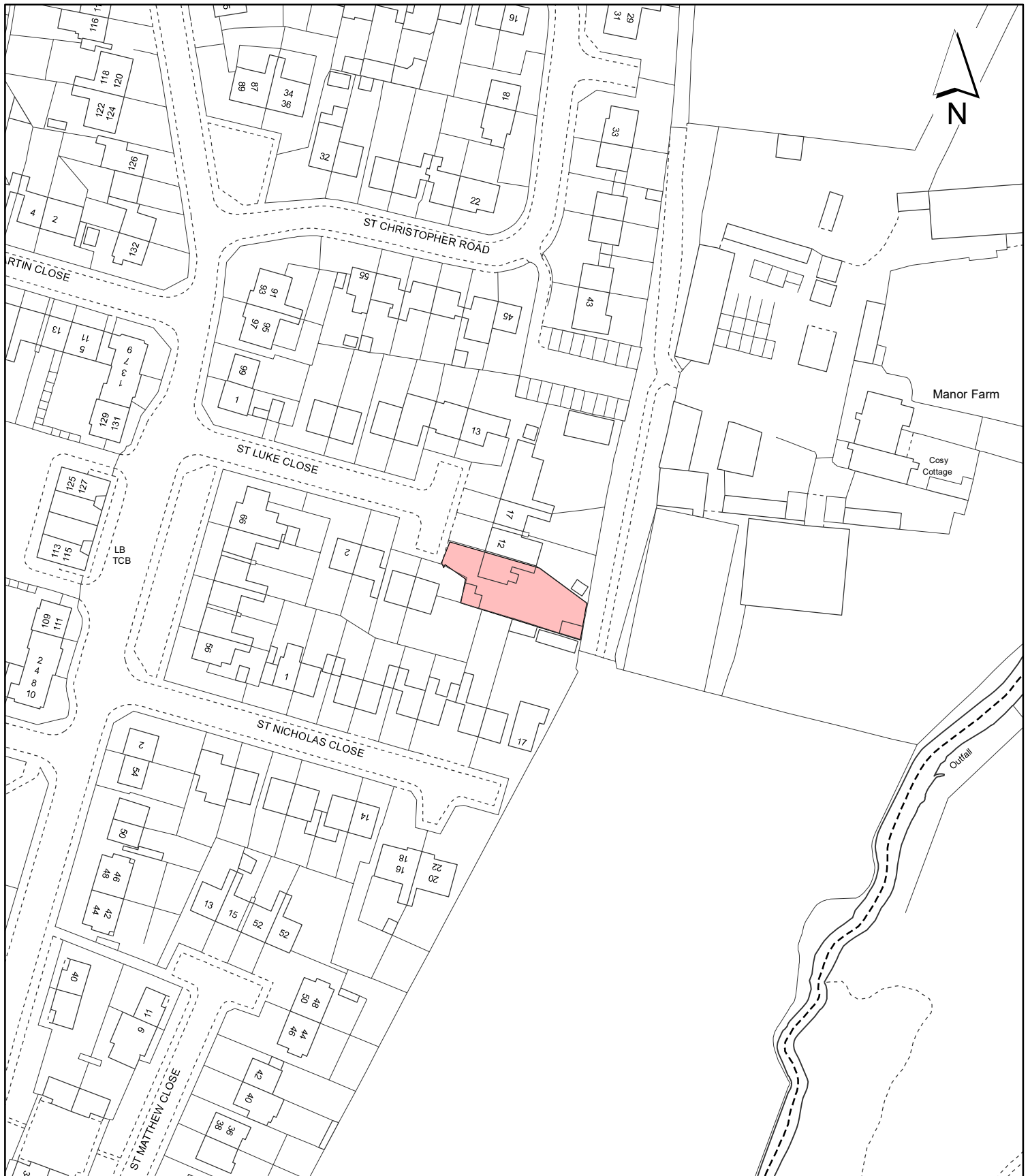
insulation.

- 12 You are advised that care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense. For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

**Contact Officer:** Zara Raza

**Telephone No:** 01895 250230





**Notes:**

 Site boundary

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Site Address:

**10 St Luke Close**

**LONDON BOROUGH OF HILLINGDON**  
 Residents Services  
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**36391/APP/2022/2104**

Scale:

**1:1,250**

Planning Committee:

**BoroughPage 49**

Date:

**December 2022**



**HILLINGDON**  
 LONDON

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**Item No.**            **Report of the Interim Director of Planning, Regeneration & Public Realm**

**Address**            R/O 25-31 WARREN ROAD ICKENHAM

**Development:**    Erection of 4 no. two storey, detached houses, with habitable accommodation in roof space, garage and associated landscaping, parking and installation of vehicular crossover.

**LBH Ref Nos:**     77265/APP/2022/2845

**Drawing Nos:**    Daylight, Sunlight, and Overshadowing Impact Assessment, dated September 2022, Ref: PR513\_V0  
Planning Statement prepared by Barker Parry Town Planning Ltd dated 25th October 2022  
22/3521/10 Rev. A  
22/3521/11 Rev. A  
22/3521/12 Rev. A  
22/3521/13 Rev. A  
22/3521/14 Rev. A  
22/3521/15 Rev. A  
22/3521/16 Rev. A  
22/3521/17 Rev. A  
22/3521/18 Rev. A  
22/3521/19  
22/3521/20  
Construction Management Plan dated October 2022  
DS20042201.03 (Tree Protection Plan)  
Ecological Survey by AA Environmental Ltd dated 6th October 2022  
Survey 1:200 @ A1 (Ground levels)  
567:001 (Refuse Vehicle AutoTrack Layout)  
Site Location Plan  
Design and Access Statement dated August 2022  
Tree Survey Report by Patrick Stileman Ltd, dated 19th May 2022, Ref: DS20042201

**Date Plans Recieved:**    09/09/2022                            **Date(s) of Amendment(s):**    09/09/2022

**Date Application Valid:**    09/09/2022

**1.    SUMMARY**

The application proposes the erection of 4 no. two storey, detached houses, with habitable accommodation in the roof space, garages and associated landscaping, parking and installation of a vehicular crossover.

With regards to the criteria of Policy DMH 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), it is considered that the proposed development passes the 'exceptionality test'. It is therefore considered, on balance, that the principle of introducing four detached dwellings on land originally forming the rear gardens of numbers 25-31 Warren Road is acceptable. In reaching this position, due weight has also been afforded to the fact that the existing properties on Heythrop Drive and Walnut Tree Close have been built on land that encompasses the former rear gardens of neighbouring properties on Warren Road and Woodstock Drive.

Having regard to the siting, scale, height, massing and design, it is considered that the proposed dwellings would not cause harm to the character and appearance of the area. The proposed development would not unduly impact on the living conditions of neighbouring residential occupiers. The proposed dwellings would provide a high standard of internal and external amenity space for future occupiers. The Highways Department is satisfied that the proposal would not present a risk to road safety, hinder the free flow of traffic, or lead to parking stress.

The proposal would deliver a high quality housing development which would widen the choice of family housing in the Borough, in accordance with the relevant planning policies and guidance in the Development plan.

It is therefore recommended that planning permission is granted, subject to the imposition of relevant planning conditions.

## **2. RECOMMENDATION**

### **APPROVAL subject to the following:**

#### **1 RES3 Time Limit**

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

#### **REASON**

To comply with Section 91 of the Town and Country Planning Act 1990.

#### **2 RES4 Accordance with Approved Plans**

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted drawings, titled Site Location Plan and numbered 22/3521/10 Rev. A, 22/3521/11 Rev. A, 22/3521/12 Rev. A, 22/3521/13 Rev. A, 22/3521/14 Rev. A, 22/3521/15 Rev. A, 22/3521/16 Rev. A, 22/3521/17 Rev. A, 22/3521/18 Rev. A, 22/3521/19, 22/3521/20 and shall thereafter be retained/maintained for as long as the development remains in existence.

#### **REASON**

To ensure the development complies with the provisions of the Hillingdon Local Plan Part 1 (2012), Part 2 (2020) and the London Plan (2021).

#### **3 RES6 Levels**

No development shall take place until plans of the site showing the existing and proposed ground levels and the proposed finished floor levels of all proposed dwellings hereby approved have been submitted to and approved in writing by the Local Planning Authority. Such levels shall be shown in relation to a fixed and known datum point. Thereafter the development shall not be carried out other than in accordance with the approved details.

#### **REASON**

To ensure that the development relates satisfactorily to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

#### **4 OM19 Construction Management Plan**

Notwithstanding the submitted Construction Management Plan (dated October 2022) and the accompanying drawing ref. 22/23521/19, prior to the commencement of any site clearance, demolition or construction work, a revised demolition and construction management plan and drawing shall be submitted to and approved in writing by the Local Planning Authority. The plan shall detail:

- (a) The phasing of development works
- (b) The hours during which development works will occur (please refer to informative I15 for maximum permitted working hours)
- (c) Measures to prevent mud and dirt tracking onto footways and adjoining roads (including wheel washing facilities)
- (d) Traffic management and access arrangements (vehicular and pedestrian) and parking provisions for contractors during the development process (including measures to reduce the numbers of construction vehicles accessing the site during peak hours)
- (e) Measures to reduce the impact of the development on local air quality and dust through minimising emissions throughout the demolition and construction process
- (f) The storage of demolition/construction materials on site
- (g) The location on site of any site office

The approved details shall be implemented and maintained throughout the duration of the demolition and construction process.

**REASON**

An updated Construction Management Plan is required because the report received on 26.10.22 lacks sufficient detail and includes unacceptable construction hours, and the location of the two storey site office should be relocated away from neighbouring site boundaries. This is to safeguard the amenity of surrounding areas in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**5 RES7 Materials (Submission)**

Prior to the commencement of any works above damp proof course level, details of all materials and external surfaces shall have been submitted to and approved in writing by the Local Planning Authority. Thereafter the development shall be constructed in accordance with the approved details and be retained as such. Details should include information relating to make, product/type, colour and photographs/images.

**REASON**

To ensure that the development presents a satisfactory appearance in accordance with Policies DMHB 11 and DMHB 12 of the Hillingdon Local Plan Part 2 (2020).

**6 NONSC Non Standard Condition**

Prior to the commencement of the development hereby approved, a sustainability and energy statement shall be submitted to and approved in writing by the Local Planning Authority. The sustainability and energy statement shall demonstrate how a 10% reduction in carbon dioxide emissions beyond Building Regulations requirement Part L 2013 (TER Baseline) has been achieved including full technology specifications and locations. Thereafter, the development shall be carried out in accordance with the approved details.

**REASON**

To ensure the development contributes to minimising the effects of, and can adapt to a changing climate in accordance with Policies DMEI 2 and DMEI 10 of the Hillingdon Local Plan: Part 2- Development Management Policies (2020) and Policy SI 2 of the London Plan (2021).

**7 NONSC Non Standard Condition**

Prior to any works on site above damp proof course level, details of step free access via all points of entry and exit shall be submitted to, and approved in writing, by the Local Planning Authority. Such provision shall remain in place for the life of the building.

**REASON**

To ensure housing of an inclusive design is achieved and maintained in accordance with Policies D5 and D7 of the London Plan (2021).

**8 NONSC Non Standard Condition**

At least one dwelling hereby approved shall be constructed in accordance with the technical specifications for an M4(3) dwelling. The remaining dwellings shall be constructed in accordance with the technical specifications for an M4(2) dwelling as set out in Approved Document M to the Building Regulations (2010) 2015.

No dwelling shall be occupied until certification of compliance with the above technical specifications has been submitted to, and approved in writing, by the Local Planning Authority. All such provisions must remain in place for the life of the buildings.

**REASON**

To not only allow the Building Control body to require the development to comply with the optional Building Regulations standards, but to also ensure the appropriate quantity and standard of accessible and adaptable housing is constructed and maintained in accordance with policy D7 of the London Plan.

**9 NONSC Non Standard Condition**

No development approved by this permission shall be commenced until a scheme for the provision of sustainable water management and water efficiency has been submitted to and approved in writing by the Local Planning Authority. The scheme shall:

- i. Provide information about the design storm period and intensity, the method employed to delay and control the surface water discharged from the site and the measures taken to prevent pollution of the receiving groundwater and/or surface waters;
- ii. Include a timetable for its implementation; and
- iii. Provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public authority or statutory undertaker and any other arrangements to secure the operation of the scheme throughout its lifetime.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:

- iv. Provide details of water collection facilities to capture excess rainwater;
- v. Provide details of how rain and grey water will be recycled and reused in the development;
- vi. Provide details of how the dwellings will achieve a water efficiency standard of no more than 110 litres per person per day maximum water consumption (to include a fixed factor of water for outdoor use of 5 litres per person per day in accordance with the optional requirement defined within Approved Document G of the Building Regulations).

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

**REASON**

To ensure the development does not increase the risk of surface water flooding in accordance with Policies DMEI 9 and DMEI 10 of the Hillingdon Local Plan Part 2 (2020) and Policies SI2 and SI 13 of the London Plan (2021).

**10 NONSC Non Standard Condition**

The demolition and construction works of the development hereby approved shall be carried out strictly in accordance with the tree protection measures specified in the Tree Survey Report by Patrick Stileman Ltd, dated 19th May 2022, Ref: DS20042201 and

approved drawing DS20042201.03 (Tree Protection Plan).

#### REASON

To ensure that trees and other vegetation can and will be retained and not damaged during construction work and to ensure that the development conforms with Policy DMHB 14 of the Hillingdon Local Plan Part 2 (2020).

#### **11 RES9 Landscaping (car parking & refuse/cycle storage)**

Notwithstanding the details specified on the approved drawings, and prior to the commencement of any works above damp proof course level, a landscape scheme shall have been submitted to and approved in writing by the Local Planning Authority. The scheme shall include: -

##### 1. Details of Soft Landscaping

1.a Planting plans (at not less than a scale of 1:100) including the proposed 13 new trees as specified in the application

1.b Written specification of planting and cultivation works to be undertaken

1.c Schedule of plants giving species, plant sizes, and proposed numbers/densities where appropriate

##### 2. Details of Hard Landscaping

2.a Refuse Storage

2.b Cycle Storage to provide secure and accessible parking for at least 2 cycles per dwelling

2.c Means of enclosure/boundary treatments

2.d Car Parking Layouts (including demonstration that at least 1 active and 1 passive electric charging point shall be provided per dwelling)

2.e Permeable Hard Surfacing Materials

##### 3. Details of Landscape Maintenance

3.a Landscape Maintenance Schedule for a minimum period of 5 years

3.b Proposals for the replacement of any tree, shrub, or area of turfing/seeding within the landscaping scheme which dies or in the opinion of the Local Planning Authority becomes seriously damaged or diseased

##### 4. Schedule for Implementation

##### 5. Details of ecological enhancements

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

#### REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with policies DMHB 11, DMHB 12, and DMHB 14 of the Hillingdon Local Plan Part 2 (2020) and Policies G5 and T6.1 of the London Plan (2021).

#### **12 RES10 Tree to be retained**

Trees, hedges and shrubs shown to be retained on the approved plan(s) shall not be damaged, uprooted, felled, lopped or topped without the prior written consent of the Local Planning Authority. If any retained tree, hedge or shrub is removed or severely damaged during (or after) construction, or is found to be seriously diseased or dying, another tree, hedge or shrub shall be planted at the same place or, if planting in the same place would leave the new tree, hedge or shrub susceptible to disease, then the

planting should be in a position to be first agreed in writing with the Local Planning Authority and shall be of a size and species to be agreed in writing by the Local Planning Authority and shall be planted in the first planting season following the completion of the development or the occupation of the buildings, whichever is the earlier. Where damage is less severe, a schedule of remedial works necessary to ameliorate the effect of damage by tree surgery, feeding or groundwork shall be agreed in writing with the Local Planning Authority. New planting should comply with BS 3936 (1992) 'Nursery Stock, Part 1, Specification for Trees and Shrubs'

Remedial work should be carried out to BS 3998:2010 'Tree work - Recommendations' and BS 4428 (1989) 'Code of Practice for General Landscape Operations (Excluding Hard Surfaces)'. The agreed work shall be completed in the first planting season following the completion of the development or the occupation of the buildings, whichever is the earlier.

#### REASON

To ensure that the trees and other vegetation continue to make a valuable contribution to the amenity of the area in accordance with policy DMHB 14 of the Hillingdon Local Plan Part 2 (2020) and to comply with Section 197 of the Town and Country Planning Act 1990.

#### **13 HO7 No roof gardens**

Access to the ground floor flat roofs of the development hereby approved shall be for maintenance or emergency purposes only and the flat roof shall not be used as a roof garden, terrace, balcony, patio or similar amenity area.

#### REASON

To prevent overlooking to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

#### **14 RES12 No additional windows or doors**

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (or any order revoking and re-enacting that Order with or without modification), no additional windows, doors or other openings shall be constructed in the side walls or side roof slopes of the dwellings hereby approved.

#### REASON

To prevent overlooking to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

#### **15 RES13 Obscure Glazing**

The first floor side windows detailed on approved drawings 22/3521/12 Rev. A, 22/3521/13 Rev. A, 22/3521/14 Rev. A and 22/3521/15 Rev. A shall be glazed with permanently obscured glass to at least scale 4 on the Pilkington scale and be non-opening below a height of 1.8 metres taken from internal finished floor level for so long as the development remains in existence.

#### REASON

To prevent overlooking to adjoining properties in accordance with policies DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

#### **16 RES14 Outbuildings, extensions and roof alterations**

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any order revoking and re-enacting that Order with or without modification); no garage(s), shed(s) or other outbuilding(s), nor extension



or roof alteration to any dwellinghouse(s) shall be erected without the grant of further specific permission from the Local Planning Authority.

**REASON**

To protect the character and appearance of the area and amenity of residential occupiers in accordance with policies DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**17 NONSC Non Standard Condition**

Prior to commencement of development, details of the access and connection to Heythrop Drive shall be submitted to the local planning authority for approval in writing. These details shall include the proposed materials and layout of the access including visibility splays, and a timetable for implementation. The access shall be implemented in accordance with the approved details prior to first occupation of any of the dwellings hereby approved and thereafter retained and maintained for the lifetime of the development.

**REASON**

To ensure suitable access to the site in accordance with Policy DMT 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policies T4, T6 and T6.1 of the London Plan (2021).

**18 NONSC Non Standard Condition**

Prior to commencement of development, an Air Quality Management Assessment shall be submitted to the Local Planning Authority for approval in writing. This shall demonstrate that the development will be at least air quality neutral and shall set out details of any required mitigation measures. Thereafter, the development shall be carried out strictly in accordance with the approved details and mitigation measures.

**REASON**

To accord with Policy DMEI 14 as the site is located in a designated Air Quality Management Area.

**INFORMATIVES**

**1 I70 LBH worked applicant in a positive & proactive (Granting)**

In dealing with the application the Council has implemented the requirement in the National Planning Policy Framework to work with the applicant in a positive and proactive way. We have made available detailed advice in the form of our statutory policies from Local Plan Part 1, Local Plan Part 2, Supplementary Planning Documents, Planning Briefs and other informal written guidance, as well as offering a full pre-application advice service, in order to ensure that the applicant has been given every opportunity to submit an application which is likely to be considered favourably.

**2 I52 Compulsory Informative (1)**

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

**3 I53 Compulsory Informative (2)**

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020) set out below, including Supplementary Planning Guidance, and to all relevant material

considerations, including The London Plan (2021) and national guidance.

DMEI 10	Water Management, Efficiency and Quality
DMEI 14	Air Quality
DMEI 2	Reducing Carbon Emissions
DMEI 7	Biodiversity Protection and Enhancement
DMEI 9	Management of Flood Risk
DMH 2	Housing Mix
DMH 6	Garden and Backland Development
DMH 7	Provision of Affordable Housing
DMHB 11	Design of New Development
DMHB 12	Streets and Public Realm
DMHB 14	Trees and Landscaping
DMHB 15	Planning for Safer Places
DMHB 16	Housing Standards
DMHB 17	Residential Density
DMHB 18	Private Outdoor Amenity Space
DMT 1	Managing Transport Impacts
DMT 2	Highways Impacts
DMT 5	Pedestrians and Cyclists
DMT 6	Vehicle Parking
LPP G7	(2021) Trees and woodlands
LPP H1	(2021) Increasing housing supply
LPP H10	(2021) Housing size mix
LPP H2	(2021) Small sites
LPP SI12	(2021) Flood risk management
LPP SI13	(2021) Sustainable drainage
LPP SI2	(2021) Minimising greenhouse gas emissions
LPP T4	(2021) Assessing and mitigating transport impacts
LPP T5	(2021) Cycling
LPP T6	(2021) Car parking
LPP T6.1	(2021) Residential parking
NPPF11	NPPF 2021 - Making effective use of land
NPPF12	NPPF 2021 - Achieving well-designed places
NPPF14	NPPF 2021 - Meeting the challenge of climate change flooding
NPPF2	NPPF 2021 - Achieving sustainable development
NPPF4	NPPF 2021 - Decision-Making
NPPF5	NPPF 2021 - Delivering a sufficient supply of homes
NPPF8	NPPF 2021 - Promoting healthy and safe communities
NPPF9	NPPF 2021 - Promoting sustainable transport

#### **4            I73            Community Infrastructure Levy (CIL) (Granting Consent)**

Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), this development is liable to pay the London Borough of Hillingdon Community Infrastructure Levy (CIL) and the Mayor of London's Community Infrastructure Levy (CIL). This will be calculated in accordance with the London Borough of Hillingdon CIL Charging Schedule 2014 and the Mayor of London's CIL Charging Schedule 2012. Before commencement of works the development parties must notify the London Borough of Hillingdon of the commencement date for the construction works (by submitting a Commencement Notice) and assume liability to pay CIL (by submitting an Assumption of Liability Notice) to the Council at [planning@hillingdon.gov.uk](mailto:planning@hillingdon.gov.uk). The Council will then issue a Demand Notice setting out the date and the amount of CIL that is payable. Failure to submit a valid Assumption of

Liability Notice and Commencement Notice prior to commencement of the development may result in surcharges being imposed.

The above forms can be found on the planning portal at: [www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil](http://www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil)

Pre-Commencement Conditions: These conditions are important from a CIL liability perspective as a scheme will not become CIL liable until all of the pre-commencement conditions have been discharged/complied with.

## **5 12 Encroachment**

You are advised that if any part of the development hereby permitted encroaches by either its roof, walls, eaves, gutters, or foundations, then a new planning application may have to be submitted. The validity of this planning permission may be challengeable by third parties if the development results in any form of encroachment onto land outside the applicant's control for which the appropriate Notice under Article 13 of the Town and Country Planning (Development Management Procedure) (England) Order 2015 has not been served.

## **6 16 Property Rights/Rights of Light**

Your attention is drawn to the fact that the planning permission does not override property rights and any ancient rights of light that may exist. This permission does not empower you to enter onto land not in your ownership without the specific consent of the owner. If you require further information or advice, you should consult a solicitor.

## **7 IT05 Wildlife and Countryside Act 1981**

Wildlife and Countryside Act 1981: Note that it is an offence under this act to disturb roosting bats, nesting birds or any other protected species. Therefore, if applicable, it is advisable to consult your tree surgeon / consultant to agree an acceptable time for carrying out the approved works.

### **3. CONSIDERATIONS**

#### **3.1 Site and Locality**

The site is located on the north side of Warren Road and encompasses parts of the former rear gardens of numbers 25-31 Warren Road. It includes trees that are protected by Tree Preservation Orders (TPO) nos. 740 and 793. It is noted that the trees at the site make a positive contribution to the sylvan quality of the area, by providing a visual break between the respective housing developments on Walnut Tree Close and Heythrop Drive. Access to the site would be gained from extending the turning head on Heythrop Drive.

The surrounding area is generally residential in character, mainly comprising detached properties. It is important to note that properties on the south side of Heythrop Drive originally formed part of the residential gardens of numbers 33 to 57 Warren Road. The properties on the north side of Heythrop Drive originally formed part of the residential gardens of numbers 30, 32-48 Woodstock Drive. The western boundary of the application site adjoins the residential gardens of numbers 1 to 3 Walnut Tree Close. The houses on Walnut Tree Close encompass part of the rear gardens of 17, 19, 21 and 23 Warren Road.

The site is not the subject of heritage policies. According to the Council's GIS, the site is designated within the Hillingdon Air Quality Management Area, Flood Zone 1 and has a Public Transport Accessibility Level (PTAL) of 1b (poor).

### **3.2 Proposed Scheme**

The application proposes the erection of 4 no. two storey, detached houses, with habitable accommodation in the roof space, garages and associated landscaping, parking and installation of a vehicular crossover.

It should be noted that the above proposal description reflects the amended wording that was agreed in writing by the agent. Neighbouring residents were re-consulted based on this revised wording (as discussed in Section 6 of this Committee Report).

### **3.3 Relevant Planning History**

#### **Comment on Relevant Planning History**

There is no relevant planning history attached to the application site.

However, it should be noted that planning permission has already been granted for the erection of dwellings on the former residential gardens of neighbouring properties on Warren Road. These approved dwellings have been built and form part of the well-established character of the area. Due weight has therefore been afforded to the following planning permissions.

In 2017, planning permission was granted at Appeal for the erection of 5 detached dwellings on the former rear gardens of numbers 17, 19, 21 and 23 Warren Road and the creation of a new access road from Applewood Close (LPA reference 62396/APP/2006/3042, Appeal reference APP/R5510/A/07/2050307/NWF). This scheme has been built and the approved dwellings are now known as numbers 1 to 5 Walnut Tree Close.

In 1987, outline planning permission was granted at Appeal for the houses on Heythrop Drive. Subsequent, planning applications were then submitted and approved in phases, i.e. 36883/F/93/1852 - numbers 15 to 19; 36883/G/93/1903 - numbers 1 to 7, 36883/M/95/935 - numbers 10 to 13, 6883/P/95/935 - numbers 21 & 22, 36883/T/95/1151 - 1 house, and 36883/X/98/498 - 1 house.

Please refer to Section 7.01 of this Committee Report for the 'principle of development' assessment.

## **4. Planning Policies and Standards**

Development Plan:

Planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

The Development Plan for the London Borough of Hillingdon currently consists of the following documents:

The Local Plan: Part 1 - Strategic Policies (2012)

The Local Plan: Part 2 - Development Management Policies (2020)

The Local Plan: Part 2 - Site Allocations and Designations (2020)

The West London Waste Plan (2015)

The London Plan (2021)

Material Considerations:

The National Planning Policy Framework (NPPF) (2021) is also a material consideration in planning decisions, as well as relevant supplementary planning documents and guidance.

## Local Plan Designation and London Plan

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

Part 2 Policies:

DMEI 10 Water Management, Efficiency and Quality

DMEI 14 Air Quality

DMEI 2 Reducing Carbon Emissions

DMEI 7 Biodiversity Protection and Enhancement

DMEI 9 Management of Flood Risk

DMH 2 Housing Mix

DMH 6 Garden and Backland Development

DMH 7 Provision of Affordable Housing

DMHB 11 Design of New Development

DMHB 12 Streets and Public Realm

DMHB 14 Trees and Landscaping

DMHB 15 Planning for Safer Places

DMHB 16 Housing Standards

DMHB 17 Residential Density

DMHB 18 Private Outdoor Amenity Space

DMT 1 Managing Transport Impacts

DMT 2 Highways Impacts

DMT 5 Pedestrians and Cyclists

DMT 6 Vehicle Parking

LPP G7 (2021) Trees and woodlands

LPP H1 (2021) Increasing housing supply

LPP H10 (2021) Housing size mix

LPP H2 (2021) Small sites

LPP SI12 (2021) Flood risk management

LPP SI13 (2021) Sustainable drainage

LPP SI2 (2021) Minimising greenhouse gas emissions

LPP T4 (2021) Assessing and mitigating transport impacts

LPP T5 (2021) Cycling

LPP T6 (2021) Car parking

LPP T6.1 (2021) Residential parking

NPPF11 NPPF 2021 - Making effective use of land

NPPF12 NPPF 2021 - Achieving well-designed places

NPPF14 NPPF 2021 - Meeting the challenge of climate change flooding

NPPF2 NPPF 2021 - Achieving sustainable development

NPPF4 NPPF 2021 - Decision-Making

NPPF5	NPPF 2021 - Delivering a sufficient supply of homes
NPPF8	NPPF 2021 - Promoting healthy and safe communities
NPPF9	NPPF 2021 - Promoting sustainable transport

## 5. Advertisement and Site Notice

5.1 Advertisement Expiry Date:- Not applicable

5.2 Site Notice Expiry Date:- Not applicable

## 6. Consultations

### External Consultees

12 neighbouring properties were originally consulted on 26th September 2022. Four additional properties were consulted on 3rd October 2022 and 28 further properties were consulted on 11th October 2022. Following the receipt of revised drawings, neighbouring properties were re-consulted for an additional 14-days with consultation expiring on 18th November 2022.

Representations from 28 neighbouring properties have been received in objection to the proposal. In addition, an objection has been received from the Ickenham Residents Association (detailed below) and an e-petition with 58 signatories (also detailed below) has been received.

The matters raised in the individual representations from neighbouring properties are summarised as follows:

- Overlooking and loss of privacy
- Felling of trees
- Harm to protected trees at the site
- Construction hours should be restricted, in the interest of residential amenity
- Right to light
- Noise, disruption and pollution
- Insufficient capacity in schools and doctors
- A three-metre high fence should be erected to the rear of Nos. 6 and 7 Windrush Close to deter criminals
- Land registry schedule requires residents on Heythrop Drive to pay for repairs and maintenance of the road
- The construction hours and duration would result in unacceptable impacts on the living conditions of neighbouring properties.

**PLANNING OFFICER RESPONSE:** Material planning considerations will be discussed in the following sections of this report. In the event of an approval, a condition would be secured requiring full details in respect to boundary treatment and means of enclosure. The concerns raised in respect to land ownership obligations are not considered to be material planning considerations. Comments regarding provision of schools and medical facilities are duly noted, however this would not constitute a reasonable ground for refusal in this case. For developments of the scale proposed, infrastructure contributions are predominantly addressed through the CIL regime (refer to section 7.20 of this report).

An e-petition against the application with 58 signatories was received by the Council. The grounds for opposition are as follows:

"We call upon Hillingdon Council to immediately reject this poorly thought-out and damaging application which will adversely affect the quality of life for residents in Heythrop Drive and adjacent roads both in the short term and long term. We call upon Hillingdon Council to immediately refuse this plan and send it back to the developers for further consideration with an instruction that if they

wish to move ahead with the plan they only do this after comprehensive and detailed consultation with all Hillingdon Residents affected in Heythrop Drive and adjacent roads to the satisfaction of all residents.

London Borough of Hillingdon has failed to carry out consultation with nearly all residents of Heythrop Drive despite the fact this plan would involve a 20%+ extension of the road and 4 new large detached houses."

**PLANNING OFFICER RESPONSE:**

In terms of the consultation process, 12 of the nearest neighbouring properties to the application site were consulted, as per the requirements of Planning legislation and Hillingdon's Statement of Community Involvement. However, during the course of the planning application process, letters were subsequently sent to all of the properties on Heythrop Drive to ensure a thorough consultation process has been carried out.

Ickenham Residents Association has submitted the following comments:

"This is an application for a back garden development - in fact over 4 back gardens. This Association objects in principle to developments in back gardens and we trust you will take this into consideration when coming to your decision"

**PLANNING OFFICER RESPONSE:**

This matter is addressed in section 7.01 of this report concerning the principle of the development.

Ministry of Defence Safeguarding- RAF Northolt:

The Defence Infrastructure Organisation (DIO) Safeguarding Team represents the Ministry of Defence (MOD) as a consultee in UK planning and energy consenting systems to ensure that development does not compromise or degrade the operation of defence sites such as aerodromes, explosives storage sites, air weapon ranges, and technical sites or training resources such as the Military Low Flying System.

The application is: Erection of 4 no. two storey, detached houses, with habitable accommodation in roof space, garage and associated landscaping, parking and installation of vehicular crossover on land rear of 25-31 Warren Road (with access from Heythrop Drive), Ickenham.

The application site occupies the statutory safeguarding zone(s) surrounding RAF Northolt. In particular, the aerodrome height, technical and birdstrike safeguarding zones surrounding the aerodrome and is approx. 3.17 km from the centre of the airfield.

After reviewing the application documents, I can confirm the MOD has no safeguarding objections to this proposal.

**Internal Consultees**

**COUNCIL'S TREES OFFICER:**

(Original comments received on 03.10.2022)

The proposed trees for removal are all category C trees (this is acceptable) and the locations for replacement trees appear appropriate.

We would need further details as to their tree protection plans and method statement to determine likely impact on the remaining trees, although if you are minded to approve this could be covered under a pre-commencement condition.

In response, the applicant has submitted a Tree Protection Plan (drawing no. DS20042201.03) which includes an Arboricultural Method Statement. Also, the drawing shows details of the no-dig zones, temporary ground protection and protective fencing that would be installed around the retained trees.

The Council's Trees Officer has reviewed the additional details that have been provided and has raised no objection, subject to the tree protection measures being adhered to.

**COUNCIL'S ACCESS OFFICER:**

(Original comments received on 30.09.2022)

This proposal for 4 dwelling houses raises accessibility concerns. Policy D7 of the 2021 London Plan requires all new housing to be designed and constructed as accessible and adaptable in accordance with M4(2) as set out in Approved Document M to the Building Regulations (2015 edition).

Revised plans should be requested prior to any grant of planning permission to address the following issues.

1. Step free access into the dwellings is required. Details should be submitted to satisfy the above policy requirement.
2. The entrance level WCs are too small, do not achieve the required clear access zones or otherwise meet the provisions set out in section 2.27(c) of Approved Document M, volume 1 (2015 edition).
3. Whilst re-evaluating the plans, particular attention should also be paid to the entrance lobby arrangement and to spatial requirements within the bedrooms, bathrooms and kitchen area. The plans should be amended accordingly to demonstrate compliance with the technical specifications within Approved Document M to the Building Regulations (2015 edition).

Conclusion: Details should be submitted prior to any planning approval."

The applicant has submitted revised drawings to address the above points. The Council's Access Officer is satisfied with the revised drawings, and has recommended two conditions in the event that planning permission were to be granted. Suitable conditions have been adopted in this recommendation for approval.

**COUNCIL'S NOISE OFFICER:**

The proposed residential development is enclosed within an established residential area hence I have no further comment to make.

**COUNCIL'S HIGHWAYS OFFICER:**

**Site Characteristics:**

The site is located within an extensive private residential catchment in Ickenham situated just north of the A40. The substantive plot consists of vacant land which lies to the rear of 25-31 Warren Road and is to be accessed via the eastern (and part private/unadopted) section of a 'double-legged' cul-de-sac (Heythrop Drive) which feeds off Woodstock Drive. This 'leg' would effectively be extended to front all four plots.

4 new four-bedroom detached dwellings are proposed (plots 1-4) with a newly created access at the north-eastern unadopted end of Heythrop Drive with a total of 13 on-plot parking spaces.

The immediate roadways are devoid of parking controls and the site address exhibits a public transport accessibility level (PTAL) rating of 1b which is considered as 'very poor' and therefore



encourages a higher dependency on the ownership and use of private motor transport.

#### Parking Provision & Internal Parking Layout:

Hillingdon Local Plan: Part 2 Policy - DMT 6 requires that new development will only be permitted where it accords with the council's adopted parking standards unless it can be demonstrated that a deviation from the standard would not result in a deleterious impact on the surrounding road network.

London Plan (2021): Policy T6.1 (Residential Parking) requires that new residential development should not exceed the maximum parking standards as set out in table 10.3.

In line with the adopted Hillingdon parking standard, the maximum parking requirement for all four plots would equate to a maximum of 2 spaces per unit whilst, by contrast for a PTAL rating of 1b, the London Plan (2021) parking standard demands a marginally lesser 1.5 spaces per unit.

Plots 1 to 3 display 3 spaces per unit (garage plus 2 on-plot spaces) whilst plot 4 displays a double garage with 2 on-plot spaces totaling 4. This exceeds the local and regional standard.

However, in this specific case this excess is considered acceptable as it reduces the potential for untoward parking displacement within and outside the site envelope resulting from the higher dependency on the private motor car due to the 'very poor' PTAL rating and absence of on-street parking controls.

#### Electric Vehicle Charging Points (EVCP's):

In line with the London Plan (2021), within the final parking quantum there is a requirement for a minimum of 20% 'active' EVCP provision with all remaining spaces being designated as 'passive' provisions. The applicant confirms that 1 'active' EVCP will be provided per dwelling which is welcomed and considered acceptable. However, all of the remaining spaces should also be designated as 'passive' provisions which has not been confirmed by the applicant, hence this aspect should be secured by way of planning condition.

#### Cycle Parking:

In terms of cycle parking there should be a provision of 2 secure and accessible spaces for each unit to conform to the adopted borough cycle parking standard. The applicant confirms that this level of provision will be inclusive to the each of the new garages which is considered acceptable.

#### Site Access and Internal Site Layout:

Access would be facilitated via a new opening from Heythrop Drive (unadopted highway and situated on the eastern flank of the site). There is no in principle objection to the proposed new opening (which incidentally, the Highway Authority (HA) has no direct jurisdiction over) onto Heythrop Drive which is private in tenure at the proposed connection point.

The principle of the new internal access road is also considered acceptable in the site circumstance supported by the internal layout and arrangement of the new roadway within the site envelope broadly conforming to the Department for Transport's (DfT) - Manual for Streets (MfS) (circa 2007) best practice for road and parking layouts. Owing to the proposed turning head within the site envelope, the layout will allow for passenger & refuse/delivery vehicles using the site to enter and leave the site in a forward gear which is the recommended practice on highway safety grounds as demonstrated by the submitted swept path analysis.

#### Vehicular Trip Generation:

Local Plan: Part 2 Policies - DMT 1 and DMT 2 require the council to consider whether the traffic generated by proposed developments is acceptable in terms of the local highway and junction capacity, traffic flows and conditions of general highway or pedestrian safety.

The proposal would clearly increase traffic generation from the site as compared to the existing dormant plot. However, with 4 new units connecting onto Heythrop Drive, any resultant vehicular movement would be considered minimal and relatively insignificant in highway impact terms. It is noted that the Planning Inspectorate is highly likely to take a similar view if the application were to be refused on excessive traffic generation and appealed thereafter. Hence, in conclusion, any likely uplift would be considered marginal in generation terms and therefore can be absorbed within the local road network without notable detriment to traffic congestion and road safety. There are no further observations.

#### Operational Refuse Requirements:

Refuse collection would be conducted via the extended roadway commencing from Heythrop Drive. In order to conform to accepted 'waste collection distances' from the public highway, any waste should be positioned within 10m of a refuse vehicle on the roadway. The indicated bin store positioning for each plot broadly conforms to the above parameter hence there are no further observations.

#### Construction Logistics Plan (CLP):

A full and detailed CLP would be a requirement given the constraints and sensitivities of the local residential road network in order to avoid/minimise potential detriment to the public realm. It will need to be secured under a suitable planning condition.

#### Conclusion:

The application has been reviewed by the Highway Authority who are satisfied that the proposal would not discernibly exacerbate congestion or parking stress, and would not raise any measurable highway safety concerns, in accordance with Local Plan: Part 2 Development Plan Policies DMT 1, DMT 2 & DMT 6 and Policy T4, T5 and T6 of the London Plan (2021).

## **7. MAIN PLANNING ISSUES**

### **7.01 The principle of the development**

Paragraph 119 of the NPPF (2021) states that planning policies and decisions should promote an effective use of land in meeting the need for homes and other uses, while safeguarding and improving the environment and ensuring safe and healthy living conditions.

Policy GG4 of the London Plan (2021) seeks to ensure that more homes are delivered. This is reinforced by Policy H1 of the Local Plan: Part 1 - Strategic Policies (2012) which gives general support to housing provision to meet and exceed the Council's minimum strategic dwelling requirement, where this can be achieved in accordance with other Local Plan policies.

Policy DMH 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that there is a presumption against the loss of gardens due to the need to maintain local character, amenity space and biodiversity. In exceptional cases a limited scale of backland development may be acceptable, subject to the following criteria:

- i) Neighbouring residential amenity and privacy of existing homes and gardens must be maintained and unacceptable light spillage avoided;
- ii) Vehicular access or car parking should not have an adverse impact on neighbours in

terms of noise or light. Access roads between dwellings and unnecessarily long access roads will not normally be acceptable;

- iii) Development on backland sites must be more intimate in mass and scale and lower than frontage properties; and
- iv) Features such as trees, shrubs and wildlife habitat must be retained or re-provided.

Policy DMH 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) does not define what constitutes an 'exceptional case' where a limited scale of backland development may be acceptable, nor does the supporting text contain a definition of this term. However, the supporting text to Policy DMH 6 explains that the Council's restrictive approach has been informed by the 2016 London Plan and reflects the direct and indirect value of gardens which includes the contribution to local character. Based on the wording of Policy DMH 6, it is considered that the policy presumption against development upon residential garden, is in the interest of maintaining local character, amenity space and biodiversity.

Whilst it is noted that the land forming the application site is under separate ownership from numbers 25-31 Warren Road, its designation as former rear garden land would nevertheless remain unchanged. The glossary in Hillingdon Local Plan: Part 1 - Strategic Policies (2012) defines 'back-land development' as "Development of 'landlocked' sites behind existing buildings, such as rear gardens and private open space, usually within predominantly residential areas. Such sites often have no street frontages." Based on this definition, it is considered that the application site would constitute as 'backland development' and that Policy DMH 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) should be applied.

With regards to criteria i) of Policy DMH 6, it is considered that the proposed development would not unduly impact on the living conditions of neighbouring residential occupiers for the reasons discussed in section 7.08 of this Committee Report. Given the position of the proposed access point and on-site car parking spaces, it is considered that no adverse impact would be caused for neighbouring occupiers, in terms of noise or light. The proposal therefore complies with criterion ii) of Policy DMH 6. The layout of the proposal has been devised so that the proposed dwellings would be provided with a street frontage that is consistent with the existing properties on the southern side of Heythrop Drive. For the reasons discussed in Section 7.07 of this Committee Report, it is considered that the proposed development would not cause harm to the character and appearance of the area. As such, the objectives of criterion iii) have been complied with.

All the Category A and B TPO protected trees would be retained as part of the proposed development. Several Category C trees would be lost. However, the Council's Tree Officer has raised no objection to the loss of these specific trees due to their limited health and low visual amenity value. Furthermore, weight has been given to the 13 new trees being proposed to mitigate the loss of the Category C trees. The planting of the proposed new trees would be secured through the imposition of a condition, if planning permission were to be granted. The submitted Ecology Report confirms that the likelihood of protected species being present at the site is low. The proposal therefore complies with criterion iv) of Policy DMH 6.

Also, due weight has been afforded to the fact that the existing properties on Heythrop Drive and Walnut Tree Close have been built on land that encompasses the former rear gardens of neighbouring properties on Warren Road and Woodstock Drive. Refer to Section 3.3 of this Committee Report for the relevant planning history.

Given the factors set out in the preceding paragraphs, it is considered that it could be

contended that the proposed development would pass the exceptionality test set out in Policy DMH 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020). However, this is subject to the other planning considerations discussed in this Committee Report, and the proposal being in accordance with all of the relevant planning policies and guidance within the Development Plan.

#### HOUSING MIX:

Policy H10 of the London Plan (2021) states that new development should consist of a range of unit sizes.

Policy DMH 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that the Council will require the provision of a mix of housing units of different sizes in schemes of residential development to reflect the Council's latest information on housing need. The Council's current information on housing need indicates a substantial borough-wide requirement for larger affordable and private market units, particularly 3 bedroom properties, as identified in the Strategic Housing Market Assessment 2016.

In accordance with Policy DMH 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), developments should demonstrate how the provision of family housing has been optimised, to address local needs. The proposed development would provide 3 x four-bedroom houses, which would make a contribution towards the Borough's need for family sized housing. Accordingly, it is considered that the proposal would consist of an appropriate housing mix in accordance with Policy DMH 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Policy H10 of the London Plan (2021).

#### **7.02 Density of the proposed development**

Policy D3 of the London Plan (2021) states that all development must make the best use of land by following a design-led approach that optimises the capacity of sites. Higher density developments should generally be promoted in locations that are well connected to jobs, services, infrastructure and amenities by public transport, walking and cycling. In other areas, incremental densification should be actively encouraged by Boroughs to achieve a change in densities in the most appropriate way. This should be interpreted in the context of Policy H2 of the London Plan (2021) which states that Boroughs should proactively support well-designed new homes on small sites below 0.25 hectares in size.

Policy DMHB 17 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all new residential development should take account of the Residential Density Matrix contained in Table 5.2, which recommends a density range of 105-175 habitable rooms per hectare and 35-50 units per hectare for sites in non-town centre areas with a PTAL rating of 0-2.

The site is located within a non-town centre area (i.e. more than 800 metres away from the closest town centre) and has a PTAL rating of 1b (very poor). Based on a total site area of 0.24 hectares, the proposed scheme would have a density level of 16 units per hectare and 129 habitable rooms per hectare. Whilst the unit number per hectare would be low in relation to the density range quoted above, it is considered that this is more reflective of the immediate local area. Furthermore, it is noted that the number of habitable rooms per hectare would accord with the density range set out in Policy DMHB 17 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

Notwithstanding the density range set out in Policy DMHB 17, it is considered that what is of greater significance to the determination of this application is the local contextual

factors. The key consideration is therefore whether the development would acceptably integrate with the character and appearance of the locality and would respect residential amenity considerations, rather than the consideration of the numerical density of the proposal. Please refer to the other sections of this Committee Report which assess these planning considerations in further detail.

#### **7.03 Impact on archaeology/CAs/LBs or Areas of Special Character**

Not applicable.

#### **7.04 Airport safeguarding**

Policy DMAV 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that the Council will ensure that uses such as housing, education and hospitals are not located in areas significantly affected by aircraft noise without acceptable mitigation measures.

The application site is within 3km of the RAF Northolt Zone. However, as the site is within an established residential area within this zone, it is considered that visibility and audibility of aircraft operations associated with RAF Northolt would not be of significant harm to the living conditions of future occupiers. It is therefore considered that it would be unreasonable to refuse the application on the ground of harm to the residential amenity of the future occupiers, in respect to aircraft noise associated with RAF Northolt. The Ministry of Defence has also confirmed that they do not have any safeguarding concerns with the proposal.

#### **7.05 Impact on the green belt**

Not applicable.

#### **7.07 Impact on the character & appearance of the area**

Policies D3 and D4 of the London Plan (2021) require development proposals to be of a high quality and to enhance the local context by delivering buildings and spaces that positively respond to local distinctiveness.

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all development will be required to be designed to the highest standards and incorporate principles of good design. It should take into account aspects including the scale of the development considering the height, mass and bulk of adjacent structures; building plot sizes and established street patterns; building lines and streetscape rhythm and landscaping.

Policy DMHB 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) re-emphasises the need for new development to be well integrated with the surrounding area and provides design criteria as to how this would be achieved.

Heythrop Drive is characterised by detached properties set back from the highway by front driveways and front gardens. The properties on Heythrop Drive comprise a consistent pattern of three different housing designs, all of which contain hipped roof profiles. Thus, creating a strong degree of uniformity between the properties in terms of their architectural style.

The proposal would involve the erection of four detached dwellings at the site, which would be consistent with the detached housing type on Heythrop Drive. The physical siting of the proposed dwellings would not detract from the street scene. This is because the proposed dwellings would be broadly in alignment with the established front and rear building line of the existing properties on the southern side of Heythrop Drive.

The ground levels on Heythrop Drive increase in gradient from east to west. This change in ground level is reflected in the built form of existing properties on the southern side of

Heythrop Drive which increases in height from east to west. As shown on the submitted street elevation drawing, the ridge height of the proposed dwellings also gently increases in eaves and ridge height from east and west. It is therefore considered that the proposed development responds well to the established roof line. Thus, ensuring that the height of the proposed dwellings appear visually coherent with the neighbouring houses.

The proposed plot width would range between approximately 13.8 metres to 17.5 metres, which is comparable to other properties within the vicinity of the site. Each of the proposed dwellings would be set-in from the side boundaries and would have sufficient areas for rear gardens. It is considered that the proposed dwellings would sit comfortably within their respective plots, and that the established rhythm of the street scene would be retained. Full details in respect to the proposed site levels and finished floor levels of the proposed dwellings would be secured by way of a condition, in the event of an approval. This condition is considered necessary in the interest of visual amenity.

In terms of design, the proposed dwellings have been designed to complement the architectural style of the existing dwellings on Heythrop Drive. In particular, the proposed main hipped roof profiles would match that of the existing properties on Heythrop Drive. In addition to this, incorporation of the front gable projections, side chimney stacks and bay front windows on Plots 1 and 3 would ensure the proposed dwellings are well articulated, whilst appearing in-keeping with the character of the surrounding street scene.

The proposed dwellings would have dormers fitted in their rear roof slope. It is noted that no other properties on Heythrop Drive has yet been extended in this manner. However, this in itself does not necessarily mean that the dormers would be harmful to the visual amenities of the area. The proposed dormers would be modest in size, and set-down from the ridge line, set-in from the sides and set-up from the eaves. It is therefore considered, on balance, that the dormers would appear as sympathetic and subservient features when viewed from the rear gardens of neighbouring properties.

It is acknowledged that there is some distinction between the design of the proposed dwellings. However, this distinction would provide interest across the proposed front elevations, which would complement the housing styles of the existing dwellings on Heythrop Drive. Most notably, the proposed dwelling on Plot 4 would have a double detached garage positioned at a ninety degree angle to its front elevation. However, the siting and design of this proposed double garage would match the double garage located on the opposite bookend of the highway on Heythrop Drive.

Full details of the external finishes and materials would be secured by condition, in the event of an approval. Also, hard and soft landscaping details would be secured by condition. Refer to Section 7.13 of this Committee Report for the planning assessment in respect to trees and landscaping.

In light of the above, it is considered that the siting, scale, height and design of the proposed dwellings would be acceptable on design grounds and that a high quality development would be delivered. The proposal would not cause harm to the character and appearance of the street scene of the surrounding area. The proposal therefore accords with Policies DMHB 11 and DMHB 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policies D3 and D4 of the London Plan (2021) and the NPPF (2021).

## **7.08 Impact on neighbours**

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states:

B) Development proposals should not adversely impact on the amenity, daylight and

sunlight of adjacent properties and open space.

Specifically, paragraph 5.38 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states: "The Council will aim to ensure that there is sufficient privacy for residents and it will resist proposals where there is an unreasonable level of overlooking between habitable rooms of adjacent residential properties, schools or onto private open spaces. A minimum of 21 metres separation distance between windows of habitable rooms will be required to maintain levels of privacy and to prevent the possibility of overlooking. In some locations where there is a significant difference in ground levels between dwellings, a greater separation distance may be necessary."

Paragraph 5.40 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states: "For the purposes of this policy [Policy DMHB 11], outlook is defined as the visual amenity enjoyed by occupants when looking out of their windows or from their garden. The Council will expect new development proposals to carefully consider layout and massing in order to ensure development does not result in an increased sense of enclosure and loss of outlook."

The residential gardens of numbers 1 to 3 Walnut Tree Close adjoin the western boundary of the application site. The flank wall of the proposed dwelling on Plot 4 would be positioned approximately 16.5 metres from the closest rear windows at number 2 Walnut Tree Close. There would be a separation distance of approximately 19 metres from number 1 Walnut Tree Close, and 20 metres from the rear elevation of number 3 Walnut Tree Close.

It is acknowledged that the outlook for the neighbouring occupiers at numbers 1 to 3 Walnut Tree Close would change as a result of the proposed development, particularly for the occupiers at number 2. However, the proposed dwelling on Plot 4 has been carefully designed with a hipped roof profile which would help to reduce the overall bulk and massing of the side elevation facing towards the rear elevations of numbers 1 to 3 Walnut Tree Close. With this in mind, and noting the aforementioned separation distances, it is considered that the loss of outlook for the occupiers at Nos. 1 to 3 Walnut Tree Close would not be so significant as to justify a reasonable ground for refusal on this basis.

A Daylight, Sunlight and Overshadowing Impact Assessment has been submitted in support of this application. The report assesses the impact that 'Plot 4' of the proposed development has on the daylight and sunlight access to the neighbouring properties at numbers 2 and 3 Walnut Tree Close.

The report states that assessments were made of the Vertical Sky Component (VSC) and daylight distribution (DD) to measure daylight access to potentially affected neighbouring properties. The BRE guidelines seek for each window to achieve a VSC of 27% or 0.8 times the existing VSC value. Regarding both 2 and 3 Walnut Tree Close, the results of the VSC analysis demonstrate that all 26 windows achieve a VSC greater than 27% or a relative VSC greater than 0.8 times the existing value. The daylight distribution analysis of the 11 rooms across 2 and 3 Walnut Tree Close demonstrates that the proposed scheme does not affect the no-skyline of any of the assessed rooms.

To assess sunlight, the BRE guidelines state that neighbouring windows within 90° due south should receive Annual Probable Sunlight Hours (APSH) of 25%, including at least 5% during the winter months (WPSH). The Daylight, Sunlight and Overshadowing Impact Assessment states that no assessed windows lie within 90° due south, and therefore were not assessed for sunlight access.

BRE Guidelines state that in relation to amenity spaces, for it to appear adequately sunlit throughout the year, at least half of the area should receive at least 2 hours of sunlight on 21st March. The Daylight, Sunlight and Overshadowing Impact Assessment states that the results of the overshadowing analysis indicate that the 2 assessed amenity spaces meet the BRE guidance. The analysis demonstrates that greater than 89% of the amenity spaces at numbers 2 and 3 Walnut Tree Close will receive at least two hours of sunlight on the 21st March under both the existing and proposed conditions; therefore, the BRE Guidelines are achieved.

In light of the aforementioned results of the Daylight, Sunlight and Overshadowing Impact Assessment, it is considered that the proposed development would not result in an unacceptable loss of daylight, sunlight or overshadowing for the occupiers at numbers 2 and 3 Walnut Tree Close and their associated rear gardens. The rear garden of number 1 Walnut Tree Close would adjoin the boundary where the proposed garage serving Plot 4 would be located. Given the single storey nature of the garage, and noting its modest eaves height at 2.3 metres, it is considered that the proposed development would not result in an unreasonable loss of loss of light or outlook for the occupiers at number 1 Walnut Tree Close.

The northern site boundary adjoins the residential gardens of the bungalows at numbers 5, 6 and 7 Windrush Close. The proposed dwellings would be set-away from this boundary by approximately 16 metres (at the closest point). The closest habitable room to habitable room relationship would be between number 7 Windrush Close and the proposed dwellings at Plots 1 and 2. There would be a separation distance of approximately 22 metres between these properties (taking into account the loft conversion that has been approved at No.7 under application 31527/APP/2021/2352. This separation distance would exceed the recommended '21-metre' guidance prescribed by Policy DMHB 11. It is noted that a 4-metre deep single storey rear extension has also been approved at No. 7 under application 31527/APP/2021/2352. However, the windows of this approved extension would face towards the rear boundary treatment, and would be separated from Plots 1 and 2 by approximately 19 metres.

Taking into account the above factors, and noting the southern aspect of the rear gardens at Nos. 5, 6 and 7 Windrush Close, it is considered that the proposed development would not adversely impact on the living conditions of these neighbouring occupiers, in terms of light, outlook, privacy or sense of enclosure. It is noted that a new access point would run adjacent to the boundary of the rear gardens of Nos. 5, 6 and 7 Windrush Close. However, this access would only serve the four proposed dwellings, and is unlikely to create such a significant level of noise and disturbance as to warrant a refusal on this basis. Besides, the existing highway on Heythrop Drive already runs along the rear boundary of Nos. 1, 3 and 5 Windrush Close.

The proposed front building line of the proposed dwelling on Plot 1 would be set-behind the front building line of number 22 Heythrop Drive. The proposed two-storey rear building line of Plot 1 would be in alignment with the rear building line of number 22. The first floor side (west) window at number 22 appears to be serving a non-habitable room. Taking into account these factors, and noting the 2.4 metre set-in from the shared boundary, it is considered that the proposed two-storey front and rear building lines of Plot 1 would not unduly impact on the living conditions of number 22 Heythrop Drive, in terms of loss of light, outlook, overbearing impact and sense of enclosure.

For guidance purposes, the recommended prescribed depth for single storey rear extensions on detached houses is 4 metres, as set out in Policy DMHD 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020). Whilst the proposed



single storey rear element on Plot 1 does not constitute as an extension per se, given its modest depth of 3.1 metres, it is considered that this aspect of the proposal would not adversely impact on the living conditions of number 22 Heythrop Drive.

There would be a separation distance in excess of 30 metres between the rear elevations of the proposed dwellings and numbers 25 to 31 Warren Road. This separation exceeds the recommended 21-metres prescribed within the supporting text of Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020). It is also noted that this separation distance would be similar to the relationship between the existing properties on the south side of Heythrop Drive and numbers 33-57 Warren Road. It is therefore considered that the proposed development would not adversely impact upon the residential amenities of the neighbouring occupiers on Warren Road, in respect to privacy, light, outlook, overbearing impact and sense of enclosure.

The outlook from the proposed dormers would not be dissimilar to the proposed first floor windows. On this basis, and having regard to the separation distances stated in the preceding paragraph, it is considered that a refusal on the grounds that the dormer windows would give rise to a loss of privacy and overlooking would be unreasonable, and unsustainable in the event of an Appeal.

In the event of an approval, a condition would be secured requiring the proposed first floor side windows on Plots 1 to 4 to be obscure glazed and non-opening up to a height of 1.8 metres from the finished floor level. Subject to such a condition, the proposal would not give rise to a loss of privacy or overlooking issues for the neighbouring occupiers at numbers 22 Heythrop Drive and numbers 2 and 3 Walnut Tree Close.

It is recognised that there would be some potential for increased levels of on-site activity to generate noise and disturbance. However, the site would be used in an exclusively residential capacity. It is therefore considered that the provision of 4 dwellings on an established residential street would not lead to such a significant change in the local noise environment to warrant a refusal on this basis.

The representations received from neighbouring residents have been duly noted. However, having regard to the above, it is considered that the proposal would not cause unreasonable harm to the living conditions of neighbours, in compliance with Policy DMHB 11 part B) of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

#### **7.09 Living conditions for future occupiers**

INTERNAL AMENITY SPACE:

Policy D6 of the London Plan (2021) sets out the requirements for the gross internal floor area of new dwellings at a defined level of occupancy. Table 3.1 of the London Plan (2021) set out the same gross internal area space standards set out in the Technical housing standards - nationally described space standard (2015).

Policy DMHB 15 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that the Council will require all new development to ensure safe and attractive public and private spaces by referring to the Council's latest guidance on Secured by Design principles.

Policy DMHB 16 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all housing development should have an adequate provision of internal space in order to provide an appropriate living environment. The space standards set out in Table 5.1 of the Hillingdon Local Plan: Part 2 - Development Management Policies

(2020) are the same as those found in Table 3.1 of the London Plan (2021).

Paragraph 5.60 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states, "When assessing the size of households the Council will assume that any bedroom over 11.5 sqm is capable of being occupied by two persons. Similarly any "study", "bonus room" or third living/reception room that is not annotated as a bedroom will be assumed to be capable of serving as a bedroom.

Parts 3) and 4) of Policy D6 of the London Plan (2021) require double bedrooms to have a floor area of at least 11.5 square metres, and for single bedrooms to have a floor area of at least 7.5 square metres.

It should be clarified that the proposal is for four x 4-bedroomed dwellings. Nevertheless it is recognised that the proposed attic room within the loft spaces of the proposed dwellings would be in excess of 11.5 square metres, and therefore could be used flexibly as an additional bedroom. It is therefore considered prudent to assess their implications as bedrooms on the quality of living accommodation.

The proposed development would provide the following:

Plot 1- A five bedroom, 9 person occupancy measuring 208 square metres (excluding the garage)

Plot 2- A five bedroom, 9 person occupancy measuring 224 square metres (excluding the garage)

Plot 3- A five bedroom, 9 person occupancy measuring 208 square metres (excluding the garage)

Plot 4- A five bedroom, 9 person occupancy measuring 212 square metres (excluding the garage)

Policy D6 of the London Plan (2021) does not stipulate the minimum requirement for a 5 bedroom 9-person three-storey dwelling, which is the occupancy level of the proposed dwellings (including the attic room within the loft space). However, with each of the proposed dwellings benefitting from a gross internal floor area of at least 208 square metres, their overall internal floor space is considered to be acceptable. It is also noted that the gross internal floor area of the proposed dwellings would far exceed the largest occupancy level rate for a two-storey dwelling referred to in Table 3.1 of the London Plan (2021), i.e. 132 square metres for a 6 bedroom 8-person two storey unit.

All the proposed bedrooms (and attic room) would exceed the minimum floor area standards set out in parts 3) and 4) of Policy D6 of the London Plan (2021). The proposed dwellings would be dual aspect with all the habitable rooms being served by windows in the front elevation or windows and dormer windows in rear elevation of the building. Thus, providing future occupiers with a reasonable level of natural light and outlook. The proposed dwellings would include ground floor front habitable windows which would face towards their respective driveways. Thus, ensuring an appropriate degree of natural surveillance is achieved.

The proposed single storey rear elements would have a depth of approximately 3.1 metres and ridge height of 3.3 metres. Given their modest dimensions, set-in from the shared side boundary and noting the position of the garage on Plot 2, it is considered that the proposed single storey rear elements would not adversely impact on the levels of light and outlook afforded to the future occupiers of the proposed dwellings.

On the above basis, it is considered that the proposed dwellings would provide

satisfactory internal living conditions for future occupants, in accordance with Policy D6 of the London Plan (2021) and Policies DMHB 15 and DMHB 16 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

#### EXTERNAL AMENITY SPACE:

Policy DMHB 18 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all new residential development and conversions will be required to provide good quality and usable private outdoor amenity space. Amenity space should be provided in accordance with the standards set out in Table 5.3, which requires houses with four bedrooms or more should provide a minimum of 100 square metres of private usable amenity space.

The proposed dwellings would be provided with private amenity space in the form of rear gardens ranging between 153 to 302 square metres. Thus, exceeding the minimum private amenity space standards set by Table 5.3 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020). It is considered that the proposed rear gardens would provide ample space and opportunity for general outdoor activity that prospective occupants could reasonably expect with dwellings of the size and type being proposed.

The proposal would provide the future occupiers of the proposed dwellings with external private amenity provision that is of a sufficient size, usability and functionality, in accordance with Policy DMHB 18 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

#### **7.10 Traffic impact, Car/cycle parking, pedestrian safety**

Paragraph 111 of the NPPF (2021) states that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

Policy T4 of the London Plan (2021) states that where appropriate, mitigation, either through direct provision of public transport, walking and cycling facilities and highways improvements or through financial contributions, will be required to address adverse transport impacts that are identified.

Policy T6 of the London Plan (2021) states that new residential development should not exceed the maximum parking standards set out in Table 10.3. All residential car parking spaces must provide infrastructure for electric or Ultra Low Emission vehicles. At least 20 per cent of spaces should have active charging facilities, with passive provision for all remaining spaces.

Policy DMT 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development proposals will be required to meet the transport needs of the development and address its transport impacts in a sustainable manner.

Policy DMT 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals must ensure that safe and efficient vehicular access to the highway network is provided, schemes do not contribute to the deterioration of air quality, noise or local amenity or safety of all road users and residents. Also, that impacts on local amenity and congestion are minimised and there are suitable mitigation measures to address any traffic impacts in terms of capacity and functions of existing roads.

Policy DMT 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires that proposals comply with the Council's parking standards in order to

facilitate sustainable development and address issues relating to congestion and amenity.

#### ACCESS:

The proposed development would be accessed via a new opening from Heythrop Drive (an unadopted highway situated on the eastern flank of the site). The Council's Highways Officer has raised no objection, in principle, to the proposed new opening onto Heythrop Drive which is private in tenure at the proposed connection point. The proposed new access point would be 4.8 metres at its connection point with Heythrop Drive, which would be of a sufficient width to permit two vehicles to safely pass each other. The Council's Highways Officer has commented that the layout and arrangement of the new roadway within the site envelope broadly conforms to the Department for Transport's (DfT) - Manual for Streets (MfS) (circa 2007) best practice for road and parking layouts.

Owing to the proposed turning head within the site envelope, the layout would allow for passenger and refuse/delivery vehicles (including emergency appliances i.e. fire tenders) using the site to enter and leave the site in a forward gear, which is the recommended practice on highway safety grounds. This has been demonstrated through the submitted swept path analysis which is based on movements of the Council's refuse collection vehicle. It is therefore considered that the proposal would not materially harm highway safety, in regards to on-site vehicle turning and manoeuvring.

Notwithstanding the Highway Officer's comments that a Unilateral Undertaking is not required, it is considered necessary that a condition is included to ensure the access from Heythrop Drive is implemented prior to first occupation of the development. Details of the materials and layout of the access, and a timetable for implementation shall be submitted to the LPA for approval prior to commencement. This condition is necessary to make the development acceptable in planning terms, by ensuring acceptable access onto Heythrop Drive would be provided in accordance with Policy DMT 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policies T4, T6 and T6.1 of the London Plan (2021).

#### ON-SITE PARKING PROVISION:

The maximum requirement for a four-bedroom or more dwelling is for up to 2 car parking spaces to be provided on-site to comply with the adopted parking standard set out in Appendix C, Table 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

It is worth noting that Policy T6.1 of the London Plan (2021) states that development comprising 3 plus bed units in outer London with a PTAL rating of 0-1 should provide a maximum of up to 1.5 space per dwelling, which is a slightly lesser requirement than the Council's adopted Car Parking Standards.

In this case, the proposed dwellings on Plots 1 to 3 would each be provided with three on-site car parking spaces in total. This would be in the form of a garage which would accommodate one vehicle and two spaces on the front driveways. The proposed dwelling on Plot 4 would be provided with four car parking spaces in total, i.e. two spaces within the garage and two spaces on the front driveway.

It is acknowledged that the proposed on-site car parking provision would exceed the local and regional car parking standards. However, the Council's Highways Officer has commented that, in this specific case, this excess is considered acceptable. The reason being is that it reduces the potential for untoward parking displacement within and outside

the site envelope, resulting from the higher dependency on the private motor car due to the 'very poor' PTAL rating and absence of on-street parking controls.

In line with Policy T6.1 of the London Plan (2021), there is a requirement for a minimum of 20% 'active' electric vehicle charging points provision within the final parking quantum of development, with all remaining spaces being designated as 'passive' provisions. Although the submitted plans show that electric charging points will be provided, further details are required to ensure that each of the proposed dwellings would be provided with 1 active and 1 passive electric charging point (in compliance with Policy T6.1 of the London Plan (2021)). This matter would be resolved by way of a condition, if planning permission were to be granted.

Accordingly, it is considered that the proposed development would provide a satisfactory number of car parking spaces and that the demand for street parking would not be exacerbated to such an extent that would prejudice highway safety. The proposal would therefore accord with objectives of Policy T6 and T6.1 of the London Plan (2021) and Policy DMT 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), in this regard.

#### VEHICLE TRAFFIC GENERATION:

The Council's Highways Officer has commented that the proposed erection of four dwellings would increase traffic generation from the site. However, the additional traffic generation along Heythrop Drive would be considered minimal and relatively insignificant in highway safety impact terms. Hence, any likely uplift would be considered marginal in traffic generation terms and therefore can be absorbed within the local road network, without notable detriment to traffic congestion and road safety. There is no evidence that increased traffic movements would have an unacceptable impact on highway safety.

The Highway Officer has recommended that a Construction Logistics Plan be secured. It is noted that the applicant has submitted a Construction Management Plan (received on 26.10.22), however this is not considered acceptable in its current form and therefore condition 4 has been recommended in order to secure a revised report which (amongst other matters) would address the Highway Officer's recommendation to secure a Construction Logistics Plan.

#### BICYCLE PROVISION:

Policy DMT 5 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development proposals will be required to ensure that safe, direct and inclusive access for pedestrians and cyclists is provided on the site connecting it to the wider network, including the provision of cycle parking in accordance with Appendix C, Table 1 or, in agreement with the Council.

The submitted plans show that the garages serving the proposed dwellings would contain a wall mounted rack which would accommodate two cycles. The proposed cycle stores would be in a secure, covered and accessible location for future occupants. In the event of an approval, a condition would be secured requiring specific details as to the design and dimensions of the wall mounted racks. Subject to such a condition, the proposal would provide an adequate provision of on-site cycle parking, in compliance with Policy T5 of the London Plan (2021) and Policy DMT 5 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

### **7.11 Urban design, access and security**

Covered in other sections of this Committee Report.

## **7.12 Disabled access**

Policy D5 of the London Plan (2021) seeks to ensure development proposals achieve the highest standards of accessible and inclusive design. Policy D7 of the London Plan (2021) requires for at least 10% of dwellings to meet Building Regulation requirement M4(3) 'wheelchair user dwellings', with all other dwellings meeting Category M4(2) 'accessible and adaptable dwellings'.

During the course of this application, revised plans were submitted to demonstrate wheelchair circulation zones and ground floor W.C. to meet Approved Document M to the Building Regulations (2015 edition). In the event of an approval, conditions would be secured in respect to step-free access details and compliance to ensure the proposed dwellings meet Building Regulation M4(2) and M4(3). Subject to such conditions, the proposed development would comply with Policies D5 and D7 of the London Plan (2021).

## **7.13 Provision of affordable & special needs housing**

Policy H2 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) states that housing provision is expected to include a range of housing to meet the needs of all types of households, and the Council will seek to maximise the delivery of affordable housing from all sites over the period of the Local Plan. For sites with a capacity of 10 or more units, the Council will seek to ensure that the affordable housing mix reflects housing needs in the borough, particularly the need for larger family units. This is supported by Policy DMH 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

The proposal is for less than 10 additional new residential units and therefore does not meet the threshold in order to require affordable housing provision. As such, the proposal is not contrary to Policy H2 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) and Policy DMH 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) in respect of affordable housing provision.

## **7.14 Trees, landscaping and Ecology**

### **TREES AND LANDSCAPING:**

Policy G1 of the London Plan (2021) states that development proposals should incorporate appropriate elements of green infrastructure that are integrated into London's wider green infrastructure network.

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that landscaping and tree planting should enhance amenity, biodiversity and green infrastructure. This is supported by Policy DMHB 14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) which states:

A) All developments will be expected to retain or enhance existing landscaping, trees, biodiversity or other natural features of merit.

B) Development proposals will be required to provide a landscape scheme that includes hard and soft landscaping appropriate to the character of the area, which supports and enhances biodiversity and amenity particularly in areas deficient in green infrastructure.

The site is covered by Tree Preservation Orders 740 and 793. A Tree Survey Report prepared by Patrick Stileman Ltd Arboricultural Consultancy (dated 19th May 2022) accompanies this application. The report identifies 24 individual trees, groups of trees and hedges (1 x Category A, 9 x Category B and 14 x Category C).

The submitted Tree Constraints Plan shows that 7 individual/ groups of trees and 1 hedge would be removed in order to facilitate the proposed development. The Council's Trees Officer has raised no objection due to the loss of these trees and hedges, given their

limited health and visual amenity value. It should be noted that all the Category A and B trees would be retained as part of the proposed development. If planning permission were to be granted, a planning condition would secure the proposed tree protection measures. Due to their siting and separation distance, it is considered that the retained trees would not adversely impact on the living conditions of the future occupiers of the proposed dwellings, in respect to loss of light or overshadowing. As such, there is unlikely to be post-development pressure to lop, top or remove the retained trees by future occupiers in the interest of residential amenity.

The proposal would include soft landscaping to the front of the proposed dwellings, which would help to soften and integrate the development site more sympathetically into its surrounding environment. Furthermore, 13 new trees are being proposed which would help mitigate the loss of the trees identified in the preceding paragraph. A condition would be secured to ensure that the private driveway and side pathways leading to the garden are covered in permeable block paving, which would allow surface water runoff to infiltrate and filter through to the subsoil layer. It is therefore considered that the proposed landscaping would make a positive contribution to the visual amenity of the street scene.

Subject to the above conditions, it is considered that the proposed development would not cause harm to any trees of high visual amenity value and that the proposed soft landscaping would help to soften the overall appearance of the proposed development. The proposal would therefore accord with Policy G1 of the London Plan (2021) and Policies DMHB 11 and DMHB 14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), in this respect.

#### IMPACT ON PROTECTED SPECIES:

Policy G6 of the London Plan (2021) states that development proposals should manage impacts on biodiversity and aim to secure net biodiversity gain. This should be informed by the best available ecological information and addressed from the start of the development process.

Policy DMEI 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that if development is proposed on or near to a site considered to have features of ecological or geological value, applicants must submit appropriate surveys and assessments to demonstrate that the proposed development will not have unacceptable effects. The development must provide a positive contribution to the protection and enhancement of the site or feature of ecological value.

Circular 6/2005: Biodiversity and Geological Conservation- Statutory Obligations and Their Impact Within The Planning System states that where there is a reasonable likelihood of a protected species being present and affected, a survey should be carried out before planning permission is granted. The Circular also advises that surveys should only be required by a planning condition in exceptional circumstances.

An Ecology Statement prepared by AA Environmental Ltd has been provided in support of the application. The Ecology Statement states that "During the walk-over survey, particular attention was paid to record the presence of badgers, bats and herpetofauna (amphibians and reptiles) that may be using the site or present in adjacent habitats". The Statement concludes that there is no evidence of badger or their setts recorded on or adjacent to the site; the existing timber shed on the site and trees provide negligible roosting opportunities for bats; and there were no ponds on the site and therefore no breeding opportunities for amphibians. On this basis, it is considered that the likelihood of protected species being present at the site is low.

Notwithstanding the above, an informative would be secured in the event of an approval advising that should protected species be found at the site, the applicant(s) must fulfil their duties under the Wildlife and Countryside Act 1981.

Also, a condition would be secured requiring an ecological enhancement plan to be submitted to the Council for consideration. Subject to this condition, the proposal would have an acceptable impact on biodiversity interest, in terms of protected species. This would be in compliance with Policy G6 of the London Plan, Policy DMEI 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and the NPPF (2021).

#### **7.15 Sustainable waste management**

Policy DMHB 11 part (d) of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development proposals should make sufficient provision for well designed internal and external storage space for general, recycling and organic waste, with suitable access for collection. External bins should be located and screened to avoid nuisance and adverse visual impacts to occupiers and neighbours.

The Council's refuse collection vehicle measures approximately 10.4 metres in length and 2.5 metres in width (as confirmed by the Council's Highways Officer). A swept path analysis has been submitted which adequately demonstrates that the Council's refuse collection vehicle of the length required could enter, turn and exit the site in forward gear. The Council's Highways Officer is therefore satisfied that the refuse collection vehicles would be able to satisfactorily service the proposed dwellings without leading to a loss of highway safety.

The Council's 'waste collection' maximum distance collection parameter is 10 metres, i.e., the distance from a refuse vehicle to the point of collection. The submitted plans indicate that the refuse/recycling stores would be provided adjacent to the flank wall of the proposed dwellings. Although the location would marginally fall outside of the '10 metre collection parameter', it is anticipated that future occupiers of the proposed dwellings would move their rubbish to the kerbside on collection days. This arrangement would be no different to how the existing properties on Heythrop Drive are currently being serviced by the Council's refuse collection crew. In the event of an approval, full details of the refuse/recycling stores (including their dimensions, external finishes and design) would be secured by condition.

Subject to the above condition, it is considered that the proposed development would be able to provide a convenient location for refuse and recycling facilities in accordance with Policy DMHB 11 part (d) of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

#### **7.16 Renewable energy / Sustainability**

Policy SI 2 of the London Plan (2021) states residential development should achieve at least a 10% improvement beyond Building Regulations 2013.

Policy DMEI 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires all developments to make the fullest contribution to minimising carbon dioxide emissions in accordance with the London Plan targets.

The application was accompanied by an Energy and Sustainability Statement. Whilst the report outlines measures that would be used such as air source heat pumps and double glazed windows, no calculations have been provided to demonstrate that the proposed dwellings would achieve at least a 10% improvement beyond Building Regulations 2013. This matter would be solved by the imposition of a planning condition, in the event of an approval. Also, a condition would be secured requiring the proposed dwellings to achieve as a minimum, a water efficiency standard of no more than 105 litres per person per day



maximum water consumption.

Subject to the above conditions, the proposal would be compliant with Policies DMEI 2 and DMEI 10 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Policy SI 2 of the London Plan (2021).

#### **7.17 Flooding or Drainage Issues**

Policy SI 12 of the London Plan (2021) states that development proposals should ensure that flood risk is minimised and mitigated, and that residual risk is addressed. Policy SI 13 of the London Plan (2021) states that development proposals should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible.

Policy DMEI 9 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals that fail to make appropriate provision for flood risk mitigation, or which would increase the risk or consequences of flooding, will be refused.

Policy DMEI 10 states that development within areas identified at risk from surface water flooding which fail to make adequate provision for the control and reduction of surface water runoff rates will be refused.

The site lies within Flood Zone 1 of the Environment Agency's Flood Risk Map. This means the site is classified as being at low risk and defined as having a less than 1 in 1,000 probability of fluvial and tidal flooding. As such, there are no restrictions on development, including more vulnerable uses such as Use Class C3 (dwellinghouses), in this location, in terms of fluvial and tidal flood risk.

In the event of an approval, a condition would be secured requiring the submission of a sustainable water management scheme, that incorporates sustainable urban drainage systems (SuDs), to be submitted to the Council for consideration. Also, the landscaping condition has been worded in such a manner to ensure that permeable hard surfacing is used for the parking areas serving the proposed dwellings.

Subject to such conditions, it is considered that the proposed development would not increase the risk of flooding on the site or elsewhere in accordance with Policies DMEI 9 and DMEI 10 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Policies SI 12 and SI 13 of the London Plan (2021).

#### **7.18 Noise or Air Quality Issues**

NOISE:

Policy D14 of the London Plan (2021) states that in order to reduce, manage and mitigate noise to improve health and quality of life, residential and non-aviation development proposals should manage noise by avoiding significant adverse noise impacts on health and quality of life.

The site would be used in an exclusively residential capacity. As such, in terms of the operational phase of the proposed development, no significant issues are considered to be raised by the proposal, in respect to noise.

Notwithstanding the above, in respect of the construction phase, Planning Officers are of the view that the Construction Management Plan received on 26th October 2022 is not satisfactory. The construction hours stated in the report exceed those stipulated under the Control of Pollution Act 1974. Also, the Construction Management Plan lacks detail in respect to the phasing of development, traffic management and measures to minimise dust and air pollution. This being said, Planning Officers are fully satisfied that these

matters could be resolved through the imposition of a pre-commencement condition (Condition 4) requiring an updated Construction Management Plan to be submitted to the Council for consideration.

#### AIR QUALITY:

Policy DMEI 14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals should demonstrate appropriate reductions in emissions. It adds that, development proposals should, as a minimum:

- i) be at least "air quality neutral";
- ii) include sufficient mitigation to ensure there is no unacceptable risk from air pollution to sensitive receptors, both existing and new; and
- iii) actively contribute towards the improvement of air quality, especially within the Air Quality Management Area.

The site is designated within an Air Quality Management Area. In the event of an approval, a condition would be secured requiring the submission of an Air Quality Management Assessment to demonstrate that the proposed development would be at least air quality neutral. Also, a condition would be secured requiring the submission of a Construction Management Plan (as noted above) to minimise air and other emissions caused during the construction phase. In light of these conditions, and noting the minor scale of the proposal, it is considered that it would not be necessary, fair or reasonable to require an air quality contribution to be secured through a legal agreement.

#### **7.19 Comments on Public Consultations**

Covered in other sections of this Committee Report.

#### **7.20 Planning obligations**

Policy DMCI 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that to ensure development is sustainable, planning permission will only be granted for development that clearly demonstrates there will be sufficient infrastructure of all types to support it. Infrastructure requirements will be predominantly addressed through the Council's Community Infrastructure Levy (CIL).

The Council adopted its own Community Infrastructure Levy (CIL) on 1st August 2014. The Hillingdon CIL charge for residential developments is £95 per square metre of additional floor space. This is in addition to the Mayoral CIL charge of £60 per square metre. CIL rates are index linked. The proposal involves the erection of new dwellings and is therefore CIL liable if planning permission were to be granted.

The Council's Highways Department has confirmed that the existing section of Heythrop Drive that would abut the newly proposed access road is private. There is, therefore, no requirement for the applicants to enter into a Unilateral Undertaking for the carrying out of highways works.

#### **7.21 Expediency of enforcement action**

Not applicable.

#### **7.22 Other Issues**

##### OTHER MATTERS:

In the event of an approval, a condition would be secured removing permitted development rights from the site to ensure that garages, sheds or other outbuildings, nor extensions or roof alterations to the proposed dwellings at the site are erected without the written consent of the Council. This is considered to be necessary in the interest of safeguarding the character and appearance of the area and amenity of residential occupiers.

## **8. Observations of the Borough Solicitor**

### General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in *Probity in Planning, 2009*.

### Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

### Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

### Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

## **9. Observations of the Director of Finance**

Not applicable.

## **10. CONCLUSION**

For the reasons set out in this Committee Report, it is considered that the proposal would comply with the objectives of national, regional and local planning policies and guidance. It is therefore recommended that the application be approved, subject to the imposition of the conditions set out in Section 2 of the Committee Report.

## **11. Reference Documents**

National Planning Policy Framework (July 2021)

The London Plan (March 2021)

Hillingdon Local Plan: Part One - Strategic Policies (November 2012)

Hillingdon Local Plan: Part Two - Development Management Policies (January 2020)

Hillingdon Local Plan Accessible Hillingdon Supplementary Planning Document  
(September 2017)

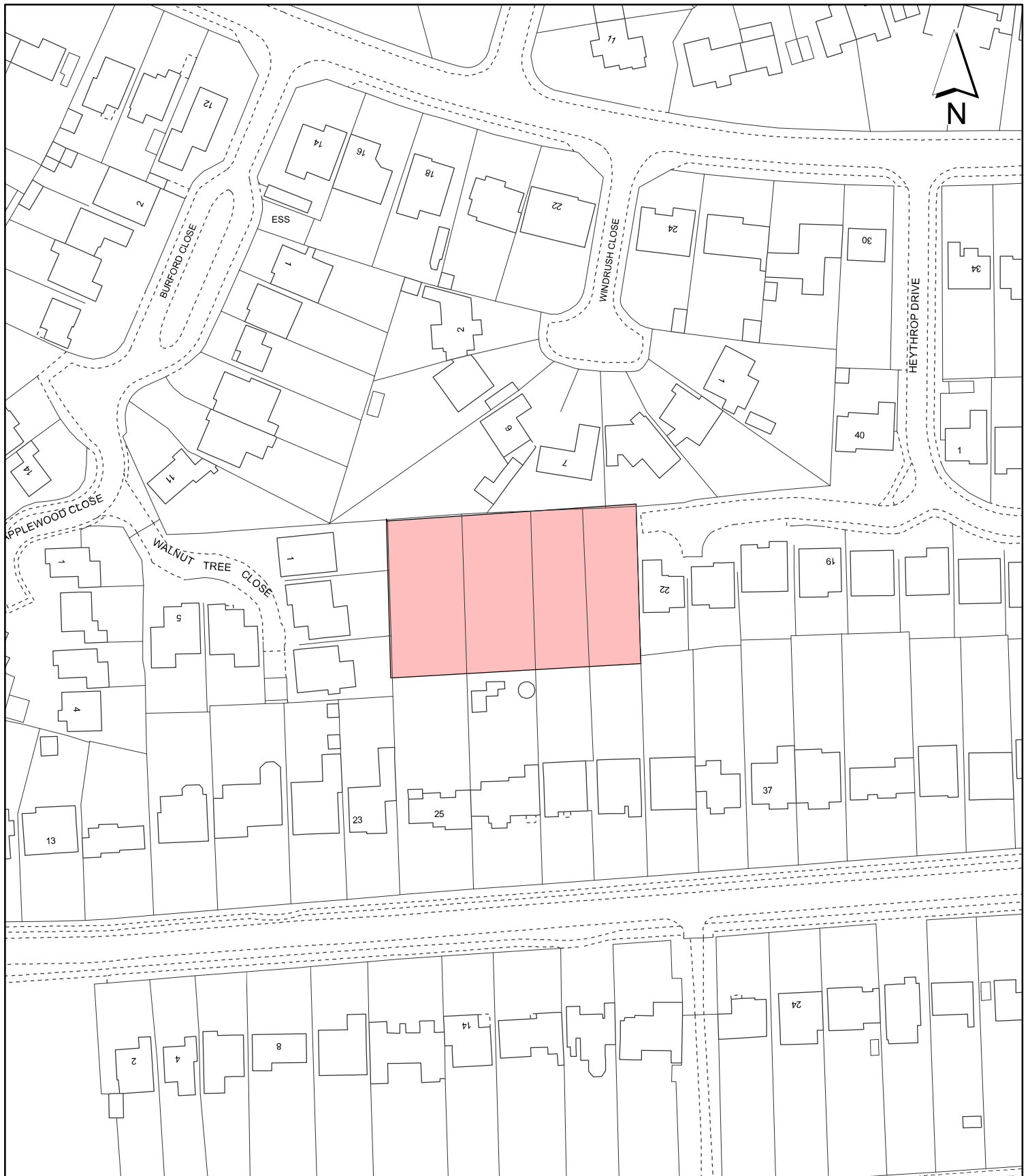
Planning Obligations Supplementary Planning Document (July 2014)

Technical Housing Standards - Nationally Described Space Standard (March 2015)

Government Circular: Biodiversity and Geological Conservation- Statutory Obligations and their Planning System (2005)

**Contact Officer:** Nesha Burnham

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

For identification purposes only.

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Site Address:

**R/O 25-31 Warren Road**

**LONDON BOROUGH OF HILLINGDON**  
Residents Services  
Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**77265/APP/2022/2845**

Scale:

**1:1,250**

Planning Committee:

**BoroughPage 85**

Date:

**December 2022**



**HILLINGDON**  
LONDON

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**Item No.**            **Report of the Interim Director of Planning, Regeneration & Public Realm**

**Address**            LAND AT LONGFORD CLOSE LONGFORD GARDEN HAYES

**Development:**    Erection of a two storey dwelling comprising two 1-bedroom flats

**LBH Ref Nos:**     77102/APP/2022/647

**Drawing Nos:**    P-LG-04  
                          FP-LC-01  
                          FP-LC-02  
                          FP-LC-03  
                          Design and Access Statement

**Date Plans Recieved:**    25/02/2022                    **Date(s) of Amendment(s):**

**Date Application Valid:**    22/03/2022

**1.     SUMMARY**

Planning permission is sought for the erection of a two storey building comprising two x 1 bedroom flats. The development also includes associated parking and amenity space.

There is important planning history which informs this recommendation to grant permission which carries weight in this decision.

Firstly, a historic permission in 2009 granted outline planning permission for a two storey building comprising two x 1 bedroom flats at the site. This application was originally refused by the Local Planning Authority and subsequently appealed. At appeal, permission was granted by the planning inspectorate (their reference APP/R5510/A/08/2092553). The previously approved development is similar to this proposal.

This 2009 permission expired without being implemented, However, a second application was granted in 2018 for 'Erection of two storey building comprising two one-bedroom flats'. Although the permission has now expired (in 2021), this history remains a significant material consideration in the determination of this application.

The current proposal being considered by Members differs from the 2018 permission in that the building is approximately 1m2 larger in footprint and 50cm higher overall. Minor fenestration alterations are also proposed alongside additional cycle storage provisions. The scheme remains directly comparable to the expired 2018 permission.

When taking into consideration that the principle of converting the site for residential purposes has been established by two previous planning permissions, that the proposal is very similar to the previously approved scheme (62949/APP/2018/3166), that the scheme would provide two additional residential units on vacant land and that some economic benefits would also arise, the application is considered acceptable, on balance.

**2.     RECOMMENDATION**

**APPROVAL subject to the following:**

**1            RES3            Time Limit**

The development hereby permitted shall be begun before the expiration of three years

from the date of this permission.

#### REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

### **2 RES4 Accordance with Approved Plans**

The development hereby permitted shall not be carried out except in complete accordance with the details shown on submitted plans numbers FP-LC-01, FP-LC-02, FP-LC-03, P-LG-04 and shall thereafter be retained/maintained for as long as the development remains in existence.

#### REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan Part 1 (2012), Part 2 (2020) and the London Plan (2021).

### **3 HO4 Materials**

The materials and finishes to be used in the construction of the external surfaces of the development hereby permitted shall be as specified on plan number FP-LC-03 and shall thereafter be retained as such.

#### REASON

To safeguard the visual amenities of the area, to accord with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

### **4 OM19 Construction Management Plan**

Prior to development commencing, the applicant shall submit a demolition and construction management plan to the Local Planning Authority for its approval in writing. The plan shall detail:

- (i) The phasing of development works
- (ii) The hours during which development works will occur (please refer to informative I15 for maximum permitted working hours)
- (iii) Measures to prevent mud and dirt tracking onto footways and adjoining roads (including wheel washing facilities)
- (iv) Traffic management and access arrangements (vehicular and pedestrian) and parking provisions for contractors during the development process (including measures to reduce the numbers of construction vehicles accessing the site during peak hours)
- (v) Measures to reduce the impact of the development on local air quality and dust through minimising emissions throughout the demolition and construction process
- (vi) The location of where demolition/construction materials will be sited throughout the construction phase.

The approved details shall be implemented and maintained throughout the duration of the demolition and construction process.

#### REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and to minimise disturbance to the local highways network and neighbouring amenities in compliance with policies DMHB 11, DMT 1 and DMT 2 of the Hillingdon Local Plan Part 2 (2020), London Plan Policy T4 and the NPPF (2021).

### **5 HO6 Obscure Glazing**

All first floor side and rear elevation windows in the proposed development (north, east and west facing) shall be glazed with permanently obscured glass to at least scale 4 on the Pilkington scale and be non-opening below a height of 1.8 metres taken from internal



finished floor level for so long as the development remains in existence.

**REASON**

To prevent overlooking to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**6 H7 Parking Arrangements (Residential)**

The vehicle parking areas shown on the approved plans, shall be constructed, designated and allocated for the sole use of the occupants prior to the occupation of the development and thereafter be permanently retained and used for no other purpose.

**REASON**

To ensure that an appropriate level of car parking provision is provided on site in accordance with policies DMT 1, DMT 2, DMT 6 of the Hillingdon Local Plan Part 2 (2020), London Plan Policy T6.1 and the NPPF (2021).

**7 RES6 Levels**

Save for demolition and site clearance works, no above ground works shall take place until plans of the site showing the existing and proposed ground levels and the proposed finished floor levels of all proposed buildings have been submitted to and approved in writing by the Local Planning Authority. Such levels shall be shown in relation to a fixed and known datum point. Thereafter, the development shall not be carried out other than in accordance with the approved details.

**REASON**

To ensure that the development relates satisfactorily to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**8 RES12 No additional windows or doors**

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no additional windows, doors or other openings shall be constructed in the walls or roof slopes of the development hereby approved.

**REASON**

To prevent overlooking to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**9 NONSC Non Standard Condition**

Prior to commencement of the hereby approved development, (excluding demolition and site clearance) a scheme for the provision of sustainable water management shall be submitted to, and approved in writing by the Local Planning Authority. The scheme shall clearly demonstrate how the approved development will incorporate sustainable urban drainage (SuDs) in accordance with the hierarchy set out in The London Plan and will:

- i. provide information on all SuDs features including the method employed to delay and control the surface water discharged from the site and:
- ii. provide a management and maintenance plan for the lifetime of the development.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:

- iii. provide details of water collection facilities to capture excess rainwater; and how water usage will be reduced in the development.

Thereafter, the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

#### REASON

To ensure that surface water run off is controlled to ensure the development does not increase the risk of flooding and is to be handled as close to its source as possible and Conserve water supplies in compliance with: Hillingdon Local Plan: Part 1 - Strategic Policies - Policy EM 6 Flood Risk Management, Hillingdon Local Plan Part 2 Development Management Policies - Policy DMEI 10 Water Management, Efficiency and Quality, and guidance contained within the London Plan (2021) and NPPF (2021).

#### **10 RES9 Landscaping (car parking & refuse/cycle storage)**

No development shall take place until a landscape scheme has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include: -

1. Details of Soft Landscaping
  - 1.a Planting plans (at not less than a scale of 1:100)
  - 1.b Written specification of planting and cultivation works to be undertaken
  - 1.c Schedule of plants giving species, plant sizes, and proposed numbers/densities where appropriate
  
2. Details of Hard Landscaping
  - 2.a Refuse Storage (including dimensions and external finishes of the storage facilities which shall be secure and enclosed)
  - 2.b Cycle Storage (including dimensions and external finishes of the storage facilities)
  - 2.c Means of enclosure/boundary treatments
  - 2.d Car Parking Layouts (including the provision of the requisite electrical charging points - 1 active and 1 passive)
  - 2.e Hard Surfacing Materials
  - 2.f External Lighting
  - 2.g Other structures (such as play equipment and furniture)
  
3. Proposed functional services above and below ground.

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

#### REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality, protect neighbouring amenities and provide adequate facilities in compliance with policies DMHB 11, DMHB 12, DMHB 14, DMEI 1 and DMT 2 of the Hillingdon Local Plan Part 2 (2020) and Policy G5 of the London Plan (2021).

#### **11 RES14 Outbuildings, extensions and roof alterations**

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any order revoking and re-enacting that Order with or without modification); no garage(s), shed(s) or other outbuilding(s), nor extension or roof alteration to any dwellinghouse(s) subject of this permission shall be erected without the grant of further specific permission from the Local Planning Authority.

#### REASON

To protect the character and appearance of the area and amenity of residential occupiers in accordance with policies DMHB 11 and DMHD 2 of the Hillingdon Local Plan Part 2 (2020).

**12 NONSC Non Standard Condition**

The ground floor dwelling must be constructed to meet the standards for a Category 2, M4(2) dwelling, as set out in Approved Document M to the Building Regulations (2010) 2015.

**REASON**

To ensure that an appropriate standard of housing stock, in accordance with London Plan policy D6, is achieved.

**INFORMATIVES**

**1 I52 Compulsory Informative (1)**

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

**2 I53 Compulsory Informative (2)**

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including The London Plan and national guidance.

DMCI 7	Planning Obligations and Community Infrastructure Levy
DMEI 2	Reducing Carbon Emissions
DMEI 7	Biodiversity Protection and Enhancement
DMEI 9	Management of Flood Risk
DMEI 10	Water Management, Efficiency and Quality
DMEI 11	Protection of Ground Water Resources
DMEI 12	Development of Land Affected by Contamination
DMH 2	Housing Mix
DMHB 11	Design of New Development
DMHB 12	Streets and Public Realm
DMHB 14	Trees and Landscaping
DMHB 15	Planning for Safer Places
DMHB 16	Housing Standards
DMHB 17	Residential Density
DMHB 18	Private Outdoor Amenity Space
DMT 1	Managing Transport Impacts
DMT 2	Highways Impacts
DMT 5	Pedestrians and Cyclists
DMT 6	Vehicle Parking
LPP D1	(2021) London's form, character and capacity for growth
LPP D3	(2021) Optimising site capacity through the design-led approach
LPP D4	(2021) Delivering good design
LPP D5	(2021) Inclusive design
LPP D6	(2021) Housing quality and standards
LPP D7	(2021) Accessible housing
LPP G5	(2021) Urban greening
LPP G6	(2021) Biodiversity and access to nature
LPP H1	(2021) Increasing housing supply
LPP H2	(2021) Small sites

LPP SI13	(2021) Sustainable drainage
LPP SI2	(2021) Minimising greenhouse gas emissions
LPP T5	(2021) Cycling
LPP T6	(2021) Car parking
LPP T6.1	(2021) Residential parking
NPPF2	NPPF 2021 - Achieving sustainable development
NPPF4	NPPF 2021 - Decision-Making
NPPF5	NPPF 2021 - Delivering a sufficient supply of homes
NPPF11	NPPF 2021 - Making effective use of land
NPPF12	NPPF 2021 - Achieving well-designed places

### **3 115 Control of Environmental Nuisance from Construction Work**

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance 'The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit ([www.hillingdon.gov.uk/noise](http://www.hillingdon.gov.uk/noise) Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

### **4 147 Damage to Verge - For Council Roads:**

The Council will recover from the applicant the cost of highway and footway repairs, including damage to grass verges.

Care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense.

For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

For Private Roads: Care should be taken during the building works hereby approved to ensure no damage occurs to the verge of footpaths on private roads during construction. Vehicles delivering materials to this development shall not override or cause damage to a private road and where possible alternative routes should be taken to avoid private roads. The applicant may be required to make good any damage caused.

## **5 IT05 Wildlife and Countryside Act 1981**

Wildlife and Countryside Act 1981: Note that it is an offence under this act to disturb roosting bats, nesting birds or any other protected species. Therefore, if applicable, it is advisable to consult your tree surgeon / consultant to agree an acceptable time for carrying out the approved works.

## **6 I73 Community Infrastructure Levy (CIL) (Granting Consent)**

Under the terms of the Planning Act 2008 (as amended) and Community Infrastructure Levy Regulations 2010 (as amended), this development is liable to pay the London Borough of Hillingdon Community Infrastructure Levy (CIL) and the Mayor of London's Community Infrastructure Levy (CIL). This will be calculated in accordance with the London Borough of Hillingdon CIL Charging Schedule 2014 and the Mayor of London's CIL Charging Schedule 2012. Before commencement of works the development parties must notify the London Borough of Hillingdon of the commencement date for the construction works (by submitting a Commencement Notice) and assume liability to pay CIL (by submitting an Assumption of Liability Notice) to the Council at [planning@hillingdon.gov.uk](mailto:planning@hillingdon.gov.uk). The Council will then issue a Demand Notice setting out the date and the amount of CIL that is payable. Failure to submit a valid Assumption of Liability Notice and Commencement Notice prior to commencement of the development may result in surcharges being imposed.

The above forms can be found on the planning portal at: [www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil](http://www.planningportal.gov.uk/planning/applications/howtoapply/whattosubmit/cil)

Pre-Commencement Conditions: These conditions are important from a CIL liability perspective as a scheme will not become CIL liable until all of the pre-commencement conditions have been discharged/complied with.

### **3. CONSIDERATIONS**

#### **3.1 Site and Locality**

The development site comprises a parcel of land located to the rear of No's 1, 3, 5 and 7 Longford Gardens. The land is currently overgrown and void of development. The area surrounding the site is residential.

To the south and west of the site are a row of two storey terraces making up No's 60 to 92 Uxbridge Road. To the north of site are properties 1 to 4 Longford Close. Those properties are flats contained within two detached buildings.

The site lies within the Hillingdon Air Quality Management Area. It has a Public Transport Accessibility Level (PTAL) of 2 and part of it lies within a contaminated land buffer.

#### **3.2 Proposed Scheme**

Planning permission is sought for the erection of a two storey building comprising two x 1 bedroom flats. The development also includes associated parking and amenity space.

#### **3.3 Relevant Planning History**

62949/APP/2007/997 Land At Longford Close Longford Gardens Hayes  
ERECTION OF A TWO STOREY BUILDING CONTAINING 2, ONE-BEDROOM MAISONETTE FLATS, WITH 3 PARKING SPACES AND FRONTAGE BIN STORE (OUTLINE APPLICATION)

**Decision:** 09-07-2007 Refused

62949/APP/2008/2087 Land At Longford Close Longford Gardens Hayes  
ERECTION OF A TWO STOREY BUILDING COMPRISING 2 ONE-BEDROOM FLATS WITH 3 PARKING SPACES (OUTLINE APPLICATION FOR APPROVAL OF ACCESS, LAYOUT AND SCALE).

**Decision:** 11-11-2008 Refused **Appeal:** 28-04-2009 Allowed

62949/APP/2009/1308 Land At Longford Close Longford Gardens Hayes  
Erection of a two-storey detached building comprising 2 one-bedroom flats with associated parking.

**Decision:** 16-09-2009 Approved

62949/APP/2018/3166 Land At Longford Close Longford Gardens Hayes  
Erection of two storey building comprising two one-bedroom flats

**Decision:** 12-12-2018 Approved

#### **Comment on Relevant Planning History**

The site's planning history is set out in section 3.3 of this report (above).

In 2018, planning permission was granted for a comparable development at the site (62949/APP/2018/3166). Although that permission has now expired, it is a significant material consideration in the determination of this application which carries significant weight.

This proposal differs from the previous permission in that the building is approximately 1m<sup>2</sup> larger in footprint and 50cm bigger in height. Minor fenestration alterations are also proposed alongside additional cycle storage provisions.

It is also worth noting that the principle of developing the site for residential purposes was established in 2009. When outline planning permission was granted for 'the erection of two one-bedroom flats' at appeal (62949/APP/2008/2087, APP/R5510/A/08/2092553).

#### **4. Planning Policies and Standards**

Planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

The Development Plan for the London Borough of Hillingdon currently consists of the following documents:

The Local Plan: Part 1 - Strategic Policies (2012)  
The Local Plan Part 2 - Development Management Policies (2020)  
The Local Plan Part 2 - Site Allocations and Designations (2020)  
The West London Waste Plan (2015)  
The London Plan (2021)

The National Planning Policy Framework (NPPF) (2021) is also a material consideration in planning decisions, as well as relevant supplementary planning documents and guidance.

## Local Plan Designation and London Plan

The following Local Plan Policies are considered relevant to the application:-

### Part 1 Policies:

- PT1.BE1 (2012) Built Environment
- PT1.H1 (2012) Housing Growth

### Part 2 Policies:

- DMCI 7 Planning Obligations and Community Infrastructure Levy
- DMEI 2 Reducing Carbon Emissions
- DMEI 7 Biodiversity Protection and Enhancement
- DMEI 9 Management of Flood Risk
- DMEI 10 Water Management, Efficiency and Quality
- DMEI 11 Protection of Ground Water Resources
- DMEI 12 Development of Land Affected by Contamination
- DMH 2 Housing Mix
- DMHB 11 Design of New Development
- DMHB 12 Streets and Public Realm
- DMHB 14 Trees and Landscaping
- DMHB 15 Planning for Safer Places
- DMHB 16 Housing Standards
- DMHB 17 Residential Density
- DMHB 18 Private Outdoor Amenity Space
- DMT 1 Managing Transport Impacts
- DMT 2 Highways Impacts
- DMT 5 Pedestrians and Cyclists
- DMT 6 Vehicle Parking
- LPP D1 (2021) London's form, character and capacity for growth
- LPP D3 (2021) Optimising site capacity through the design-led approach
- LPP D4 (2021) Delivering good design
- LPP D5 (2021) Inclusive design
- LPP D6 (2021) Housing quality and standards
- LPP D7 (2021) Accessible housing
- LPP G5 (2021) Urban greening
- LPP G6 (2021) Biodiversity and access to nature
- LPP H1 (2021) Increasing housing supply
- LPP H2 (2021) Small sites
- LPP SI13 (2021) Sustainable drainage
- LPP SI2 (2021) Minimising greenhouse gas emissions
- LPP T5 (2021) Cycling
- LPP T6 (2021) Car parking
- LPP T6.1 (2021) Residential parking

NPPF2	NPPF 2021 - Achieving sustainable development
NPPF4	NPPF 2021 - Decision-Making
NPPF5	NPPF 2021 - Delivering a sufficient supply of homes
NPPF11	NPPF 2021 - Making effective use of land
NPPF12	NPPF 2021 - Achieving well-designed places

## **5. Advertisement and Site Notice**

**5.1** Advertisement Expiry Date:- Not applicable

**5.2** Site Notice Expiry Date:- Not applicable

## **6. Consultations**

### **External Consultees**

30 neighbouring properties were directly notified of the proposal on 24/03/22. In response to the public consultation, 3 letters of objection were received against the proposed development.

The objections raised have been summarised below.

- 1) The proposal would be an eyesore and it would be out of keeping with the character of the area, reducing openness and disrupting symmetry;
- 2) The proposal would be sited very close to neighbours and would have an adverse impact on the amenities of neighbours due to light and privacy loss, as well as noise intensification;
- 3) The proposal would have an adverse impact on the local highways network and pedestrian safety;
- 4) The proposal would block/interrupt emergency service routes to the area;
- 5) The proposal would result in increased pollution and poor air quality;
- 6) The position of the proposed bin storage will attract rodents and cause harmful smells during the summer;
- 7) The proposal would increase the likelihood of antisocial behaviour in the area;
- 8) The development would cause sewerage issues;
- 9) Concerns regarding the impact on the security of neighbours.

In addition to those objections, a petition with 29 signatures has been received against the proposed development. The petition lists a number of key concerns which align with those listed in 1-9, above:

- impact on openness and symmetry of the area;
- privacy;
- security;
- loss of light;
- access for emergency vehicles restricted;
- anti-social activities;
- traffic and parking;
- on going sewage problems;
- development is an eyesore;
- harmful impact on the quality of life in and around Longford Close.

Planning Officer Response:

Issues relating to points 1-5 are considered in the main body of this report.

In response to point 6, a condition has been recommended for inclusion on the decision notice



requiring that the proposed bin storage area be secure and enclosed. This would minimise the chances of bins being stored out in the open outside of collection days.

In response to point 7 and point 9, there is no guarantee that the proposal would increase anti social behaviour or pose a security risk in the area. Two additional homes would provide increased natural surveillance than the existing, vacant plot.

In response to point 8, Building Regulations are in place to ensure that the proposal has no undue adverse impact on the area's utility infrastructure. In addition a condition has been recommended for inclusion on the decision notice to ensure the sites drainage impacts are appropriately managed.

### **Internal Consultees**

#### **ACCESS OFFICER:**

Any grant of planning permission should include the following condition:

The ground floor dwelling must be constructed to meet the standards for a Category 2, M4(2) dwelling, as set out in Approved Document M to the Building Regulations (2010) 2015 REASON: To ensure that an appropriate standard of housing stock, in accordance with London Plan policy 3.8(c), is achieved and maintained.

#### **CONTAMINATION:**

No objection/ no comments.

#### **HIGHWAYS OFFICER:**

The proposed scheme is to construct 2 x 1 bed flats (use class C3) with two car park spaces and cycle spaces 4nos located at the rear of the site. The site has a PTAL rating of 2.

The site achieves a poor PTAL rating of 2.

The Mayor of London adopted a new and revised London Plan in March 2021, consequently the car parking standards set out in the London Plan 2021 which are restricted in line with levels of existing and future public transport accessibility and connectivity, take precedence over those of the Local Development Plans of Local Authorities except where local Development Plans specify lower local maximum standards.

The maximum residential car parking standards set out in the London Plan Policy T6: Table 10.3 are predominately lower than the maximum parking standards set out in the Hillingdon Local Plan: Part 2 Development Management Policies (2020) and so the London Plan parking standards take precedent.

Whilst the provision of car and cycle parking complies with the parking standards however the layouts are substandard, in accordance with MfS/MfS2 the bays should have the required 6m aisle/width area in front to allow vehicles to manoeuvre in and out of the bays, scaling off the basic plan submitted the area in front is approx. 4m only. Nevertheless, MfS states the following:

Where space is limited it may not be possible to provide for vehicles to get into the spaces in one movement. Some back and fore manoeuvring may be required. This is likely to be acceptable where traffic volumes and speeds are low. Given the parking is at the end of the dead end road such a limited width is acceptable, also for this reason pedestrian visibility splays are not required.

Also, in line with The Hillingdon Local Plan: Part 2 Development Management Policies (2020) Policy DMT 6: there is also a requirement for an Electric Vehicle Charging Point (EVCP) provision for the new dwelling. The HA believes that there is sufficient scope for the applicant to satisfactorily address the EV requirement and therefore could be conditioned.

## RECOMMENDATION:

No objection subject to the following conditions:

### Conditions:

Before the occupation of the proposed development, EV charging points in accordance with the London Plan shall be submitted to and approved in writing by the Local Planning Authority.

The car parking layout and EV charging points and cycle storage shall be constructed and laid out in accordance with the agreed details and completed prior to occupation of the development hereby approved and shall thereafter be retained, maintained, and kept available for the occupants of the development at all times.

### Planning Officer Response:

The above comments are noted. All comments received have been taken into consideration in the main body of the report. The matters suggested by the Council's Highway and Access Officers for inclusion as conditions, have been included in the proposed conditions set out in Section 2 of this report.

## **7. MAIN PLANNING ISSUES**

### **7.01 The principle of the development**

#### PRINCIPLE OF DEVELOPMENT:

The principle of developing the site for residential purposes has previously been established by planning permissions:  
(62949/APP/2018/3166 and 62949/APP/2008/2087/APP/R5510/A/08/2092553).

In the 2009 appeal the Inspector's decision noted:

"The proposal site is an unused and undeveloped parcel of land between the back gardens of 1-7 (odd numbers) Longford Gardens and Longford Close which is a private access road serving 1-4 Longford Close. The site is currently in poor condition and suffers from fly tipping, is vegetated with mixed scrub, and is of an uneven and untended character."

Policy DMH 6 relates to "Garden and Backland Development". This policy states that there is a presumption against the loss of gardens due to the need to maintain local character, amenity space and biodiversity. Given the site remains in an untended and overgrown state, it is not considered garden land for the purposes of Policy DMH 6. The site can however, be regarded as 'backland development' and this is a relevant consideration. However, this backland proposal has been established by previous permissions, noted above, and is therefore considered acceptable 'in principle'.

#### HOUSING MIX:

Policy H10 of the London Plan (2021) states that schemes should generally consist of a range of unit sizes and sets out a number of factors which should be considered when determining the appropriate housing mix on a particular scheme. This includes local evidence of need.

Policy DMH 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires the provision of a mix of housing units of different sizes in schemes of residential development to reflect the Council's latest information on housing need. Paragraph 4.6 outlines that there is a substantial borough-wide requirement for larger affordable and private market units, particularly three-bedroom properties or family

homes.

Family housing is defined within the glossary of the London Plan (2021) which outlines it must generally be of a size that has three or more bedrooms.

The proposed development includes 2 x 1 bedroom flats and would therefore fail to provide a housing mix which contributes to the Borough's family housing supply. Notwithstanding this point, planning permission has been granted for other schemes at the site which comprise 2 x 1 bedroom flats (62949/APP/2018/3166 and 62949/APP/2008/2087/APP/R5510/A/08/2092553). This is a significant material consideration which weighs in favour of the proposed development and therefore the proposed housing mix is considered to be acceptable in this particular circumstance.

It is also relevant to note that the site is quite constrained and the amount of private amenity space that can be provided is limited by its position, siting and arrangement. Therefore it is better suited to providing 1-bed flats which would likely attract a different owner/occupier for which the garden space provided would be sufficient in terms of quality and quantity.

#### **7.02 Density of the proposed development**

Policy D3 of the London Plan (2021) states that all development must make the best use of land by following a design-led approach that optimises the capacity of sites. Higher density developments should generally be promoted in locations that are well connected to jobs, services, infrastructure and amenities by public transport, walking and cycling. In other areas, incremental densification should be actively encouraged by Boroughs to achieve a change in densities in the most appropriate way. This should be interpreted in the context of Policy H2 of the London Plan (2021) which states that Boroughs should proactively support well-designed new homes on small sites below 0.25 hectares in size.

Policy DMHB 17 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all new residential development should take account of the Residential Density Matrix contained in Table 5.2.

Numerical density levels are considered to be more appropriate to larger sites and are not typically used in the assessment of schemes of less than 10 units. The key consideration is therefore whether the development would acceptably integrate with the character and appearance of the locality and would respect residential amenity considerations, rather than the consideration of the density of the proposal.

Please refer to the other sections of this Committee Report which assess these planning considerations in further detail.

#### **7.03 Impact on archaeology/CAs/LBs or Areas of Special Character**

Not applicable to the proposed development.

#### **7.04 Airport safeguarding**

Not applicable to the proposed development.

#### **7.05 Impact on the green belt**

Not applicable to the proposed development.

#### **7.06 Environmental Impact**

**NOISE FROM THE PROPOSED DEVELOPMENT:**

Policy D14 of the London Plan (2021) states that in order to reduce, manage and mitigate noise to improve health and quality of life, residential and non-aviation development proposals should manage noise by avoiding significant adverse noise impacts on health and quality of life.

Whilst there would be some increase in activity and noise levels at the site. The level of noise generated from the proposed development would not be significantly above the level of noise which currently exists in this residential environment.

It should also be noted that construction works are temporary and therefore so are the associated impacts. The Environmental Protection Act (1990) and Pollution Act (1974) are in place to ensure that construction works are carried out in an appropriate and environmentally friendly manner. A condition requiring a construction management plan has been recommended for inclusion on the decision notice in order to minimise the impacts of construction on neighbouring residents and the environment as far as practicable.

#### AIR QUALITY:

Policy DMEI 14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals should demonstrate appropriate reductions in emissions. It adds that, development proposals should, as a minimum:

- i) be at least "air quality neutral";
- ii) include sufficient mitigation to ensure there is no unacceptable risk from air pollution to sensitive receptors, both existing and new; and
- iii) actively contribute towards the improvement of air quality, especially within the Air Quality Management Area.

The site is located within an Air Quality Management Area. A condition has been recommended for inclusion on the decision requiring the submission of a Construction Management Plan. The condition will aid in minimising vehicle movements by outlining a plan for the construction of the building. Minimising vehicle movements would help reduce emissions caused during the construction phase. Due to the minor nature of the development and the fact that similar applications have been approved without the requirement for a Air Quality Management Assessment or financial contribution toward air quality improvement, those measures are not considered to be proportionate or necessary for the proposed development to be acceptable.

#### SUSTAINABLE CONSTRUCTION AND USE OF RESOURCES:

Policy SI 2 of the London Plan (2021) states that residential development should achieve at least a 10% improvement beyond Building Regulations 2013.

Policy DMEI 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires all developments to make the fullest contribution to minimising carbon dioxide emissions in accordance with the London Plan targets.

Again, due to the minor nature of the development and the fact that similar applications have been approved without the requirement for a sustainable construction methods and materials condition, those measures are not considered to be necessary for the proposed development. It is noted that the proposal will need to comply with relevant standards contained within the Building Regulations.

#### **7.07 Impact on the character & appearance of the area**

Paragraph 126 of the NPPF (2021) seeks the creation of high quality, beautiful and sustainable buildings.

Paragraph 130 of the NPPF (2021) states 'Planning policies and decisions should ensure that developments:

- a) will function well and add to the overall quality of the area, not just for the short term but

over the lifetime of the development;

- b) are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;
- c) are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d) establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;
- e) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
- f) create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.'

Policies D1 and D3 of the London Plan (2021) require development proposals to be of high quality and to enhance the local context by delivering buildings and spaces that positively respond to local distinctiveness.

Hillingdon Local Plan Part 1: Strategic Policies (2012) Policy BE1 states: 'The Council will require all new development to improve and maintain the quality of the built environment in order to create successful and sustainable neighbourhoods, where people enjoy living and working and that serve the long-term needs of all residents. All new developments should achieve a high quality of design in all new buildings, alterations, extensions and the public realm which enhances the local distinctiveness of the area, contributes to community cohesion and a sense of place.'

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states:

'All development, including extensions, alterations and new buildings will be required to be designed to the highest standards and, incorporate principles of good design including: i) harmonising with the local context by taking into account the surrounding scale of development, height, mass and bulk of adjacent structures; building plot sizes and widths, plot coverage and established street patterns; building lines and setbacks, rooflines, streetscape rhythm, for example, gaps between structures and other streetscape elements, such as degree of enclosure; architectural composition and quality of detailing; local topography, views both from and to the site; and impact on neighbouring open spaces and their environment.'

Policy DMHB 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) re-emphasises the need for new development to be well integrated with the surrounding area and provides design criteria as to how this would be achieved.

As noted in the site description, the development site comprises a parcel of land located to the rear of No's 1, 3, 5 and 7 Longford Gardens. The land is currently overgrown and void of development. The proposed building would be approximately 7.95m high, 6.2m wide and 11.1m deep. It would be finished in red multi facing brick with redland roof tiles and white upvc windows (full material details are shown on plan no.FP-LC-03).

The proposed development has been designed to appear as a detached, two storey residential development. This relates to the established context. This largely residential area is characterised by two storey terraces, detached properties and semi detached

properties. The development would therefore be in keeping with the mixed character of properties in the immediate locality.

Properties within the area are finished in mixtures of brick and render and many of them have upvc windows. As such, the material finishes of the building would be acceptable. Buildings with a similar layout (one flat at ground floor and one flat at first floor) also exist within the area (e.g. at No's 1-4 Longford Close). As such, this style of flatted development would not be out of keeping. Furthermore, the development site is located within a small cul-de-sac to the rear of Longford Gardens, limiting its visual impact. For these reasons the proposal is considered to have an acceptable impact on the character and appearance of the area.

It is also important to note that the proposed two storey building has a similar massing, design and layout to the appeal scheme from 2009. Whilst this is a historic decision, there has been little change to the character and appearance of buildings surrounding the site. The decision therefore remains relevant and the Inspector made the following comments when deciding the appeal:

"I am satisfied that a suitable design could be devised that would fit in with the locality. The design before me would take up 31 percent of the site with buildings and associated structures which I consider would not be excessive. Furthermore, the proposal would meet UDP requirements for the minimum distance between dwellings and for the size of amenity space. Whilst the building would occupy most of the width of the plot it would be similar to the two backland developments at 1-4 Longford Close. The appeal site is also comparable to the size of the plot in Longford Gardens. Taking into account all these factors and the guidance in Planning Policy Statement 3: Housing on promoting higher densities, I find that the proposal would not lead to a cramped appearance or that the site would be overdeveloped."

Additionally, whilst it is noted that the design of the building is somewhat unique, in terms of design and layout the proposed development is almost identical to the most recently approved scheme at the development site (62949/APP/2018/3166) and only differs in that the building is approximately 1m<sup>2</sup> larger in footprint and 50cm bigger in height. Minor fenestration alterations are also proposed (size/design of window openings), alongside additional cycle storage provisions.

Taking into consideration, all of the above, the design of the proposed building is considered to be acceptable and the development therefore complies with the aforementioned policies, having an acceptable impact on the character and appearance of the area.

## **7.08 Impact on neighbours**

Paragraph 130 (f) of the NPPF (2021) states that new development should seek to 'create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.'

Hillingdon Local Plan Part 2: Development Management Policies (2020) Policy DMHB 11 sets out design guidance for all new development in the borough. Part B of the policy states 'development proposals should not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space.'

Guidance for Policy DMHB 11 states: 'The Council will aim to ensure that there is sufficient privacy for residents and it will resist proposals where there is an unreasonable

level of overlooking between habitable rooms of adjacent residential properties, schools or onto private open spaces. A minimum of 21 metres separation distance between windows of habitable rooms will be required to maintain levels of privacy and to prevent the possibility of overlooking. In some locations where there is a significant difference in ground levels between dwellings, a greater separation distance may be necessary.'

Guidance for Policy DMHB 11 also states: 'For the purposes of this policy, outlook is defined as the visual amenity enjoyed by occupants when looking out of their windows or from their garden. The Council will expect new development proposals to carefully consider layout and massing in order to ensure development does not result in an increased sense of enclosure and loss of outlook. Single aspect dwellings should be avoided.'

The proposed building would be approximately 7.95m high, 6.2m wide and 11.1m deep.

Privacy is a key issue in this case given the proximity of the new building to neighbouring buildings and gardens. It has also been raised as a key issue in the objections and petition. The north facing first floor windows of the building would serve a bathroom and utility room. The windows are high level and therefore limit the likelihood of overlooking to near neighbours. Furthermore, as they are not habitable rooms they have been conditioned to be obscure glazed with restricted opening.

The south facing first floor windows serve a bedroom and living room and they look toward No's 72-78 Uxbridge Road at a distance of approximately 23m. Due to the separation distance, the proposal is not considered to cause any harmful loss of privacy or overlooking to those neighbours.

The west facing first floor window of the development would serve a landing and would therefore cause no significant overlooking to the rear elevations of properties on Uxbridge Road. This relationship was approved in 2018 and remains acceptable in the current policy context as similar site circumstances remain. A condition has also been included to ensure the landing window is obscurely glazed.

The east facing first floor windows of the development would serve the living room and kitchen of the proposed first floor flat. The windows would face toward the rear elevations of No's 1, 3, 5 and 7 Longford Gardens. The windows are squat in design and have been positioned to be high level. The submitted section plan for the proposed development suggests that the foot of the windows would be approximately 2m above ground level. Due to their height and design they would not give rise to a harmful level of overlooking toward neighbouring properties on Longford Gardens. Furthermore a condition has been recommended to secure these as obscure glazed.

The ground floor windows and openings of the proposed building would look toward the main road, the site's boundary treatments and would otherwise provide views achievable from public vantage points.

For the reasons mentioned above, the proposed development is considered to cause no undue overlooking or loss of privacy to neighbouring properties.

Due to its separation distance from the habitable room windows of all neighbouring properties (approximately 15m or more), the proposal is considered to have an acceptable impact on both daylight and sunlight. There would be no harmful loss of light to neighbouring habitable rooms or external amenity spaces. Due to the orientation of the proposed building in relation to the sun, shadows would be cast toward the main road

(Longford Close) and away from No's 1, 3, 5 and 7 Longford Gardens (until the evening) and this is acceptable. This relationship is also directly comparable to that granted in 2009 on appeal, and subsequently in 2018. There has been no significant change in policy or circumstance to warrant a different conclusion on daylight and sunlight in this proposal.

It is noted that the development would cause some sense of enclosure towards the rear gardens of properties on Longford Gardens, however, substantial weight has been given to the 2009 and 2018 permissions which deemed this relationship to be acceptable. Importantly, the views of the new building would be partially obscured by outbuildings which exist to the rear of No's 1, 3, 5 and 7 Longford Gardens, providing a buffer at the ground floor level. The distance is sufficient to ensure there is no harmful loss of outlook to rear windows.

For these reasons the proposed development is considered to have an acceptable impact on the amenities of neighbours. The development accords with the development plan, particularly DMHB 11 of the Hillingdon Local Plan Part 2: Development Management Policies (2020), giving due weight to the other material considerations also identified.

#### **7.09 Living conditions for future occupiers**

##### **INTERNAL AMENITY SPACE PROVISION:**

Policy DMHB 15 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development will be required to comprise good design and create inclusive environments whilst improving safety and security by incorporating specific measures, which includes ensuring adequate defensible space is provided.

Policy DMHB 16 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all housing development should have an adequate provision of internal space in order to provide an appropriate living environment. The space standards set out in Table 5.1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) are the same as those found in Table 3.1 of the London Plan (2021).

Policy D6 of the London Plan (2021) sets out the minimum internal floor space standards required for residential developments in order to ensure that there is an adequate level of amenity for future occupants. Table 3.1 of Policy D6 of the London Plan (2021) states that:

- A one storey dwelling with 1 bedroom, 2 person occupancy should provide a GIA of at least 50 square metres.

Also of importance is Policy D6 (8) of the London Plan (2021) which states: The minimum floor to ceiling height must be 2.5m for at least 75 per cent of the Gross Internal Area of each dwelling.

Both of the proposed flats would have a GIA of 50sqm. They would also have 2.5m floor to ceiling heights, and would provide their future occupants with habitable spaces with outlook, access to light, parking and cycle storage. For the above reasons, the proposal would provide satisfactory living conditions for future residents and would comply with Policy D6 of the London Plan (2021).

It is noted that the ground floor flat has side facing living room windows which could potentially be looked into by residents of the first floor flat. This relationship is not ideal, however given the fact there is only one other dwelling, with a limited amount of occupiers and shared communal and amenity spaces proposed, this would not be so harmful as to warrant a refusal of planning permission, particularly given this exact relationship was



granted in 2009 and 2018.

#### EXTERNAL AMENITY SPACE PROVISION:

Policy DMHB 18: 'Private Outdoor Amenity Space' of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that applications for residential development should provide adequate levels of private, well designed and located amenity space. The policy advises that 1 bedroom flats should be provided with 20sqm of private amenity space.

A 30sqm shared garden would be provided for the future residents of both flats. The ground floor flat would also have a 5.5sqm private patio area. Identical amenity space provisions were provided in the previously approved scheme at the development site and it is therefore considered that it would be unreasonable to refuse the proposal on these grounds. The amenity space is, on balance, of a sufficient size and quality to accommodate the two 1-bed units.

#### **7.10 Traffic impact, Car/cycle parking, pedestrian safety**

##### PARKING AND TRIP GENERATION:

Policy DMT 2 of the Hillingdon Local Plan Part 2: Development Management Policies (2020) states:

'Development proposals must ensure that:

- i) safe and efficient vehicular access to the highway network is provided to the Council's standards;
- ii) they do not contribute to the deterioration of air quality, noise or local amenity or safety of all road users and residents;
- iii) safe, secure and convenient access and facilities for cyclists and pedestrian are satisfactorily accommodated in the design of highway and traffic management schemes;
- iv) impacts on local amenity and congestion are minimised by routing through traffic by the most direct means to the strategic road network, avoiding local distributor and access roads; and
- v) there are suitable mitigation measures to address any traffic impacts in terms of capacity and functions of existing and committed roads, including along roads or through junctions which are at capacity.'

Policy DMT 5 of the Hillingdon Local Plan Part 2: Development Management Policies (2020) states:

'A) Development proposals will be required to ensure that safe, direct and inclusive access for pedestrians and cyclists is provided on the site connecting it to the wider network, including:

- i) the retention and, where appropriate, enhancement of any existing pedestrian and cycle routes;
- ii) the provision of a high quality and safe public realm or interface with the public realm, which facilitates convenient and direct access to the site for pedestrian and cyclists;
- iii) the provision of well signposted, attractive pedestrian and cycle routes separated from vehicular traffic where possible; and
- iv) the provision of cycle parking and changing facilities in accordance with Appendix C, Table 1 or, in agreement with Council.'

Policy DMT 6 of the Hillingdon Local Plan Part 2: Development Management Policies (2020) states:

'Development proposals must comply with the parking standards outlined in Appendix C Table 1 in order to facilitate sustainable development and address issues relating to congestion and amenity. The Council may agree to vary these requirements when:

- i) the variance would not lead to a deleterious impact on street parking provision, congestion or local amenity; and/or
- ii) a transport appraisal and travel plan has been approved and parking provision is in accordance with its recommendations.

The Mayor of London adopted a new and revised London Plan in March 2021, consequently the car parking standards set out in the London Plan take precedence over those in the Local Development Plan, except where the Local Plan specifies lower local maximum standards.

The London plan advises that 0.75 parking spaces should be provided for 1 bed properties in outer London. As such, 1.5 parking spaces should be provided for the two proposed flats. 2 parking spaces are proposed. Taking into consideration that half a parking space cannot be provided and that 1 parking space would be provided for each flat, the proposed parking arrangement is acceptable.

Furthermore the arrangement is identical to the previously approved scheme (62949/APP/2018/3166).

#### ELECTRIC CHARGING POINTS:

Part G) of Policy T6 and part C) of Policy T6.1 of the London Plan (2021) state that all residential car parking spaces must provide infrastructure for electric or Ultra-Low Emission vehicles. At least 20 per cent of spaces should have active charging facilities, with passive provision for all remaining spaces. Accordingly, one car parking space should be provided with an active electric charging point and the remaining space provided with passive electric vehicle charging infrastructure. A condition has been recommended to secure this.

#### ACCESSIBLE PARKING:

In accordance with the Accessible Hillingdon SPD, 10% of car parking spaces must be for blue badge holders (disabled users) which equates to 1 car parking space. Taking into consideration the previous permission at the site, the constrained size of the site and the fact that 10% of spaces equates to 0.2% of the proposed parking arrangement. The requirement for disabled parking space is not considered to be necessary for the proposed development.

#### BICYCLE PARKING:

Appendix C, Table 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) requires the provision of cycle parking for new residential developments. 4 cycle parking space are proposed, 2 for each property. This provision is considered to be acceptable. If the application is approved, a condition is recommended to secure full details of the dimensions, external finish and design of the cycle stores.

#### REFUSE/RECYCLING COLLECTION:

Policy DMHB 11 part (d) of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that development proposals should make sufficient provision for well-designed internal and external storage space for general, recycling and organic waste, with suitable access for collection. External bins should be located and screened to avoid nuisance and adverse visual impacts to occupiers and neighbours.

If the development is approved, full details of the proposed refuse store (including its dimensions, external finishes and design) are recommended to be secured by condition.

#### **7.11 Urban design, access and security**

London Plan (2021) Policy D7 states: 'To provide suitable housing and genuine choice for London's diverse population, including disabled people, older people and families with young children, residential development must ensure that:

1) at least 10 per cent of dwellings (which are created via works to which Part M volume 1 of the Building Regulations applies) meet Building Regulation requirement M4(3) 'wheelchair user dwellings'

2) all other dwellings (which are created via works to which Part M volume 1 of the Building Regulations applies) meet Building Regulation requirement M4(2) 'accessible and adaptable dwellings'.

As mentioned by the Borough's Access Officer, no accessibility concerns are raised by the proposed development, providing the ground floor flat is constructed to meet the standards for a Category 2, M4(2) dwelling, as set out in Approved Document M to the Building Regulations (2010) 2015. This has been secured by condition.

#### **7.13 Provision of affordable & special needs housing**

Policy H2 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) states that housing provision is expected to include a range of housing to meet the needs of all types of households, and the Council will seek to maximise the delivery of affordable housing from all sites over the period of the Local Plan. For sites with a capacity of 10 or more units, the Council will seek to ensure that the affordable housing mix reflects housing needs in the borough, particularly the need for larger family units. This is supported by Policy DMH 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020).

The proposal is for less than 10 residential units and does not meet the threshold in order to require affordable housing provision. As such, the proposal is not contrary to Policy H2 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012) and Policy DMH 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) in this respect.

#### **7.14 Trees, landscaping and Ecology**

TREES AND LANDSCAPING:

Policy G1 of the London Plan (2021) states that development proposals should incorporate appropriate elements of green infrastructure that are integrated into London's wider green infrastructure network.

Policy DMHB 14 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that:

A) All developments will be expected to retain or enhance existing landscaping, trees, biodiversity or other natural features of merit.

B) Development proposals will be required to provide a landscape scheme that includes hard and soft landscaping appropriate to the character of the area, which supports and enhances biodiversity and amenity particularly in areas deficient in green infrastructure.

C) Where space for ground level planting is limited, such as high rise buildings, the inclusion of living walls and roofs will be expected where feasible.

The proposed site layout plan suggests that the proposal would have no adverse impact on significant trees. A landscaping condition has been recommended in the event of an approval.

## ECOLOGY:

Policy DMEI 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that if development is proposed on or near to a site considered to have features of ecological or geological value, applicants must submit appropriate surveys and assessments to demonstrate that the proposed development will not have unacceptable effects. The development must provide a positive contribution to the protection and enhancement of the site or feature of ecological value.

Whilst overgrown, the site does not contain any ponds or open woodland and is located in an urban location. There are no protected sites of ecological interest adjacent to the site. It is therefore considered that the likelihood of protected species being present at the site is low.

For completeness, an informative has been included on this recommendation relating to protected species, informing the applicant(s) that they must fulfill their duties under the Wildlife and Countryside Act 1981 (as amended).

### **7.17 Flooding or Drainage Issues**

Policy SI 12 of the London Plan (2021) states that development proposals should ensure that flood risk is minimised and mitigated, and that residual risk is addressed. Policy SI 13 of the London Plan (2021) states that development proposals should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible.

Policy DMEI 9 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that proposals that fail to make appropriate provision for flood risk mitigation, or which would increase the risk or consequences of flooding, will be refused. Policy DMEI 10 states that development within areas identified at risk from surface water flooding which fail to make adequate provision for the control and reduction of surface water runoff rates will be refused.

The site lies within Flood Zone 1 of the Environment Agency's Flood Risk Map. This means the site is classified as being at low risk and defined as having a less than 1 in 1,000 probability of fluvial and tidal flooding. As such, there are no restrictions on development, including more vulnerable uses such as Use Class C3 (dwellinghouses), in this location, in terms of fluvial and tidal flood risk.

Notwithstanding the above, a SuDS planning condition has been recommended for inclusion on the decision notice (in the event of approval) to ensure that water is adequately and appropriately managed on site.

### **7.19 Comments on Public Consultations**

Comments received in response to the public consultation have been summarised in section 6 (above) and addressed within the relevant sections of the report.

### **7.20 Planning obligations**

Policy DMCI 7 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that to ensure development is sustainable, planning permission will only be granted for development that clearly demonstrates there will be sufficient infrastructure of all types to support it. Infrastructure requirements will be predominantly addressed through the Council's Community Infrastructure Levy (CIL).

The Council adopted its own Community Infrastructure Levy (CIL) on 1st August 2014. The Hillingdon CIL charge for residential developments is £95 per square metre of additional floor space. This is in addition to the Mayoral CIL charge of £60 per square

metre. CIL rates are index linked. The proposal involves the erection of new dwellings and is therefore CIL liable.

**7.21 Expediency of enforcement action**

Not applicable to the proposed development.

**7.22 Other Issues**

**CONTAMINATED LAND**

Local Plan Policy DMEI 12 states:

A) Proposals for development on potentially contaminated sites will be expected to be accompanied by at least an initial study of the likely contaminants. The Council will support planning permission for any development of land which is affected by contamination where it can be demonstrated that contamination issues have been adequately assessed and the site can be safely remediated so that the development can be made suitable for the proposed use.

B) Conditions will be imposed where planning permission is given for development on land affected by contamination to ensure all the necessary remedial works are implemented, prior to commencement of development.

C) Where initial studies reveal potentially harmful levels of contamination, either to human health or controlled waters and other environmental features, full intrusive ground investigations and remediation proposals will be expected prior to any approvals.

D) In some instances, where remedial works relate to an agreed set of measures such as the management of ongoing remedial systems, or remediation of adjoining or other affected land, a S106 planning obligation will be sought.

A small portion of the site and proposed building is located within a landfill buffer area. The Council's Contaminated Land Officer was consulted on the application and raised no objection to the proposal and no concerns.

**8. Observations of the Borough Solicitor**

**General**

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probitry in Planning, 2009.

**Planning Conditions**

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

### Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

### Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

## 9. Observations of the Director of Finance

Not applicable to the proposed development.

## 10. CONCLUSION

The principle of developing the site for residential purposes has previously been established by planning permissions (now expired) and the proposal is comparable to the previously approved schemes, including ref. 62949/APP/2018/3166 granted in 2018. This planning history is a significant material consideration.

The Local Plan was adopted in 2020, which has changed the policy context. However, these changes have not been so substantial as to warrant a different conclusion on this case, particularly given the site circumstances and site constraints remain largely unchanged.

It is also relevant that the scheme would provide two additional residential units on vacant, derelict land which would add new homes to the Borough's housing supply. Small economic benefits would also arise through employment for construction and the purchase of materials.

Overall, it is considered that the development is acceptable and is recommended for approval, subject to conditions.

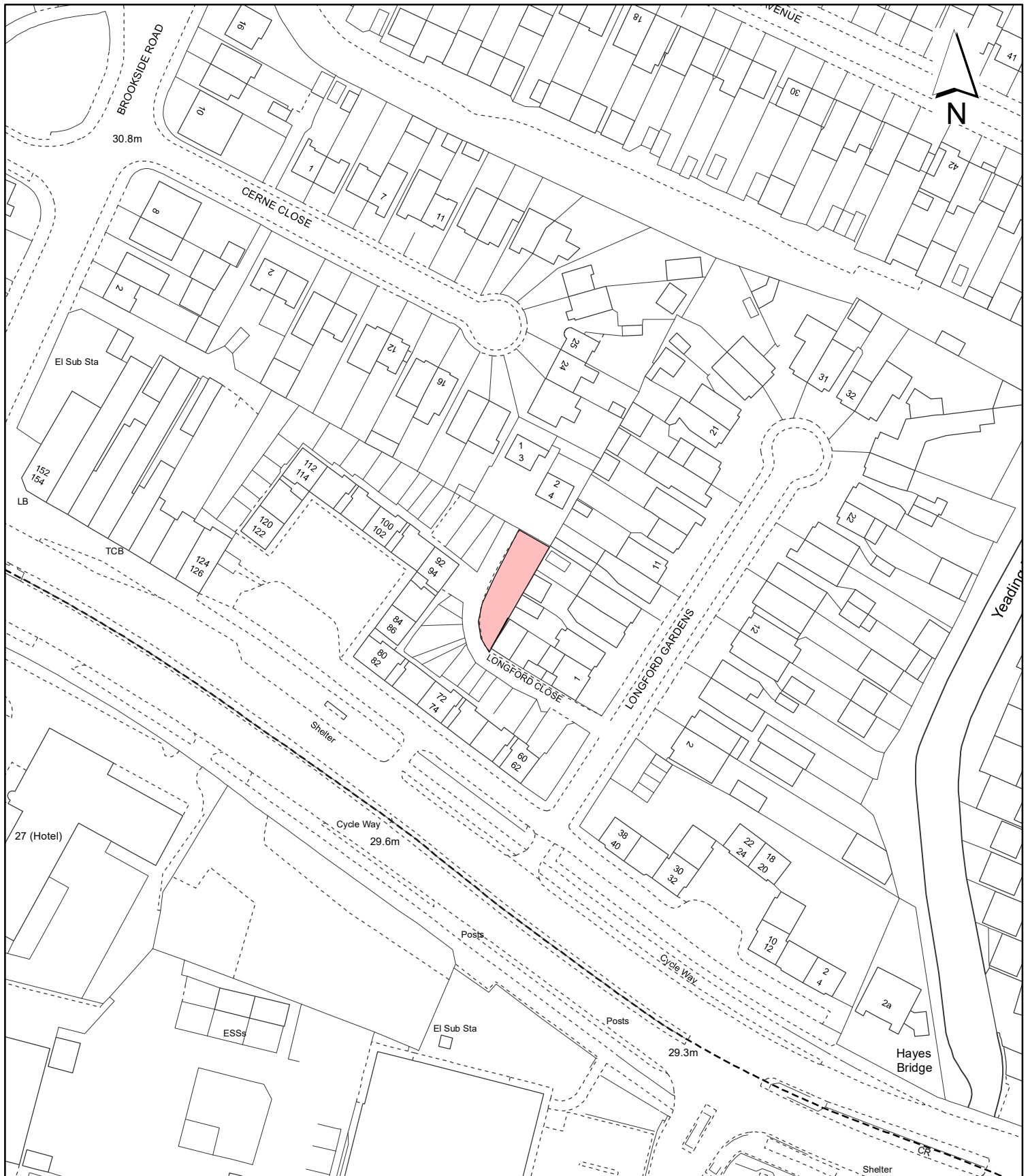
## 11. Reference Documents

Hillingdon Local Plan: Part One - Strategic Policies (November 2012)

Hillingdon Local Plan: Part Two - Development Management Policies (January 2020)  
The London Plan (March 2021)  
National Planning Policy Framework (NPPF) (July 2021)  
National Planning Practice Guidance (NPPG)  
Technical Housing Standards - Nationally Described Space Standard (2015) (as amended)

**Contact Officer:** Haydon Richardson

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

For identification purposes only.  
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Site Address:

**Land at Longford Close  
 Longford Gardens**

Planning Application Ref:  
**77102/APP/2022/647**

Planning Committee:  
**BoroughPage 112**

Scale:  
**1:1,250**

Date:  
**December 2022**

**LONDON BOROUGH  
 OF HILLINGDON  
 Residents Services  
 Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111



**HILLINGDON**  
 LONDON



A

**Item No.** Report of the Interim Director of Planning, Regeneration & Public Realm

**Address** 14 HIGH STREET HAREFIELD

**Development:** Application for planning permission for the permanent retention of the existing outdoor seating area at the rear of the coffee shop and replacing existing fence and floor, previously approved under planning application reference number 47647/APP/2020/2017.

**LBH Ref Nos:** 47647/APP/2022/2552

**Drawing Nos:** 1570.02.03.Exg.001  
1570.02.03.Exg02.002  
1570.04.03.PlIn02.060  
Design and Access Statement  
1570.02.03.Exg02.060  
1570.04.03.PlIn02.002 A

**Date Plans Recieved:** 13/08/2022      **Date(s) of Amendment(s):** 13/08/2022

**Date Application Valid:** 13/08/2022      17/10/2022

## 1. SUMMARY

This application seeks planning permission to permanently retain an existing outdoor seating area at the rear of the coffee shop. This would include replacing the existing fence and paving. No. 14 High Street, Harefield is a Grade II listed building and the site is within the Harefield Village Conservation Area.

Temporary planning permission for outdoor seating was granted during Covid-19 restrictions to enable social distancing (ref. 47647/APP/2020/2017 dated 27-08-20). However, the subsequent planning application seeking to retain the outdoor seating was refused due to the poor quality materials used in the fencing and surfacing. The materials resulted in 'less than substantial harm' to the Harefield Village Conservation Area and Grade II listed building, which were not outweighed by public benefits (ref. 47647/APP/2022/1409 dated 28-06-22).

This application is an amended scheme which comprises better quality fencing and hard surfaces more suitable for the setting of a listed building and within a conservation area. The Council's Conservation Officer does not raise any objection to the revised scheme and no harm is identified to the designated heritage assets. In this regard the application addresses the previous reason for refusal and is therefore recommended for approval subject to conditions.

## 2. RECOMMENDATION

**APPROVAL subject to the following:**

**1 COM3 Time Limit**

The development hereby permitted shall be completed in its entirety within 4 calendar months from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

**2 RES4 Accordance with Approved Plans**

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers

1570.02.03.Exg.001  
1570.04.03.PIn02.002 Rev A  
1570.04.03.PIn02.060

and shall thereafter be retained/maintained for as long as the development remains in existence.

#### REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan Part 1 (2012), Part 2 (2020) and the London Plan (2021).

### **3 NONSC Shall not be attached to Listed Building**

The fencing hereby approved shall not be attached to the Grade II Listed Building.

#### REASON

To preserve the historic significance of the designated heritage asset, in accordance with DMHB 1 and DMHB 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policies BE1 and HE1 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012), Policy HC1 of the London Plan (2021) and Chapter 16 of the National Planning Policy Framework (2021).

### **4 NONSC Hours of Use**

The seating area hereby approved shall not be used except between the hours of 0900 to 1800 Mondays to Sundays.

#### REASON

To ensure that the amenity of the occupiers of adjoining or nearby properties is not adversely affected in accordance with Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020), Policy EM8 of the Hillingdon Local Plan: Part 2 - Strategic Policies (2012) and Policies D13 and D14 of the London Plan (2021).

### **5 RES7 Materials (Submission)**

No development shall take place until details of the fencing, the wood stain finish and the York stone paving have been submitted to and approved in writing by the Local Planning Authority. Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and photographs/images.

#### REASON

To ensure that the development presents a satisfactory appearance in accordance with Policies DMHB 1, DMHB 2, DMHB 11 of the Hillingdon Local Plan Part 2 (2020), Policies BE1 and HE1 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012), Policy HC1 of the London Plan (2021) and Chapter 16 of the National Planning Policy Framework (2021).

## **INFORMATIVES**

### **1 I52 Compulsory Informative (1)**

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The

Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

## **2 153 Compulsory Informative (2)**

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including The London Plan (2021) and national guidance.

DMTC 1	Town Centre Development
DMTC 4	Amenity and Town Centre Uses
DMHB 1	Heritage Assets
DMHB 2	Listed Buildings
DMHB 4	Conservation Areas
DMHB 11	Design of New Development
DMHB 14	Trees and Landscaping
DMT 2	Highways Impacts
DMT 5	Pedestrians and Cyclists
DMT 6	Vehicle Parking
LPP D5	(2021) Inclusive design
LPP D8	(2021) Public realm
LPP D13	(2021) Agent of change
LPP D14	(2021) Noise
LPP GG5	(2021) Growing a good economy
LPP HC1	(2021) Heritage conservation and growth
LPP SD6	(2021) Town centres and high streets
LPP SD7	(2021) Town centres: development principles and Development Plan Documents
LPP T6.3	(2021) Retail parking
NPPF6	NPPF 2021 - Building a strong, competitive economy
NPPF7	NPPF 2021 - Ensuring the vitality of town centres
NPPF16	NPPF 2021 - Conserving & enhancing the historic environment
NPPF12	NPPF 2021 - Achieving well-designed places

## **3 170 LBH worked applicant in a positive & proactive (Granting)**

In dealing with the application the Council has implemented the requirement in the National Planning Policy Framework to work with the applicant in a positive and proactive way. We have made available detailed advice in the form of our statutory policies from Local Plan Part 1, Local Plan Part 2, Supplementary Planning Documents, Planning Briefs and other informal written guidance, as well as offering a full pre-application advice service, in order to ensure that the applicant has been given every opportunity to submit an application which is likely to be considered favourably.

### **3. CONSIDERATIONS**

#### **3.1 Site and Locality**

The application site comprises a cafe (Use Class E(b)), occupied by We Love Coffee, located on the west side of High Street, Harefield. The site forms part of the Harefield Village Conservation Area and is designated as part of the Harefield Local Centre and Core Shopping Area as part of the Hillingdon Local Plan: Part 2 - Development

Management Policies (January 2020). Notably, no. 14 High Street is a Grade II Listed Building.

### 3.2 Proposed Scheme

Planning permission is sought for the permanent retention of an outdoor seating area at the rear of the coffee shop. The seating area would amount to approximately 47 square metres in footprint. The existing reclaimed palette fencing would be replaced with a new fencing enclosure and a York stone hard surface would be provided.

### 3.3 Relevant Planning History

47647/APP/2020/2017 14 High Street Harefield

Erection of a 2m high fence to form a temporary outdoor seating area at the rear of the coffee shop.

**Decision:** 27-08-2020 Approved

47647/APP/2022/1409 14 High Street Harefield

Permanent retention of the existing 2m high fence and outdoor seating area at the rear of the coffee shop (previous application ref. 47647/APP/2020/2017) (retrospective application).

**Decision:** 28-06-2022 Refused

#### Comment on Relevant Planning History

The relevant planning history is listed above, notably temporary planning permission ref: 47647/APP/2020/2017 which approved for a period of 2 years the erection of a 2m high fence to form a temporary outdoor seating area at the rear of the coffee shop. That temporary approval lapsed on 27 August 2022.

This application is a revised resubmission of planning application ref: 47647/APP/2022/1409 for the permanent retention of the existing 2m high fence and outdoor seating area at the rear of the coffee shop. Application 47647/APP/2022/1409 was refused for the following reason:

1. The development proposal due to the siting, design, materials and construction would result in 'less than substantial harm' to the setting of the Listed Building, would fail to preserve the appearance and character of the Conservation Area and would harm the general visual amenities of the area. As there are no longer exceptional circumstances to justify the development proposal and no public benefits identified to outweigh the harm, the development proposal would conflict with Policies BE1 and HE1 of the Hillingdon Local Plan: Part 1 - Strategic Policies (2012), Policies DMHB 1, DMHB 2, DMHB 4 and DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) and Paragraph 202 of the National Planning Policy Framework (2021).

## 4. Planning Policies and Standards

Development Plan:

Planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

The Development Plan for the London Borough of Hillingdon currently consists of the following documents:

The Local Plan: Part 1 - Strategic Policies (2012)

The Local Plan: Part 2 - Development Management Policies (2020)

The Local Plan: Part 2 - Site Allocations and Designations (2020)  
The West London Waste Plan (2015)  
The London Plan (2021)

Material Considerations:

The National Planning Policy Framework (NPPF) (2021) is also a material consideration in planning decisions, as well as relevant supplementary planning documents and guidance.

### **Local Plan Designation and London Plan**

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

PT1.HE1 (2012) Heritage

Part 2 Policies:

DMTC 1 Town Centre Development

DMTC 4 Amenity and Town Centre Uses

DMHB 1 Heritage Assets

DMHB 2 Listed Buildings

DMHB 4 Conservation Areas

DMHB 11 Design of New Development

DMHB 14 Trees and Landscaping

DMT 2 Highways Impacts

DMT 5 Pedestrians and Cyclists

DMT 6 Vehicle Parking

LPP D5 (2021) Inclusive design

LPP D8 (2021) Public realm

LPP D13 (2021) Agent of change

LPP D14 (2021) Noise

LPP GG5 (2021) Growing a good economy

LPP HC1 (2021) Heritage conservation and growth

LPP SD6 (2021) Town centres and high streets

LPP SD7 (2021) Town centres: development principles and Development Plan Documents

LPP T6.3 (2021) Retail parking

NPPF6 NPPF 2021 - Building a strong, competitive economy

NPPF7 NPPF 2021 - Ensuring the vitality of town centres

NPPF16 NPPF 2021 - Conserving & enhancing the historic environment

NPPF12 NPPF 2021 - Achieving well-designed places

### **5. Advertisement and Site Notice**

**5.1** Advertisement Expiry Date:- Not applicable

**5.2** Site Notice Expiry Date:- **28th September 2022**  
**28th September 2022**

## **6. Consultations**

### **External Consultees**

13 neighbours were consulted by letter dated 25-08-22. A site notice was displayed on 06-09-22 and an advertisement was placed in the Gazette.

49 letters of support were received along with a petition in support with 126 signatures.

5 letters of objection were received citing concerns regarding the following (summarised) matters:

- Impact on the fabric of the listed building
- Noise, smoke, smell
- Access, parking, refuse, servicing of the newly approved restaurant at No.12

### **PLANNING OFFICER COMMENTS:**

The fencing would not require any alteration to the main building and as such the historic fabric of the listed building would not be affected. A condition has been included in paragraph 2 of this report to ensure that no part of the fencing enclosure attaches to the listed building. The design and heritage considerations are discussed in the 'Impact on the character & appearance of the area' section at paragraph 7.07 of this report.

Noise and odour are discussed in the 'Impact on neighbours' section at Paragraph 7.08.

Access, parking and refuse arrangements would be unchanged and it is noted the Highways Officer does not raise any concerns. The fencing would be located within the application site and this is not considered to impact on the servicing of the recently approved restaurant at No.12. These matters are discussed in the 'Traffic impact, Car/cycle parking, pedestrian safety' section at Paragraph 7.10.

### **Internal Consultees**

#### **ACCESS OFFICER:**

I have considered the detail of this planning application and have no comments to make.

#### **CONSERVATION OFFICER:**

The application seeks planning permission for the permanent retention of the existing outdoor seating area to the rear of the coffee shop at 14 High Street, Harefield with the replacement of the fencing and flooring previously approved under planning application reference number 47647/APP/2020/2017 (temporary permission). 14 High Street, Harefield is a grade II listed building that is situated within a prominent location within the Harefield Village Conservation Area.

The Grade II Listed Building probably dates from the early part of the 18th century and has benefitted from mid-19th and 20th century alterations. The existing shop front along the High Street elevation is a 20th century addition. The formal building frontage comprises a symmetrical composition three windows wide with a painted roughcast finish and decorative window surrounds that makes a positive contribution to the street scene.

The rear elevation is much simpler in appearance and reflects the service side of the building. The elevation is characterised partly by a pebble dash render, London stock brick and six over 6 glazing bar timber sash windows. A later external staircase provides access to the first-floor flats above. Prior to the external seating area being introduced, the rear service area was a functional and untidy space that was used primarily for bin storage / car parking and contributed very little to the character and appearance of the conservation area nor the setting of the listed building.

The creation of a fenced off seating area for the cafe was granted temporary permission two years ago in response to Covid restrictions and the need for social distancing and the avoidance of enclosed spaces to allow the cafe to continue operating. Despite the permission the works were poorly executed and did not accord with the planning approval, introducing reclaimed timber pallets providing an untidy appearance. Rear yards associated with shops are usually enclosed with brick walls with access provided by timber gates. This would be the preferred treatment for enclosing the space. Notwithstanding this, the current enclosures and boundary treatments to the rear of the shops are primarily timber fencing. Timber fencing can, therefore, be supported in this instance as it would have a negligible impact on the character and appearance on this part of the conservation area and little impact on the setting of the listed building.

There are concerns, however, with the proposed grey porcelain floor tiles which would be an inappropriate modern finish to the setting of the listed building and wider conservation area. The floor finishes should be changed to a more traditional paving such as York Stone.

#### HIGHWAYS OFFICER:

There are no envisaged highway related issues associated with this proposal as pedestrian access is satisfactory as is servicing to the rear of No's 12-16 which would remain broadly unaffected.

#### PLANNING OFFICER RESPONSE:

In respect to the comments received from the Conservation Officer, the grey porcelain tiles have been revised to York stone to address these concerns. A condition has been included requiring full details of the York stone paving and timber fencing. It is noted that the Council's Access Officer and Highways Officer do not raise any concerns.

## **7. MAIN PLANNING ISSUES**

### **7.01 The principle of the development**

The application site is designated as part of the Harefield Local Centre and Core Shopping Area as part of the Hillingdon Local Plan: Part 2 - Development Management Policies (January 2020).

Policy E5 of the Hillingdon Local Plan: Part 1 - Strategic Policies (November 2012) states that local parades will be protected, enhanced and managed to ensure they meet the needs of the local community and enhance the quality of life for local residents, particularly those without access to a car.

Policy DMTC 3 of the Hillingdon Local Plan: Part 2 - Development Management Policies (January 2020) states:

A) The Council will protect and enhance the function of local centres and local shopping parades by retaining uses that support their continued viability and attractiveness to the locality they serve. In considering applications for changes of use of shops the Council will ensure that:

- i) the local centre or shopping parade retains sufficient essential shop uses to provide a range and choice of shops appropriate to the size of the parade, and its function in the Borough shopping hierarchy;
- ii) at least of 50% of the local centre or shopping parade is retained as Use Class A1 shops; and
- iii) the surrounding residential area is not deficient in essential shop uses.

As noted above, there is planning policy support for applications which protect the function of local centres and local shopping parades. Notably, the seating area would not impact on the provision of essential shops within the local centre and core shopping area. Therefore the principle of the development is acceptable, subject to compliance with wider

planning considerations.

**7.02 Density of the proposed development**

Not applicable to this application.

**7.03 Impact on archaeology/CAs/LBs or Areas of Special Character**

Please see 'Impact on the character & appearance of the area' section of the report at Paragraph 7.07.

**7.04 Airport safeguarding**

Not applicable to this application.

**7.05 Impact on the green belt**

Not applicable to this application.

**7.07 Impact on the character & appearance of the area**

**POLICY CONTEXT**

The application site comprises a cafe, located on the west side of High Street, Harefield, forming part of the Harefield Village Conservation Area, Harefield Local Centre and Core Shopping Area. Notably, no. 14 High Street is a Grade II Listed Building. The following planning policies are considered relevant:

Paragraph 126 of the National Planning Policy Framework (2021) seeks the creation of high quality, beautiful and sustainable buildings. Parts b) and c) of paragraph 130 of the NPPF (2021) states that planning policies and decisions should ensure that developments are visually attractive as a result of good architecture and are sympathetic to local character and history, including the surrounding built environment.

Paragraph 199 of the NPPF (2021) states that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be).

Paragraph 202 of the NPPF (2021) states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

Policies D1, D3 and D4 of the London Plan (2021) require development proposals to be high quality and to enhance the local context and be delivering buildings and spaces that positively respond to local distinctiveness.

Policy HC1 of the London Plan (2021) states that development proposals affecting heritage assets, and their settings, should conserve their significance, by being sympathetic to the assets' significance and appreciation within their surroundings.

Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (2012) states that all new developments should achieve a high quality of design in all new buildings and the public realm contributes to community cohesion and a sense of place.

Policy HE1 of the Hillingdon Local Plan: Part One Strategic Policies (2012) states that the Council will conserve and enhance Hillingdon's distinct and varied environment, its settings and the wider historic landscape, which includes the designated heritage assets such as statutorily Listed Buildings, Conservation Areas and Scheduled Ancient Monuments.

Policy DMHB 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies



(January 2020) states:

A) The Council will expect development proposals to avoid harm to the historic environment. Development that has an effect on heritage assets will only be supported where:

- i) it sustains and enhances the significance of the heritage asset and puts them into viable uses consistent with their conservation;
- ii) it will not lead to a loss of significance or harm to an asset, unless it can be demonstrated that it will provide public benefit that would outweigh the harm or loss, in accordance with the NPPF;
- iii) it makes a positive contribution to the local character and distinctiveness of the area;
- iv) any extensions or alterations are designed in sympathy, without detracting from or competing with the heritage asset;
- v) the proposal would relate appropriately in terms of siting, style, scale, massing, height, design and materials;
- vi) buildings and structures within the curtilage of a heritage asset, or in close proximity to it, do not compromise its setting; and
- vii) opportunities are taken to conserve or enhance the setting, so that the significance of the asset can be appreciated more readily.

Policy DMHB 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (January 2020) states that planning permission will not be granted for proposals which are considered detrimental to the setting of a Listed Building.

Policy DMHB 4 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that new development, including alterations and extensions to existing buildings, within a Conservation Area or on its fringes, will be expected to preserve or enhance the character or appearance of the area. It should sustain and enhance its significance and make a positive contribution to local character and distinctiveness.

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all development will be required to be designed to the highest standards and incorporate principles of good design.

Policy DMHB 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) advises that development should be well integrated with the surrounding area.

## ASSESSMENT

As the fencing is not attached to the building, the Council's Conservation Officer has confirmed that listed building consent is not required. However, it is free standing and planning permission is required and a key consideration in determining the application is the setting of the listed building.

As previously stated by the Council's Conservation Officer, the principle of using the space for outdoor seating is considered acceptable. Albeit the reclaimed pallets used as fencing was not considered an appropriate boundary treatment in relation to the Listed Building and was considered harmful to its setting. It was also not considered to contribute positively to the setting of the listed building or the conservation area.

In general, the seating area is located to the rear of the premises and is not visible from the street scene, however this does not mean that the Conservation Area or the setting of the Listed Building should not be preserved or enhanced.

The constructed fencing does not reflect the approved plans (for a temporary period) but instead appear to be constructed from misaligned reclaimed pallets of varying degrees of maintenance. The visual impact is harmful to the setting of the listed building, the conservation area and the general amenities of the area. The previous application to retain the palette fencing was refused as it is considered to pose 'less than substantial harm', which was not outweighed by public benefits.

However, this revised and resubmitted application now proposes higher quality timber fencing in the form of Harmony Cedar look panel fencing which is considered to be a significant improvement on the existing palette fencing and a more suitable treatment for its sensitive site context. In addition, following the Conservation Officer's comments, the scheme has been amended to provide a traditional hard surface constructed with York stone paving. The specific details of the fencing and paving would be secured by condition. The development proposal would therefore preserve the setting of the listed building and the appearance and character of the Harefield Village Conservation Area.

As no harm is identified to the heritage assets, the resubmission application would overcome the previous reasons for refusal. In this regard, the development proposal would accord with planning policies referred to above.

#### **7.08 Impact on neighbours**

Policy D13 (Agent of Change) of the London Plan (2021) states -

A The Agent of Change principle places the responsibility for mitigating impacts from existing noise and other nuisance-generating activities or uses on the proposed new noise-sensitive development. Boroughs should ensure that Development Plans and planning decisions reflect the Agent of Change principle and take account of existing noise and other nuisance-generating uses in a sensitive manner when new development is proposed nearby.

B Development should be designed to ensure that established noise and other nuisance generating uses remain viable and can continue or grow without unreasonable restrictions being placed on them.

C New noise and other nuisance-generating development proposed close to residential and other noise-sensitive uses should put in place measures to mitigate and manage any noise impacts for neighbouring residents and businesses.

D Development proposals should manage noise and other potential nuisances by:

- 1) ensuring good design mitigates and minimises existing and potential nuisances generated by existing uses and activities located in the area
- 2) exploring mitigation measures early in the design stage, with necessary and appropriate provisions including ongoing and future management of mitigation measures secured through planning obligations
- 3) separating new noise-sensitive development where possible from existing noise generating businesses and uses through distance, screening, internal layout, soundproofing, insulation and other acoustic design measures.

E Boroughs should not normally permit development proposals that have not clearly demonstrated how noise and other nuisances will be mitigated and managed.

Policy D14 (Noise) of the London Plan (2021) states -

A In order to reduce, manage and mitigate noise to improve health and quality of life, residential and other non-aviation development proposals should manage noise by:

- 1) avoiding significant adverse noise impacts on health and quality of life
- 2) reflecting the Agent of Change principle as set out in Policy D13 Agent of Change
- 3) mitigating and minimising the existing and potential adverse impacts of noise on, from, within, as a result of, or in the vicinity of new development without placing unreasonable restrictions on existing noise-generating uses
- 4) improving and enhancing the acoustic environment and promoting appropriate

soundscapes (including Quiet Areas and spaces of relative tranquillity)

5) separating new noise-sensitive development from major noise sources (such as road, rail, air transport and some types of industrial use) through the use of distance, screening, layout, orientation, uses and materials - in preference to sole reliance on sound insulation

6) where it is not possible to achieve separation of noise-sensitive development and noise sources without undue impact on other sustainable development objectives, then any potential adverse effects should be controlled and mitigated through applying good acoustic design principles

7) promoting new technologies and improved practices to reduce noise at source, and on the transmission path from source to receiver.

Policy EM8 of the Hillingdon Local Plan: Part 1 - Strategic Policies (November 2012) states that the Council will seek to ensure that noise sensitive development and noise generating development are only permitted if noise impacts can be adequately controlled and mitigated.

Policy DMHB 11 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) seeks to ensure that development proposals do not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space. The supporting text for this policy states that the Council will expect new development proposals to carefully consider layout and massing in order to ensure development does not result in an increased sense of enclosure and loss of outlook.

The application site forms part of a Core Shopping Area within the Harefield Local Centre. Generally, commercial uses are located at ground floor with residential flats above, including nos. 12 to 22 High Street, Harefield. The proposed seating area would also be sited approximately 5.2 metres east of the garden space of no.1 Taylor Close and further from nos. 2 and 3 Taylor Close.

In terms of the impact of the proposed seating area on neighbouring residents, it is considered that the proposed fencing would not impact on the outlook from or daylight and sunlight received by neighbouring properties. The fencing would also serve the purpose of containing some of the noise impact associated with the space. The applicant notes that any noise impact would be limited to operating hours which would be between 9am and 6pm Monday to Sunday. A suitable condition is recommended in the event of an approval.

Although the proposed development would be introducing a potential noise impact on neighbouring residents, the nature of a cafe is considered less likely to be associated with significant noise generation. It is noted that local residents have cited concerns regarding noise, odour and smoke. However, given the location of the site within a Core Shopping Area, and the hours of use would be limited to sociable hours (as opposed to unsociable hours), the proposed development is not considered to generate significant harm to neighbouring residents to warrant a refusal on amenity grounds.

As such, the proposal is not considered contrary to Policies D13 and D14 of the London Plan (2021), Policy EM8 of the Hillingdon Local Plan: Part 2 - Strategic Policies (November 2012) and Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (January 2020).

#### **7.09 Living conditions for future occupiers**

Not applicable to this application.

#### **7.10 Traffic impact, Car/cycle parking, pedestrian safety**

Policy DMT 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (January 2020) states that development proposals will be required to meet the transport

needs of the development and address its transport impacts in a sustainable manner.

Policy DMT 2 of the Hillingdon Local Plan: Part 2 - Development Management Policies (January 2020) states that proposals must ensure that safe and efficient vehicular access to the highway network is provided, schemes do not contribute to the deterioration of air quality, noise or local amenity or safety of all road users and residents. Also that impacts on local amenity and congestion are minimised and there are suitable mitigation measures to address any traffic impacts in terms of capacity and functions of existing and committed roads.

Policy DMT 6 of the Hillingdon Local Plan: Part 2 - Development Management Policies (January 2020) requires that proposals comply with the Council's parking standards in order to facilitate sustainable development and address issues relating to congestion and amenity.

The proposal would not generate additional trips that would be considered detrimental to the local highway network. There is also sufficient space for servicing and delivery to continue to the rear of the premises. The site's bin storage provision would be re-sited but would not be compromised in terms of capacity or access. The Council's Highway Officer does not raise any objections.

#### **7.11 Urban design, access and security**

##### URBAN DESIGN

Please see 'Impact on the character & appearance of the area' section of the report at Paragraph 7.07.

##### ACCESS

Policy D5 of the London Plan (2021) states - Development proposals should achieve the highest standards of accessible and inclusive design. They should:

- 1) be designed taking into account London's diverse population
- 2) provide high quality people focused spaces that are designed to facilitate social interaction and inclusion
- 3) be convenient and welcoming with no disabling barriers, providing independent access
- 4) be able to be entered, used and exited safely, easily and with dignity for all.

Wide access gates are proposed, allowing for access by wheelchair users. The existing surfacing would not be considered suitable for a permanent seating area, however the scheme has been revised with hard surfacing improvements that would improve accessibility.

##### SECURITY

The security of the site is not considered to be compromised by the proposed development. The proposed use is also considered to increase activity to the rear of the premises which is likely to discourage anti-social behaviour.

#### **7.12 Disabled access**

Please see 'Urban design, access and security' section of the report at Paragraph 7.11.

#### **7.13 Provision of affordable & special needs housing**

Not applicable to this application.

#### **7.14 Trees, landscaping and Ecology**

Not applicable to this application.

#### **7.15 Sustainable waste management**

Not applicable to this application.

**7.16 Renewable energy / Sustainability**

Not applicable to this application.

**7.17 Flooding or Drainage Issues**

Not applicable to this application.

**7.18 Noise or Air Quality Issues**

Please see 'Impact on neighbours' section of the report at Paragraph 7.08.

**7.19 Comments on Public Consultations**

Please see 'External Consultees' section of the report at Paragraph 6.

**7.20 Planning obligations**

Not applicable to this application.

**7.21 Expediency of enforcement action**

Not applicable to this application.

**7.22 Other Issues**

No other issues are identified.

**8. Observations of the Borough Solicitor**

**General**

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

**Planning Conditions**

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

**Planning Obligations**

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

**Equalities and Human Rights**

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected

characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

#### **9. Observations of the Director of Finance**

Not applicable to this application.

#### **10. CONCLUSION**

For the reasons set out in this report, it is considered that the proposal would comply with the objectives of national, regional and local planning policies and guidance. It is therefore recommended that the application be approved, subject to the imposition of the conditions set out in Section 2 of the Committee Report.

#### **11. Reference Documents**

Hillingdon Local Plan: Part 1 - Strategic Policies (2012)

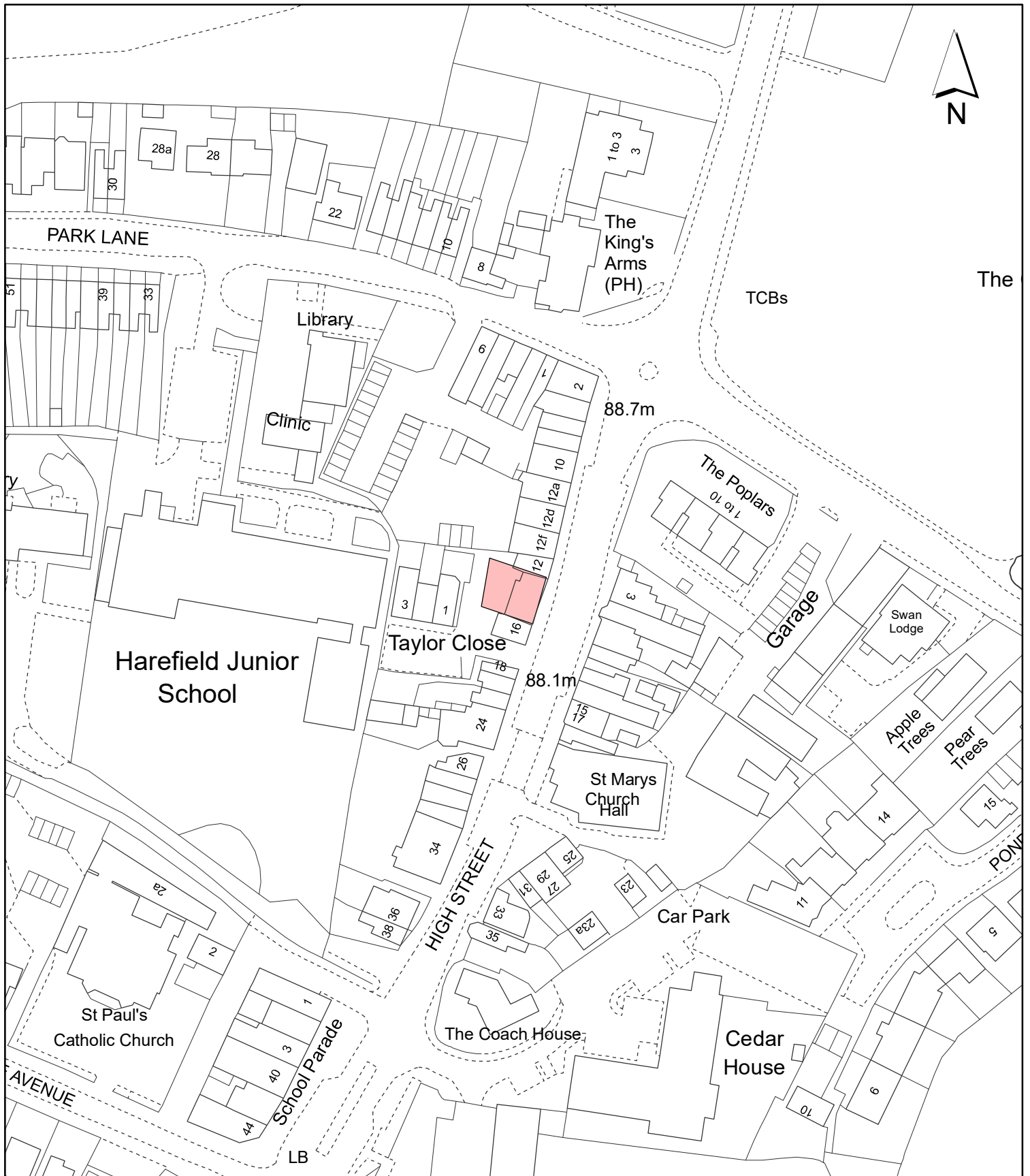
Hillingdon Local Plan: Part 2 - Development Management Policies (2020)

London Plan (2021)

National Planning Policy Framework (2021)

**Contact Officer:** Christos Chrysanthou

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

For identification purposes only.  
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Site Address:

**14 High Street**

**LONDON BOROUGH OF HILLINGDON**  
**Residents Services Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**47647/APP/2022/2552**

Scale:

**1:1,250**

Planning Committee:

**BoroughPage 127**

Date:

**December 2022**



**HILLINGDON**  
 LONDON

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A

**Item No.**            **Report of the Interim Director of Planning, Regeneration & Public Realm**

**Address**            AXIS HOUSE 242 BATH ROAD SIPSON

**Development:**    Change of use of parking spaces from residential use to a paying public car park.

**LBH Ref Nos:**     43794/APP/2022/2500

**Drawing Nos:**    Design & Access / Planning Statement (September 2022)  
4D-304 P 00  
4D-304 E 00

**Date Plans Recieved:**    09/08/2022                            **Date(s) of Amendment(s):**

**Date Application Valid:**    12/09/2022

## 1. **SUMMARY**

The application seeks planning permission to change the use of the residential car park within the application site to a paid public car park, to be shared between the existing residents and paying members of the public.

Very few details have been submitted to assess the potential impacts of this change of use and the site has a complicated planning history which is further complicated by separate landowners for different parts of the Axis House car park.

Nonetheless, based on the information submitted, the proposal is considered to be an unacceptable land use in this location, compromising what appears to already be insufficient residential parking for 71 flats whilst reducing residential amenity, increasing the perception of an unsafe environment and potentially leading to a significant amount of displacement on-street into the surrounding roads, which include the Strategic Road Network and areas identified as having very high levels of congestion.

There is significant local opposition to the proposal, with the majority of the 44 objections coming from existing occupants and landlords within Axis House.

On this basis, the proposal is recommended for refusal.

## 2. **RECOMMENDATION**

**REFUSAL for the following reasons:**

### 1            NON2            **Impact on Local Parking and Highways**

The proposed change of use would result in the removal of all wheelchair accessible parking spaces for existing residents of Axis House and has the potential to attract additional vehicle movements to the site and lead to a significant amount of displacement on-street parking, as existing residents who use the car park would be forced to seek alternative spaces nearby, which in turn could exacerbate the very high levels of vehicle congestion in the area. Additionally, the proposed land use would be directly associated with the operation of Heathrow Airport and therefore is an inappropriate land use by definition outside of the Heathrow Airport boundary. As such, the proposal would be contrary to Policies T1, T2, T4, T6 and T6.1 of the London Plan (2021) and Policies DMT 1, DMT 2, DMT 6 and DMAV 2 of the Hillingdon Local Plan: Development Management Policies (2020).

## **2 NON2 Impact on Residential Amenity**

The proposed change of use would lead to a significant loss of amenity for the existing residents of Axis House through an increase in vehicle movements which are not associated with the residential use of the site and an associated increase in noise, disturbance and air pollution alongside a loss of privacy, and an increase in the opportunities for crime and anti-social behaviour. On this basis, the proposal would be contrary to Policies D5, D6, D11, D14 and SI 1 of the London Plan (2021), Policies BE1 and EM8 of the Hillingdon Local Plan: Strategic Policies (2012), Policy DMHB 11 of the Hillingdon Local Plan: Development Management Policies (2020) and the intentions of the Mayor's Housing SPG (2016) and Secured by Design guidance (2015 and 2019) in providing a safe and secure environment.

## **3 NON2 Lack of Air Quality Assessment or Transport Statement**

Neither an Air Quality Assessment or Transport Statement has been submitted to support the change of use and the site falls within an Air Quality Management Area and an Air Quality Focus Area, where there are known air quality issues. As such, the proposal has failed to demonstrate that it complies with Policy SI 1 of the London Plan (2021) and Policy DMEI 14 of the Hillingdon Local Plan: Development Management Policies (2020).

### **INFORMATIVES**

#### **1**

Under Article 14 of the Town and Country Planning (Development Management Procedure) (England) Order 2015 (as amended), an applicant is required to declare whether they are the sole owner of all of the land to which the application relates, and sign the relevant certificate and serve notice accordingly. As part of the application, Certificate B has been signed, and it is stated that the applicant has served the required notice on two owners (or agricultural tenants) of the land to which this application relates.

The number of comments received from landlords who own and lease out properties within Axis House indicates that the ownership of all of the land within the application site has not been correctly identified as there appear to be more than two landlords, and other comments indicate that some residents have purchased and own their flats.

As such, whilst not a reason for refusal, the applicant is advised that had permission been granted and it was later found that the correct notices had not been served, the permission would have been invalid and would not have been implementable.

### **3. CONSIDERATIONS**

#### **3.1 Site and Locality**

The application site comprises part of the car park for Axis House, a residential building which was converted from an office and extended in 2015-2016 to allow its use as a block of 71 flats. The application site only includes the eastern part of the parking area, comprised of 30 parking spaces immediately to the front of Axis House, and it is understood that the western half of the car park is not available to the residents of Axis House and is now under separate ownership.

The surrounding area is a relatively car-dominated environment, with Bath Road to the south acting as one of the main routes from Heathrow Airport to central London. To the north, the character is more suburban residential, with low-rise semi-detached and terraced housing making up most of the buildings in the area, with on-street controls (Parking Management Scheme H1) preventing on-street parking.

### 3.2 Proposed Scheme

The application seeks planning permission to change the use of the residential car park within the application site to a paid public car park, to be shared between the existing residents and paying members of the public.

### 3.3 Relevant Planning History

43794/APP/2014/1542 Axis House 242 Bath Road Sipson

Change of use of offices to 64 Flats (Application for Prior Approval under Schedule 2 Part 3 of the Town and Country Planning (General Permitted Development) Order 1995 (as amended))

**Decision:** 27-05-2014 PRN

43794/APP/2015/1272 Axis House 242 Bath Road Sipson

Conversion of roof space to habitable use to include a rear dormer and a side dormer and 8 x rooflights to create 7 x 1-bed and 1 x 2-bed self contained flats with associated parking

**Decision:** 20-07-2015 Refused

43794/APP/2015/2822 Axis House 242 Bath Road Sipson

Conversion of roof space to habitable use to include a rear dormer and a side dormer and 17 rooflights to create 6 x 1-bed and 1 x 2-bed self contained flats with associated parking

**Decision:** 15-10-2015 Approved

43794/APP/2016/2814 Axis House 242 Bath Road Sipson

Details pursuant to conditions 4 (Landscaping) and 5 (Parking) of planning permission Ref: 43794/APP/2015/2822 dated 15/10/2015 (Conversion of roof space to habitable use to include a rear dormer and a side dormer and 17 rooflights to create 6 x 1-bed and 1 x 2-bed self contained flats with associated parking)

**Decision:** 31-10-2016 Approved

43794/APP/2016/4445 Axis House 242 Bath Road Sipson

Erection of a part 4-storey, part 7-storey 150 bedroom hotel (Use Class C1) including the excavation of a basement to provide 62 car parking spaces and associated landscaping.

**Decision:** 18-08-2017 Refused

43794/APP/2017/3704 Axis House 242 Bath Road Sipson

Erection of a 5-storey 128 bedroom hotel including the excavation of a basement to provide 50 car parking and associated landscaping

**Decision:** 05-12-2017 Approved

43794/APP/2018/2779 Axis House 242 Bath Road Sipson

Erection of a 6-storey 157-bedroom hotel including plant room on the roof, the excavation of a basement to provide car parking and associated landscaping.

**Decision:** 19-02-2019 Approved

43794/APP/2021/3685 Axis House 242 Bath Road Sipson

Formal Description: Erection of a hotel and associated access, parking, landscaping and refuse storage.

Further Details: Erection of a 157-bedroom hotel of varying height up to 7 storeys, with 23 parking spaces, access from Egerton Way, landscaping and refuse storage.

**Decision:**

**Comment on Relevant Planning History**

Starting with the prior approval application to convert Axis House into 64 flats (Ref: 43794/APP/2014/1542), approved on the 24th of May 2014, the entire parking area around Axis House was safeguarded as a parking area for these new flats. This was shown on plan (Drawing No. PL103) and secured by Condition 1, which required details of a maximum of 64 car parking spaces, including 6 disabled parking bays and 6 motorcycle parking spaces to be submitted. This condition was never discharged.

Following the refusal of an application to convert the roofspace of Axis House on the 20th of July 2015 into 8 additional flats (Ref: 43794/APP/2015/1272), a subsequent application to convert the roofspace into 7 flats was approved, as external amenity space provision was now going to be provided and the units were now of an acceptable standard internally (Ref: 43794/APP/2015/2822). This permission was subject to condition 4 (Landscaping) and 5 (Parking Allocation Scheme), and relevant details were submitted and approved on the 31st of October 2016 (under reference 43794/APP/2016/2814). The submitted and approved plan (PL103 Rev. G) under that application shows an external amenity area adjacent to Egerton Way, 80 car parking spaces across the site, with 8 specifically allocated in the south-west corner for the additional 7 units approved under the full application (Ref: 43794/APP/2015/2822).

The above planning history relates to the entire site (both the western and eastern halves which have subsequently been separated), however is relevant as it establishes the level of parking which should have been safeguarded for the residents of Axis House. Based on first hand accounts and written responses to consultations on this application, it seems as though parking has never been provided in the levels it was secured at, and at most, 30 car parking spaces have been provided for the existing residents of Axis House.

In this respect, several applications to build a hotel on the western half of the site have been approved since Axis House was converted, although all permissions have now lapsed, and the first application for a hotel in this location (Ref: 43794/APP/2016/4445) was explicitly refused because "the proposal has not demonstrated that sufficient off street parking would be provided for the residents of Axis House residents and visitors and users of the hotel", with 30 spaces retained for Axis House as part of that refused application. A subsequent application was submitted (Ref: 43794/APP/2017/3704), which was approved on the 11th of October 2017, and which overcame this previous reason for refusal by re-providing 30 spaces at surface level in addition to 20 basement spaces (50 total), which was considered an acceptable compromise.

The most recently approved hotel application (Ref: 43794/APP/2018/2779), approved on the 3rd of April 2019, was for a 6-storey, 157-bedroom hotel, which also re-provided 50 parking spaces for the residents of Axis House. This most recently approved application is similar to a current live planning application (Ref: 43794/APP/2021/3685), with the main difference being the addition of one more storey (to seven storeys) and the removal of the basement level, with 30 spaces proposed to be retained for current residents of Axis House.

#### 4. **Planning Policies and Standards**

Planning law requires that applications for planning permission be determined in accordance with the development plan unless material considerations indicate otherwise.

The Development Plan for the London Borough of Hillingdon consists of the following documents:

The Local Plan: Part 1 - Strategic Policies (2012)  
The Local Plan: Part 2 - Development Management Policies (2020)  
The Local Plan: Part 2 - Site Allocations and Designations (2020)  
The London Plan (2021)  
The West London Waste Plan (2015)

The National Planning Policy Framework (NPPF) (2021), Planning Practice Guidance, as well as relevant supplementary planning documents and guidance are all material consideration in planning decisions.

The proposed development has been assessed against development plan policies and relevant material considerations.

#### **Local Plan Designation and London Plan**

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment  
PT1.EM11 (2012) Sustainable Waste Management  
PT1.EM8 (2012) Land, Water, Air and Noise

Part 2 Policies:

DMAV 2 Heathrow Airport  
DME 5 Hotels and Visitor Accommodation  
DMHB 11 Design of New Development  
DMT 1 Managing Transport Impacts  
DMT 2 Highways Impacts  
DMT 6 Vehicle Parking  
LPP D11 (2021) Safety, security and resilience to emergency  
LPP D14 (2021) Noise  
LPP D5 (2021) Inclusive design  
LPP D6 (2021) Housing quality and standards  
LPP S11 (2021) Improving air quality  
LPP S17 (2021) Reducing waste and supporting the circular economy  
LPP T1 (2021) Strategic approach to transport  
LPP T2 (2021) Healthy Streets  
LPP T4 (2021) Assessing and mitigating transport impacts  
LPP T6 (2021) Car parking  
LPP T6.1 (2021) Residential parking

## 5. Advertisement and Site Notice

5.1 Advertisement Expiry Date:- Not applicable

5.2 Site Notice Expiry Date:- Not applicable

## 6. Consultations

### External Consultees

#### METROPOLITAN POLICE:

No response received at the time of writing the report. Any response will be reported in the Addendum.

#### TfL (28-11-22):

The applicant has identified that the existing spaces in the car park are underutilised. As such, this proposal is seeking to secure approval for these spaces to be changed into a paying public car park. It is noted that the applicant has said residents at Axis House will still be able to park there but provided no detail on how this will be managed.

As highlighted within the submitted material, the site is within an area of PTAL 4 and has good accessibility to bus services, as such the focus should be on promoting sustainable and active travel to this area rather than facilitating private vehicle use.

Policy T1 of the London Plan states that development proposals should facilitate the delivery of the Mayor's strategic target of 80 per cent of all trips in London to be made by foot, cycle or public transport by 2041. This proposal would continue to facilitate and promote private vehicle movement within this area, counter intuitive to Policy T1.

As identified within Policy T2 of the London Plan, proposals should seek to reduce the dominance of vehicles on London's streets whether stationary or moving. The applicant has said that existing residents at Axis House will still be able to park at the site, but provided no detail on how this will be managed. In the absence of this information, it is possible that residents utilising this parking provision could be displaced and park in the surrounding streets, thus increasing their car-dominance contrary to Policy T2. Furthermore, the promotion of the underutilised spaces to support additional vehicle movement in this area will increase the dominance of cars on the surrounding streets. As such, any underutilised parking spaces at this site should be repurposed into other uses, such as cycle parking, improved public realm and additional storage.

The applicant has stated that the 'proposed use would respond to significant demand for parking close to the airport for both workers and travellers' and that the parking area is 'better suited to catering for significant airport generated parking in the area'. Heathrow Airport benefits from excellent public transport access to all four terminals, including the recently opened Elizabeth Line. As such, as highlighted above and given the current policy context, the focus should be on facilitating sustainable and active travel to this area rather than by private vehicle.

Hillingdon Local Plan Policy T4 'Heathrow Airport' does not support airport operational development located outside the boundary of Heathrow Airport and also requires any development achieving sustainable mode shift for airport passenger journeys. As such, the provision of a pay and display car park to cater for workers and traveller at the airport is contrary to this policy.

In line with Policy T1 and T2, TfL objects to this application. Any surplus car parking should be repurposed into other uses, such as improved public realm and cycle parking provision.

## **Internal Consultees**

ACCESS OFFICER (14-09-22):

This proposed Change of Use from a residential to public car park does not raise an accessibility concern, subject to the appropriate number of accessible parking spaces designed to BS 8300-1:2018 in accordance with the London Plan.

AIR QUALITY OFFICER (16-11-22):

The proposed development is located within the LBH Air Quality Management area (AQMA), and within the A4 Corridor Focus Area (FA), bringing additional traffic emissions which will add to current poor air quality.

As per the London Plan and LBH Local Action Plan 2019-2024, developments need to be neutral as minimum and positive in Focus Areas, contributing to the reduction of air pollutant emissions in these sensitive locations. LBH requires new developments to incorporate air quality positive design measures from the outset and suitable mitigation measures to reduce pollution, especially in areas where the air quality is already poor (LBH Air Quality Local Action Plan 2019-2024), namely Focus Areas. Furthermore, policy DMEI 14 of the emerging London Borough of Hillingdon Local Plan (part 2), requires active contribution towards the continued improvement of air quality, especially within the Air Quality Management Area. Finally, the London Plan (March 2021) requires development to be air quality neutral as minimum and air quality positive in certain circumstances, actively contributing to reduce pollutant emissions to the atmosphere. The proposal is not supported by either an air quality report nor a transport assessment report and therefore it is not possible to ascertain whether the application meets regional and local policies nor the level to which the total emissions associated with the operation of the development need to be mitigated.

HIGHWAYS OFFICER (SUMMARY) (24-11-22):

The existing residential car park is directly in front of Axis House and is used by its residents for car parking, this established car parking would be lost if the site is changed to a public car park most likely resulting in residents' cars being displaced on-street. This could lead to errant/injudicious parking that increases the risk to road safety and impedes the free flow of traffic. This is an issue that the applicant has failed to address in their application. There are Highway objections to this proposal due to insufficient information, the Highway Authority requires the applicant to demonstrate how the existing parking requirements of the residents would be catered for if the car park were to become a public car park.

The application is contrary to the published London Plan 2021 Policy T2 Healthy Streets which requires the development proposals reduce, not increase "the dominance of vehicles on London's streets whether stationary or moving" and Policy T4 Assessing and mitigating transport impact which requires that development proposals "should not increase road danger".

NOISE OFFICER (10-11-22):

No noise impact assessment has been carried out in support of the proposed application. The timing of the use of the car park may reasonably be expected to change given the likelihood of its use by airport users and this may be more likely to disturb residents during the night-time hours of 2300 until 0700. Regular awakenings at night would be considered a significant noise effect. Whilst it might be possible to regulate through a condition, imposing restrictions on when it could be used, this would likely be inconsistent with its proposed use and would be difficult to enforce. I would recommend objection on noise grounds.

## **7. MAIN PLANNING ISSUES**

### **7.01 The principle of the development**

The principle of whether the change of use of the car park is acceptable rests on whether the impacts to existing residents would be significant and the potential impacts on the surrounding highway network. The impacts of this proposed change of use are discussed in the relevant sections below (Impact on Neighbours, Traffic Impact, Access and Security, Disabled Access, Trees and Landscaping, Sustainable Waste Management, and Noise and Air Quality).

**7.02 Density of the proposed development**

Not relevant to this application.

**7.03 Impact on archaeology/CAs/LBs or Areas of Special Character**

The proposal would not have an impact on any heritage assets.

**7.04 Airport safeguarding**

The proposal would have no impact on airport safeguarding.

**7.05 Impact on the green belt**

The proposal would have no impact on the Green Belt or Metropolitan Open Land.

**7.07 Impact on the character & appearance of the area**

The proposal would have no impact on the character and appearance of the area.

**7.08 Impact on neighbours**

Policy DMHB 11 of the Hillingdon Local Plan: Development Management Policies and Policy BE1 of the Hillingdon Local Plan: Strategic Policies both seek to ensure that new development does not adversely impact on the residential amenity of neighbouring properties.

Policy D14 of the London Plan (Noise) states that new noise and other nuisance-generating development proposed close to residential and other noise-sensitive uses should put in place measures to mitigate and manage any noise impacts for neighbouring residents.

Additionally, Policy EM8 of the Hillingdon Local Plan: Strategic Policies (Land, Water, Air and Noise) states the council will promote the maximum possible reduction in noise levels and will minimise the number of people potentially affected by new developments. As such, the council will seek to ensure that noise sensitive development and noise generating development are only allowed if noise impacts can be adequately controlled and mitigated.

Furthermore, whilst mainly discussing design for new housing, the Mayor's Housing SPG is relevant in that it outlines a number of design principles which should be adhered to, to achieve a high standard of accommodation. In particular, para. 2.3.7 sets out that noise, privacy and air quality issues should be carefully considered for ground floor flats with windows facing directly onto busy roads or adjacent non-residential land uses or activities. Standards 28 (Privacy) and 30 (Noise) further highlight the importance of a home providing a sense of retreat, and outline that habitable rooms within dwellings be provided with an adequate level of privacy in relation to the street and other publicly accessible spaces, highlighting specifically that noise from the street and adjoining properties can cause stress, sleep disturbance and lead to conflicts between residential occupants and non-residential users if not managed carefully.

As the proposed change of use would allow members of the public into an existing residential car park whilst continuing to serve existing residents, it is inevitable that if the application were approved, demand for spaces will increase, which in turn would lead to more vehicles entering and exiting the site on a regular basis. In addition to more vehicle movements, the timings of these comings and goings are more likely to be at unsociable hours, possibly associated with early or late flights from Heathrow, which means that even



if there were less overall vehicle movements, the impacts would still be significant.

This would be felt especially for the ground floor units facing north and west, which have habitable windows abutting the footpath at the southern and eastern edges of the car park, who would be very sensitive to the flashing of car lights, noise associated with the loading and unloading of cars with luggage and general disturbance within 5 metres of their windows. In turn, there is the potential for this to lead to future conflicts between members of the public and existing residents, and lead to a poorer living environment.

On this basis, it is considered that the introduction of a public paid car park in place of an existing residential car park which is both visually and physically linked to the residential building would significantly worsen existing levels of amenity for current residents in terms of noise, disturbance and privacy, contrary to the intentions of Policy D14 of the London Plan and Policies EM8 and DMHB 11 of the Hillingdon Local Plan Parts 1 and 2.

#### **7.09 Living conditions for future occupiers**

Not relevant to this application.

#### **7.10 Traffic impact, Car/cycle parking, pedestrian safety**

Policy T1 of the London Plan (Strategic approach to transport) seeks development proposals to facilitate the delivery of the Mayor's strategic target of 80 per cent of all trips in London to be made by foot, cycle or public transport by 2041. All development should make the most effective use of land, reflecting its connectivity and accessibility by existing and future public transport, walking and cycling routes, and ensure that any impacts on London's transport networks and supporting infrastructure are mitigated.

In addition, Policy T2 of the London Plan (Healthy Streets) requires development proposals to demonstrate how they will reduce the dominance of vehicles on London's streets whether stationary or moving, be permeable by foot and cycle and connect to local walking and cycling networks as well as public transport.

Moreover, Policy T4 of the London Plan (Assessing and mitigating transport impacts) sets out that development proposals should reflect and be integrated with current and planned transport access, capacity and connectivity. When required, transport assessments or statements should be submitted with proposals to ensure that impacts on the capacity of the transport network (including impacts on pedestrians and the cycle network), at the local, network-wide and strategic level, are fully assessed. Policy T4 further explains that where appropriate, mitigation, either through direct provision of public transport, walking and cycling facilities and highways improvements or through financial contributions, will be required to address adverse transport impacts that are identified.

Policy T6 of the London Plan (Car Parking) states that car parking should be restricted in line with levels of existing and future public transport accessibility and connectivity. Car-free development should be the starting point for all development proposals in places that are (or are planned to be) well connected by public transport, with developments elsewhere designed to provide the minimum necessary parking ('car-lite'). Car-free developments have no general parking but should still provide disabled persons parking. The maximum standards for car parking outlined in the London Plan take account of PTAL as well as London Plan spatial designations and use classes, and the supporting text further outlines that developments in town centres generally have good access to a range of services within walking distance, and so car-free lifestyles are a realistic option for many people living there. Policy T6 further sets out that where parking is provided, it should reflect the current approach to parking and not be re-provided at previous levels where this exceeds the maximum parking standards.

Policy T6.1 of the London Plan (Residential parking) states that new residential

development should not exceed the maximum parking standards set out in Table 10.3. These standards are a hierarchy with the more restrictive standard applying when a site falls into more than one category. Policy T6.1 further sets out that for 3% of dwellings, at least one designated disabled persons parking bay per dwelling should be available from the outset and these spaces must be for residents' use only (whether M4(2) or M4(3) dwellings), not be allocated to specific dwellings, unless provided within the curtilage of the dwelling, and explains that these spaces should count towards the maximum parking provision for the development. Part (H) of Policy T6.1 specifically states that all disabled persons parking bays associated with a residential development must be for residents' use only.

At a Borough level, Policy DMT 1 of the Hillingdon Local Plan Part 2 (Managing Transport Impacts) requires proposals to address transport impacts in a sustainable manner, whilst maximising safe, convenient and inclusive accessibility to and from within developments for all people to ensure that proposals have no significant adverse transport or associated air quality and noise impacts on the local and wider environment, particularly the strategic road network. The supporting text to Policy DMT 1 highlights that the strategic road network is at capacity, leading to congestion and environmental impacts, with congestion hotspots identified in the Local Plan Part 1. Bath Road (The A4) is a major east-west arterial route through the borough and a key part of the strategic road network, as part of the main route into and out of Heathrow Airport, and the junction between the M4, A4 and A408 (approximately 400 metres to the west of the application site) has been identified as a congestion hotspot, experiencing particularly high levels of traffic and associated environmental impacts. As such, proposals will be expected to include measures that do not contribute to further congestion and where possible, seek to reduce car use in the borough.

Additionally, the Council will not support development which unacceptably contributes to traffic movements, especially on the strategic highway network, and proposals which adversely impact on road user safety (including pedestrians) or affect residential amenity through noise, congestion or inadequate parking provision will also be resisted. Parking areas, where they are provided, must be safe and provide adequate vehicular access, servicing and parking areas.

Policy DMT 6 of the Hillingdon Local Plan Part 2 (Vehicle Parking) sets parking standards higher than those of the London Plan, however as the London Plan was adopted more recently and the PTAL is 4, these higher standards are no longer appropriate. Policy DMT 6 does, however, reiterate that all car parks provided for new developments will be required to contain conveniently located reserved spaces for wheelchair users and those with restricted mobility, and car parking areas must have bays of appropriate dimensions and with sufficient manoeuvring space, be well lit, safe and secure, and landscaped to a high standard, with provision for electric vehicle parking.

The supporting text to Policy DMAV 2 of the Hillingdon Local Plan Part 2 (Heathrow Airport) sets out that both land and air transport activity, at and around Heathrow Airport, are significant contributors to traffic congestion, noise and emissions in the Heathrow Area. Development directly related to the airport should be located within the airport's boundaries to ensure that environmental impacts associated with Heathrow's operation are managed. The supporting text to Policy DMAV 2 further sets out that proposals related to the airport but operating outside the boundary (such as car parking) can contribute to air quality issues and traffic congestion and should be carefully managed on this basis.

This is further expanded on in the supporting text to Policy DME 5 of the Hillingdon Local

Plan Part 2 (Hotels and Visitor Accommodation), which outlines that because the Heathrow area is subject to specific demand for land uses directly related to the airport, including a mix of industrial uses, offices and hotels, the surrounding area is also subject to pressure for off-site airport-related car parking, which should be resisted because of the sustainability and air quality objectives of the Local Plan.

As the planning history for the site indicates, parking provision was secured for the residents of Axis House at much higher levels than were ever provided on-site, with 64 spaces secured under the prior approval application to convert Axis House into 64 flats (Ref: 43794/APP/2014/1542) and an additional 8 spaces secured for the 7 flats approved when the roofspace was extended (Ref: 43794/APP/2015/2822). Whilst 80 spaces were then shown on a subsequent plan to approve details, given that Axis House only contains one- and two-bedroom flats, it is reasonable to expect that a 1:1 ratio should have been provided following implementation of the two permissions and 71 spaces should have been provided. It was indicated on plan that this would include 6 wheelchair accessible spaces.

Nonetheless, it is important to have regard to the Policy T6 of the London Plan, and, based on current London Plan standards for a site with a PTAL of 4 for a development of 71 one- and two-bedroom flats, between 36 and 53 parking spaces would be the maximum allowed, based on 0.5 - 0.75 spaces per flat. In accordance with Policy T6.1 of the London Plan, applying the more restrictive parking standard when a site falls into more than one category, it is considered that the application site has a PTAL of 4, which is considered to represent a good level of access to public transport (on a scale of 0-6b, where 0 is the worst and 6b is the best). However, it should be noted that the good PTAL is slightly deceptive for this part of Bath Road as whilst the Underground Stations within Heathrow Airport are relatively close to the application site, and these improve the wider area's PTAL, there is no pedestrian or bicycle access to Heathrow Airport from Bath Road and these stations can only be reached by car or bus which somewhat restricts ease of access, contrary to the assertion of the submitted Planning Statement which claims that the site "is within walking distance of Heathrow Airport". It should be further noted that the reliance on buses, at least for immediate travel to a connecting interchange, means that actual journey times are often longer than would be expected because of the particularly busy junction of the M4, A4 and A408, which leads to very high levels of congestion in the immediate area and drastically increases what should be relatively short journey times.

The plans submitted with this application do not accurately depict existing site conditions, showing a total of 30 car parking spaces, none of which are wheelchair accessible and omitting key features of the existing (and presumably proposed) car park such as signage, electronic barriers, and bike and bin stores. The submitted plans show four car parking spaces (numbered 01 to 04) next to the existing entrance of the car park whereas they should show the two wheelchair accessible bays and one standard bay which are currently there, albeit all of these bays are inaccessible and cannot be used at present because of the placement of the electronic barrier within the site, raiseable bollards to the front of these spaces and because bins appear to be stored externally within the car park in place of a parking bay, instead of in the designated bin store. These are the only two wheelchair accessible spaces which appear to be present on site at the moment, contrary to the level agreed under the previous applications (6), which means that at some point after permission was granted, 4 of the pre-existing 6 wheelchair accessible bays which were on site and should have been retained were converted into 6 standard bays (numbered 05 to 10 on the plans). Further, the parking bays located closest to the site exit are unusable because they are too narrow to accommodate cars and the second electronic barrier has also been placed in a way which means these spaces are inaccessible and yet they are shown on the existing and proposed plans as contributing to

the total number of parking bays.

Taking the above into account, it is considered that the existing residents of Axis House have access to 24 parking spaces, none of which are wheelchair accessible. This is below the reported number of 30 within the submitted information, and below the number secured through previous planning consents. Moreover, both the existing and proposed plans (which are virtually identical) fail to accurately demonstrate what existing conditions are like, and what they would be if permission was granted, given that they indicate no change to the layout but are not accurately depicting the site at present.

In addition to the inaccuracy of the drawings, the 'Design and Access / Planning Statement' submitted in support of the application does not include anywhere near the level of detail which would be expected to justify this change of use, with no quantitative assessment of the existing level of car park usage, average spare capacity, existing or proposed pricing structures for existing residents or members of the public, or an assessment of the impact of additional vehicle movements on the surrounding road network having regard to the very high levels of congestion in the area at present.

In this regard, the Planning Statement sets out that the applicant "seeks to have the car-park available for public use as it is his experience that it has been under used over recent years with many spaces being vacant much of the time... [and Axis House has a] ...transient resident population (being home to many contract workers) and has much lower car ownership levels than more traditional residential areas". No evidence has been submitted to support this claim, however based on the number and type of comments received in response to public notification from current residents, it is considered that existing demand is actually very high. Additionally, whilst only a snapshot in time, historic photos of the site appear to show the car park very well used since the office was converted into residential flats, with nearly all available spaces in use, alongside cars parked on the kerbs of the footpaths within the site, which has been corroborated as a relatively common occurrence by existing residents and is indicative of an inadequate number of car parking spaces.

The Planning Statement further sets out that because parking is at a premium around the airport, the proposal seeks to respond to this demand, but that residents of Axis House would be able to continue using it as a residential car park. It is not clear how this would work in practice, as residents would usually be leased a specific bay (or there would be the same number of residential permits as available bays), to ensure the car park is not oversubscribed, as this could lead to unsafe parking inside the site (as appears to happen now) or parking on the surrounding roads which already experience very high levels of parking stress. It is not clear who would get priority if the car park was oversubscribed or what realistic alternative options for existing residents with a vehicle would be in the event their car park was full and this could lead to future conflicts. It is also not clear if existing residents would be charged at the same or similar levels to members of the public. Paid public car park rates are comparatively very high compared to residential parking within a site (which is often free) and therefore existing residents may be priced out of their own on-site parking if opened up to the public, which would act to artificially drive down use by existing residents who would likely be unwilling or unable to pay premium airport rates year-round, in the way in which holidaymakers would for 1-3 weeks at a time. In this regard, Policy T6 of the London Plan sets out that a Parking Design and Management Plan (PDMP) should have been submitted to explain how the car parking area will be managed, making reference to Transport for London guidance. However, as no PDMP has been submitted, and it would not be appropriate to secure this by condition as the long-term management of the site is critical to the acceptability of the proposed change of use, the proposed shared-use of the car park is not considered feasible.

Notwithstanding the generally poor quality of the submission and the inaccuracy of the drawings, it is assumed that there is no intention to alter the layout of the parking area and that it would remain as existing, including the barriers and bollards which are not shown on plan, and that the proposal relates purely to the change of use with the addition of some soft landscaping around the peripheries of the site. The existing layout allows 24 cars to be parked in designated bays, with both of the wheelchair accessible spaces and the standard bay next to the entrance inaccessible (because of the barrier, bollards and bins), together with the two bays closest to the site exit (which are inaccessible because they are too narrow and because of the exit barrier).

Assessed against the current London Plan standards, the maximum level of on-site parking allowed would be 53, including an allowance for 10% of flats to have access to an accessible bay, amounting to 7 for 71 flats. The current provision of 24 usable parking spaces, including 0 wheelchair accessible bays, falls far below the maximum allowed, and the lack of access to any disabled parking bays is completely unacceptable. There is clearly scope within the wider Axis House car park to accommodate the amount originally secured (71), or to accommodate a reduced number based on London Plan standards (53), however the current provision fails to provide even a 0.5 parking ratio and has in no way been justified as an acceptable amount. Whilst it is understood that the western part of the car park has been sold and is now under separate ownership to the eastern half (denoted by the red line boundary for this application), land ownership issues are not relevant to the planning process and splitting the ownership of this one plot does not override the need to comply with planning conditions which relate to the whole site, and does not automatically mean that they are two distinct planning units, when they are not.

As such, given that there are already very significant concerns that the existing level of parking provision is insufficient to accommodate the needs of the existing residents of Axis House, the introduction of additional demand for spaces as a result of opening the site as a paid public car park would exacerbate what appears to be existing high levels of demand. In turn, this could lead to future conflicts between residents and other users of the car park as they jostle for available spaces, as well as creating tensions between residents who have access to a space and those who have not. There is also the potential for very high levels of displacement onto the surrounding roads, which appears to be common already, which could have knock-on effects on the strategic road network and worsen high levels of congestion in the area.

In any event, the Local Plan is clear that development associated with the airport should be restricted to within the airport's boundaries, and this includes car parking. The submitted information outlines that the target market for the car park would be airport users (workers or tourists), and it is therefore considered that the proposal would have direct links to the operation of the airport and is therefore an inappropriate location for this type of development. Even if it was argued that the public car park was for general use, its proximity to the airport would inevitably attract airport users and the link would always be more than incidental.

Whilst there is no assessment of the impact of the change of use on the surrounding highway network within the submission, it is likely that allowing more opportunities for general public parking in an area known to have very high levels of congestion would lead to a higher number of vehicle movements to and from the site on a weekly basis, and would attract an additional number of vehicle movements to the area simply by existing. This would be contrary to established planning policies which seek to reduce the reliance on the private car, especially in areas of known transport or air quality concern, such as is the case here.

Overall, the proposal is considered wholly unacceptable in terms of its impacts on the surrounding local and strategic highway network with virtually no justification put forward to support the proposed change of use. On this basis, the proposal is recommended for refusal.

#### **7.11 Urban design, access and security**

Policy D11 of the London Plan (Safety, security and resilience to emergency) sets out that boroughs should work with the Metropolitan Police Service's 'Design Out Crime' Officers to identify the community safety needs and necessary infrastructure to maintain a safe and secure environment and reduce the fear of crime. Proposals should seek to maximise building resilience and minimise potential physical risks, and should include measures to design out crime that deter terrorism, assist in the detection of terrorist activity and help mitigate its effects. These measures should be considered at the start of the design process to ensure they are inclusive and aesthetically integrated into the development and the wider area. Measures to design out crime, including counter terrorism measures, should be integral to proposals, taking into account the principles contained in guidance such as the Secured by Design (SBD) Scheme published by the Police.

Policy BE1 of the Hillingdon Local Plan: Strategic Policies (Built Environment) encourages the creation of safe and secure environments that reduce crime and fear of crime, anti-social behaviour and risks from fire and arson, having regard to Secure by Design standards.

Sections 1(16) and 1(20) of the Secured by Design for Homes (2019) and the Secured by Design for Commercial Development (2015) guidance documents discuss the requirements for residential and commercial car parks. Both guidance documents set out that whilst not a requirement, publicly accessible car parks can demonstrate that they are safe places by showing that they can and will be awarded with a Park Mark Award through the Safer Parking Scheme, which is operated by the British Parking Association.

There is little information on how the site would be managed, and some concerns are raised that the existing car park would not be SBD accredited for residential uses, and the proposed shared residential and commercial use would almost certainly fail against SBD standards. For example, the existing access is barrier controlled with a code available to residents, which presumably would be shared with members of the public. In turn this would either result in a high number of people able to get access to the site (if the code remains unchanged) or the code will need to be changed on a regular basis which is not practical for existing residents. Similar problems arise if fob access is introduced, as members of the public would have temporary access to at least the residential car park, and would likely be able to gain access to the building, assuming the same fob would be used, as multiple fobs would, again, be impractical. There is also no information on whether additional security measures would be needed, such as CCTV or lighting, and how these would affect the residential uses within the building.

The safety and security of the site as a whole has been raised as a significant concern of the existing site (with reports of a broken main door and thefts from within the lobby), and the introduction of further non-residents onto the site without first addressing current SBD concerns would be premature.

Essentially, taking the SBD guidance in the round, it is considered that there would be too many practical on-site security issues which would arise if the car park were used by both residents and members of the public which would be very difficult to effectively manage. This would, at the very least, increase the perception of an unsafe environment for existing residents and has the potential to lead to increased opportunities for crime and anti-social behaviour. On this basis, the proposal is considered to be unacceptable.

## **7.12 Disabled access**

Policy D5 of the London Plan (Inclusive design) sets out that proposals should achieve the highest standards of accessible and inclusive design by providing high quality people focused spaces that are designed to facilitate social interaction and inclusion, be convenient and welcoming with no disabling barriers, providing independent access without additional undue effort, separation or special treatment, and be able to be entered, used and exited safely, easily and with dignity for all.

The correct number of disabled spaces has never been provided for existing residents, with only two wheelchair accessible spaces present on site at the moment, contrary to the level agreed under the previous application which are in any event, inaccessible, because of the layout.

Approval of this scheme would legitimise the existing lack of wheelchair accessible spaces available for residents, who should be provided for regardless of the outcome of this current application, and additionally the proposal has the potential to result in no car spaces available for residents (in the event that all spaces are occupied by paying members of the public) which raises very significant accessibility concerns. A number of comments received on this application outline that some existing residents have mobility issues and a car is essential for their day-to-day functions, and therefore the potential for the complete removal of all parking bays for residents raises significant concerns.

As with other aspects of the proposal, access for existing residents does not appear to have been considered prior to submission or as part of the proposed change of use, and non-compliance with the disabled parking requirements forms a reason for refusal.

## **7.13 Provision of affordable & special needs housing**

Not relevant to this application.

## **7.14 Trees, landscaping and Ecology**

It is noted that the proposed plan includes an element of soft landscaping around the edges of the site. In this regard, whilst some soft landscaping would be welcome, it should be noted that SBD guidance highlights that a parking bay surrounded by vegetation or other obstructions (such as utility boxes or pay and display machines) may provide cover to interfere with vehicles. An encroaching or uncontrolled undergrowth can hinder natural surveillance, restrict access for the car user, impact on the fall of light from nearby columns and can also hinder any CCTV coverage. As such, shrubs should be selected to have a mature growth height no higher than 1 metre, and trees should have no foliage, shoots or lower branches below 2 metres, thereby allowing a 1 metre clear field of vision. This could be controlled by condition if all other aspects of the scheme were acceptable.

## **7.15 Sustainable waste management**

Policy SI 7 (Reducing waste and supporting the circular economy) and Policy D6 (Housing quality and standards) of the London Plan require developments to be designed with adequate, flexible, and easily accessible storage space and collection systems that support, as a minimum, the separate collection of dry recyclables (at least card, paper, mixed plastics, metals, glass) and food.

This is supported by the Mayor's Housing SPG which outlines that communal refuse and recycling containers, communal bin enclosures and refuse and recycling stores should be easily accessible to all residents including children and wheelchair users, and located on a hard, level surface. Refuse and recycling stores within buildings should further be located to limit the nuisance caused by noise and smells and maintained to a high hygiene standard.

At a borough level, Policy EM 11 of the Hillingdon Local Plan: Strategic Policies

(Sustainable Waste Management) states that the council will aim to reduce the amount of waste produced in the borough. To achieve this, the council will require all new developments to address waste management at all stages of a development's life from design and construction through to the end use and activity on site. This should be read alongside the requirements of Policy DMHB 11 of the Hillingdon Local Plan: Development Management Policies, which sets out that developments should make sufficient provision for well-designed internal and external storage space for general, recycling and organic waste, with suitable access for collection. In practice, this means residents should not have to travel further than 30 metres to the bin store from their flat, and bin stores should be located within 10 metres of the collection point, with unobstructed room to manoeuvre bins to and from the bin store on collection days.

As part of the approved scheme, enclosed refuse stores were proposed in the north-eastern corner of the site. Whilst these appear to be present (albeit one is in a slightly different location to the approved plans), they do not appear to be in use as refuse stores. Bins appear to be stored externally within the car park adjacent to the barrier, blocking access to at least one parking bay (alongside the barriers and bollards).

As such, given that the site is not currently functioning as intended in terms of refuse storage and collection, it is difficult to assess whether the introduction of paying members of the public onto the site would significantly affect waste management across the site. If, as proposed, all spaces could be accessed (which would in any event involve repositioning of the entrance and exit barrier), the external bins which currently take up one space would need to be stored elsewhere. Proper management of the building could alleviate this concern, however there is no indication of whether the proposal would affect refuse storage and collection within the submitted documents, and whether there is a strategy in place for existing residents. This is indicative of poor site management however does not form a reason for refusal.

#### **7.16 Renewable energy / Sustainability**

Not relevant to this application.

#### **7.17 Flooding or Drainage Issues**

Not relevant to this application.

#### **7.18 Noise or Air Quality Issues**

Policy SI 1 of the London Plan (Improving air quality) states that proposals should not lead to further deterioration of existing poor air quality or create any new areas that exceed air quality limits, or delay the date at which compliance will be achieved in areas that are currently in exceedance of legal limits, so as not to create unacceptable risk of high levels of exposure to poor air quality.

As such, as a minimum, proposals should be at least Air Quality Neutral and should use design solutions to prevent or minimise increased exposure to existing air pollution, whilst making provision to address local problems of air quality in preference to post-design or retro-fitted mitigation measures. Additionally, Policy EM8 of the Hillingdon Local Plan: Strategic Policies (Land, Water, Air and Noise) requires all development to protect both existing and new sensitive receptors, and not result in the deterioration of local air quality.

The application site falls within the borough's Air Quality Management Area (AQMA) and the A4 Corridor Air Quality Focus Area (AQFA). In accordance with Policy SI 1 of the London Plan and Policy EM8 of the Hillingdon Local Plan, developments need to be neutral as minimum in general and air quality positive in Focus Areas, contributing to the reduction of air pollutant emissions in these sensitive locations. New developments within the borough are expected to incorporate air quality positive design measures from the outset and suitable mitigation measures to reduce pollution, especially in areas where the



air quality is already poor, namely Focus Areas.

Whilst the submission does not discuss whether there would be an increase in vehicular movements associated with the shared residential and public use of the car park and what the impacts of this would be, the proposed change of use is likely to result in a greater number of vehicle movements to and from the site and attract more vehicles to an already busy and heavily congested area. This is likely to lead to some worsening of air quality associated with vehicle emissions, including from vehicle exhaust fumes in close proximity to the flats within Axis House and a worsening of air quality in the immediate area.

As no assessment has been made by the applicant, it is difficult to predict the level of harm to local air quality (beyond the application site), however because of the site's inclusion within the A4 AQFA, the proposal should demonstrate an air quality positive approach and any worsening of air quality will be resisted. On the basis that no information has been provided to assess air quality impacts, this will form a reason for refusal.

#### **7.19 Comments on Public Consultations**

Letters dated 14-09-22 were sent to 81 nearby properties, including properties on Doghurst Avenue and within Axis House.

44 objections were received following public notification, raising the following (summarised) material considerations:

- Proposal is in violation of previous permission and agreements
- Only half of the full parking provision has ever been provided
- Parking should be retained for residents only
- Loss of parking spaces
- Previously assigned car parking spaces have been withdrawn
- Car parking area is also used as an external amenity space by residents
- On-street parking is not possible as surrounding roads are very busy or have controls
- On-street parking is largely occupied by taxis
- Parking is not underused, it is oversubscribed and in high demand from residents
- Proposed fees for parking are not reasonable for residents
- There are suitable alternative public car parks nearby
- Increase in noise
- Increase in air pollution
- Increase in health risks
- Increase in anti-social behaviour
- Loss of security
- Loss of privacy
- Loss of accessibility

These are discussed in the relevant sections above, however, in general, these concerns are shared by the local planning authority and form reasons for refusal.

Other objections relating to land ownership and lease agreements are not material considerations, however this has still been addressed in the 'Other Issues' section, below.

#### **7.20 Planning obligations**

Not relevant to this application.

#### **7.21 Expediency of enforcement action**

Following the determination of this application by committee, and if refused in line with the officer's recommendation, it is considered that the applicant may be subject to formal planning enforcement proceedings as a means to ensuring that if the proposed change of

use has already been implemented then this use stops and the car park is returned to residential uses only.

Additionally, as noted from the Planning History of the site, none of the original permissions to extend and convert Axis House into residential flats were implemented in accordance with the approved details, which required 71 spaces to be provided on a 1:1 ratio for any residents of Axis House, and the council may initiate formal planning enforcement proceedings to remedy this if it is deemed to be in the public interest.

## **7.22 Other Issues**

Under Article 14 of the Town and Country Planning (Development Management Procedure) (England) Order 2015 (as amended), an applicant is required to declare whether they are the sole owner of all of the land to which the application relates, and sign the relevant certificate and serve notice accordingly. As part of the application, Certificate B has been signed, and it is stated that the applicant has served the required notice on two owners (or agricultural tenants) of the land to which this application relates.

The number of comments received from landlords who own and lease out properties within Axis House indicates that the ownership of all of the land within the application site has not been correctly identified as there appear to be more than two landlords, and other comments indicate that some residents have purchased and own their flats.

As such, whilst not a reason for refusal, the applicant is advised that had permission been granted and it was later found that the correct notices had not been served, the permission would have been invalid and would not have been implementable.

## **8. Observations of the Borough Solicitor**

### **General**

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

### **Planning Conditions**

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

### **Planning Obligations**

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related

to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

#### Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

#### **9. Observations of the Director of Finance**

N/A.

#### **10. CONCLUSION**

The proposal is considered to be an unacceptable land use in this location, compromising what appears to already be insufficient residential parking for 71 flats whilst reducing residential amenity, increasing the perception of an unsafe environment and potentially leading to a significant amount of displacement parking into the surrounding roads, which include the Strategic Road Network and areas identified as having very high levels of congestion.

On this basis, the proposal is recommended for refusal.

#### **11. Reference Documents**

The London Plan (2021)  
Mayor's Housing SPG (2016)

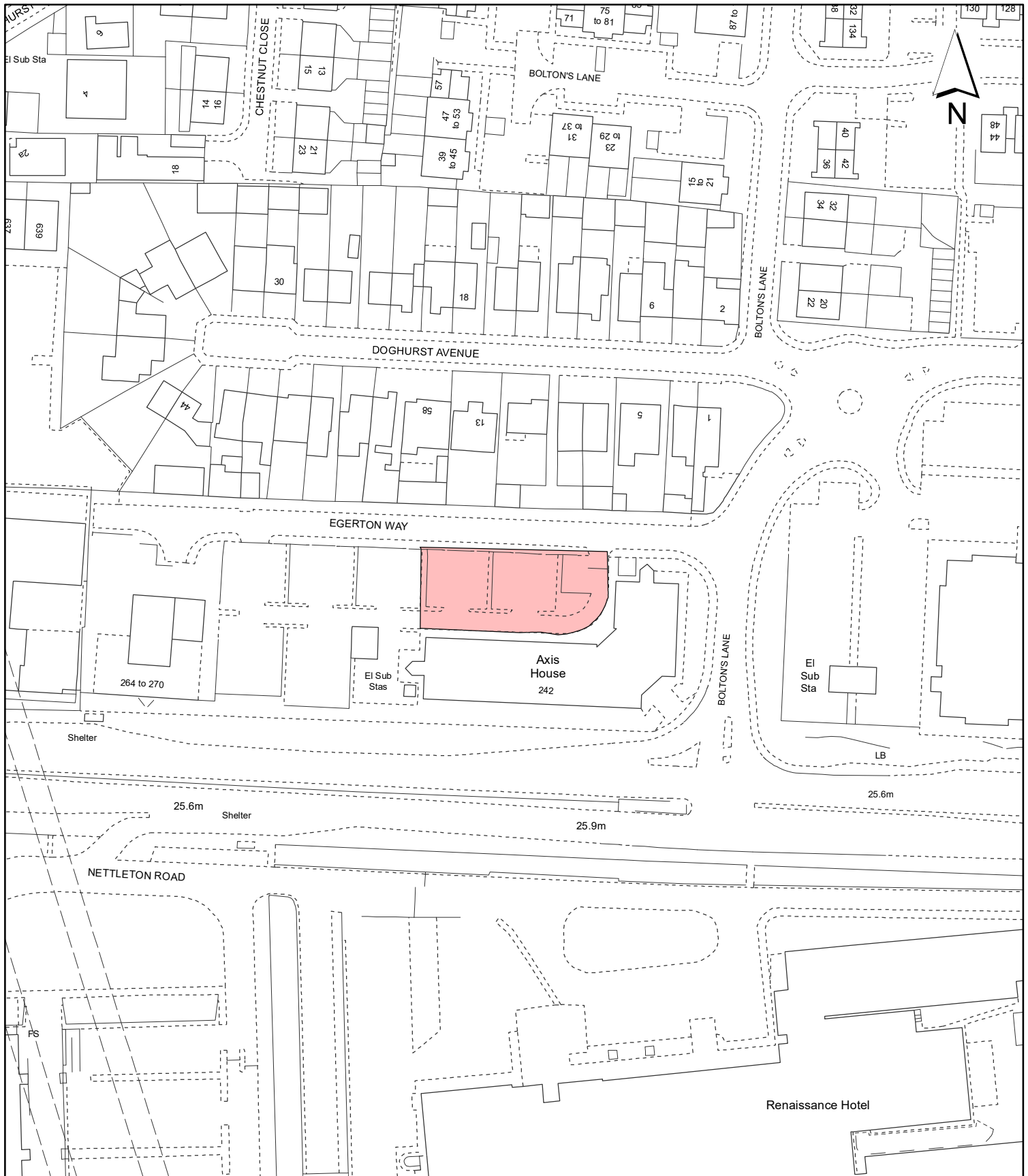
The Local Plan: Part 1 - Strategic Policies (2012)  
The Local Plan: Part 2 - Development Management Policies (2020)  
The Local Plan: Part 2 - Site Allocations and Designations (2020)  
The West London Waste Plan (2015)

NPPF (2021)

Secured by Design for Homes (2019)  
Secured by Design for Commercial Development (2015)

**Contact Officer:** Andrew Thornley

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

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Site Address:

**Axis House  
 242 Bath Road**

**LONDON BOROUGH  
 OF HILLINGDON**  
 Residents Services  
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:  
**43794/APP/2022/2500**

Scale:  
**1:1,250**

Planning Committee:  
**BoroughPage 148**

Date:  
**December 2022**



A

**Item No.**            **Report of the Interim Director of Planning, Regeneration & Public Realm**

**Address**            6 GLEBE AVENUE ICKENHAM

**Development:**    Subdivision of premises into two to provide one barber shop (Class E) and one beauty salon (Sui Generis), and alterations to shopfronts including the installation of a canopy (retrospective application)

**LBH Ref Nos:**     2015/APP/2022/1894

**Drawing Nos:**    Location Plan  
06 001 - rev 02 dated Nov 22

**Date Plans Recieved:**    10/06/2022                      **Date(s) of Amendment(s):**    10/06/2022

**Date Application Valid:**    08/08/2022

## 1. SUMMARY

This retrospective application seeks planning permission to sub-divide the premises into two to provide one barber shop (Class E) and one beauty salon (Sui Generis). Alterations to the shopfront also require planning permission, and involve an installed canopy/awning and an additional door. These works and change of use were carried out in November 2020. The premises was previously a sandwich bar known as 'Pickles'.

The uses are considered complementary to the variety of shops and services in the parade, and the principle of development is considered acceptable. No harm has been identified with regards to the material considerations discussed in this report.

The replacement shopfront with two doors to the subdivided unit is considered acceptable as the grey framing and fenestration is consistent, and the break line between the two units is minimal. This allows for the brick shopfront pilasters to dominate, and the rhythm of shopfronts across the terrace is not significantly affected. The awning proposed for retention also unites the shopfronts. Therefore the appearance of the shopping parade and character of the Ickenham Village Conservation Area would be preserved.

As such the application is being recommended for approval subject to conditions.

## 2. RECOMMENDATION

**APPROVAL subject to the following:**

### 1            COM4            **Accordance with Approved Plans**

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers untitled Location Plan, 06 001 - rev 02 dated Nov 22 and shall thereafter be retained/maintained for as long as the development remains in existence.

#### REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan Parts 1 (November 2012) and 2 (January 2020) and the London Plan (2021).

### 2            A20            **Access to Buildings for People with Disabilities**

Within 3 months from the date of this planning permission the shopfronts hereby approved shall be modified to acheive step-free access for wheelchair users from the

public footway, via a profiled threshold or water bar not exceeding 15 mm in height and a doorway width of no less than 1000 mm, with all such features remaining in place for the life of the building.

**REASON**

To ensure an Accessible and Inclusive development for everyone in accordance with London Plan Policy D5.

**3 COM22 Operating Hours**

The premises hereby approved shall not be open for customers outside of the following hours:-

09:00 to 20:00, Mondays - Sundays.

**REASON**

To safeguard the residential amenity of the occupiers of adjoining and nearby properties in accordance with Policy DMHB 11 and DMTC 4 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020).

**4 NONSC Non Standard Condition**

The premises shall not be used for deliveries and collections, including waste collections other than between the hours of 08:00 and 18:00, Mondays to Fridays, 08:00 to 13:00 Saturdays and not at all on Sundays and Bank and Public Holidays.

**REASON**

To safeguard the residential amenity of the occupiers of adjoining and nearby properties in accordance with Policies DMHB 11 and DMTC 4 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020).

**INFORMATIVES**

**1 I52 Compulsory Informative (1)**

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

**2 I59 Councils Local Plan : Part 1 - Strategic Policies**

On this decision notice policies from the Councils Local Plan: Part 1 - Strategic Policies appear first, then relevant Local Plan Part 2 (2020), then London Plan Policies (2021). Hillingdon's Full Council adopted the Hillingdon Local Plan: Part 1 - Strategic Policies on 8 November 2012 and the Hillingdon Local Plan Part 2 on 16 January 2020.

**3 I70 LBH worked applicant in a positive & proactive (Granting)**

In dealing with the application the Council has implemented the requirement in the National Planning Policy Framework to work with the applicant in a positive and proactive way. We have made available detailed advice in the form of our statutory policies from Local Plan Part 1, Local Plan Part 2, Supplementary Planning Documents, Planning Briefs and other informal written guidance, as well as offering a full pre-application advice service, in order to ensure that the applicant has been given every opportunity to submit an application which is likely to be considered favourably.

**4**

This permission does not authorise the display of advertisements or signs, separate consent for which may be required under the Town and Country Planning (Control of Advertisements) Regulations 1992. To display an advertisement without the necessary consent is an offence that can lead to prosecution. For further information and advice, contact - Residents Services, 3N/04, Civic Centre, High Street, Uxbridge, UB8 1UW (Tel. 01895 250574).

### **3. CONSIDERATIONS**

#### **3.1 Site and Locality**

This application site is occupied by a barbers and a beauty salon, situated within Ickenham Local Centre, on a small retail parade on Glebe Avenue. The site was previously occupied by a sandwich bar but was subdivided in November 2020 to accommodate a barbers and beauty salon. It is these uses and associated shop front alterations for which retrospective planning permission is sought.

The site is accessed to the rear from Edinburgh Road with a service yard utilised by delivery vehicles. The property is accessed to the front via a slip road from Glebe Avenue. The site lies within Ickenham Local Centre and Ickenham Village Conservation Area.

#### **3.2 Proposed Scheme**

Planning permission for the sub-division of premises into two to provide one barber shop and one beauty salon (Class E), and alterations to shopfronts including the installation of a canopy (retrospective application).

Two separate fascia signs have been erected above the respective units. The submitted drawings have been revised to amend the fascia signs to provide a single dark grey colour scheme to further unify the appearance of the shop fronts. This is welcomed. Nevertheless, it is recognised that the fascia signs would be subject to separate advertisement consent. Therefore, while this amendment is supported, an informative has been included within this recommendation advising the applicant of the need for them to apply for advertisement consent for any fascia signage, including the existing.

#### **3.3 Relevant Planning History**

2015/APP/2016/2441      6 Glebe Avenue Ickenham

Change of use from A1 to A5

**Decision:** 24-08-2016    NFA

2015/APP/2016/3944      6 Glebe Avenue Ickenham

Change of use from retail (Use Class A1) to restaurant (Use Class A3) and installation of extractor flue to rear (Prior Approval)

**Decision:** 08-03-2017    Refused

#### **Comment on Relevant Planning History**

There is no directly relevant planning history.

### **4. Planning Policies and Standards**

Development Plan:

Planning law requires that applications for planning permission be determined in

accordance with the development plan, unless material considerations indicate otherwise.

The Development Plan for the London Borough of Hillingdon currently consists of the following documents:

The Local Plan: Part 1 - Strategic Policies (2012)  
The Local Plan: Part 2 - Development Management Policies (2020)  
The Local Plan: Part 2 - Site Allocations and Designations (2020)  
The West London Waste Plan (2015)  
The London Plan (2021)

Material Considerations:

The National Planning Policy Framework (NPPF) (2021) is also a material consideration in planning decisions, as well as relevant supplementary planning documents and guidance.

### **Local Plan Designation and London Plan**

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

PT1.HE1 (2012) Heritage

Part 2 Policies:

DMHB 1 Heritage Assets

DMHB 4 Conservation Areas

DMHB 11 Design of New Development

DMHB 12 Streets and Public Realm

DMHB 13 Shopfronts

DMT 6 Vehicle Parking

DMTC 3 Maintaining the Viability of Local Centres and Local Parades

LLP D1 (2021) London's form character and capacity for growth

LPP D3 (2021) Optimising site capacity through the design-led approach

LPP D4 (2021) Delivering good design

LPP D5 (2021) Inclusive design

LPP D8 (2021) Public realm

LPP GG1 (2021) Building strong and inclusive communities

LPP HC1 (2021) Heritage conservation and growth

LPP SD6 (2021) Town centres and high streets

NPPF6 NPPF 2021 - Building a strong, competitive economy

NPPF12 NPPF 2021 - Achieving well-designed places

NPPF16 NPPF 2021 - Conserving & enhancing the historic environment

### **5. Advertisement and Site Notice**

**5.1 Advertisement Expiry Date:- 8th September 2022**

**5.2 Site Notice Expiry Date:- 15th September 2022**



## **6. Consultations**

### **External Consultees**

24 neighbours, Ickenham Residents Association, Ickenham Conservation Area Panel and Ruislip Residents Association were consulted by letter dated 10-08-22. A site notice was displayed outside the site on 23-08-22 and a newspaper advert placed on 17-08-22.

No comments have been received.

### **Internal Consultees**

ACCESS OFFICER:

Any approval of this proposed subdivision of this shop unit must ensure that the current standard of accessibility is maintained. The following condition should be attached to any approval of this retrospective application:

The shopfront hereby approved shall ensure that step-free access for wheelchair users is maintained from the public footway, via a profiled threshold or water bar not exceeding 15 mm in height and a doorway width of no less than 1000 mm, with all such features remaining in place for the life of the building. REASON: To ensure an Accessible and Inclusive development for everyone in accordance with London Plan policy D5.

PLANNING OFFICER RESPONSE:

It is noted that the current access has a minimal step. Therefore the recommended condition for a threshold strip has been included in Section 2 of this report. Since the proposal has already been implemented this condition has been worded to require that this be provided within 3 months of the date of this planning permission.

CONSERVATION OFFICER:

6 Glebe Avenue is situated within the Ickenham Conservation Area and forms part of a terrace of shops that probably date from the 1960s. The application seeks retrospective permission to subdivide the shop unit into two to provide one barber shop (Class E) and one beauty salon (Sui Generis), and alterations to shopfronts including the installation of a canopy.

The 1960s terrace sits adjacent to an interwar terrace of shops that create a rhythm of shop fronts / fascia signs that are separated by brick pilaster surrounds. The replacement shopfront now contains two doors to allow an access into each of the shops. This is considered acceptable as the grey framing and fenestration is consistent and the break line between the two units is minimal. This allows for the brick shopfront pilasters to dominate and the rhythm of shopfronts across the terrace is not significantly affected.

The only improvement that I would suggest is to the fascia signs which would benefit from being of one base colour so that a visual break is not created and a consistent fascia width is retained. The awning, however, does help to provide some mitigation by uniting the shop frontage.

The harm identified is less than substantial at the lower end of the scale. Further harm is likely to occur in the future with pressure from future occupiers wanting to change their part of the shopfront, awning and fascia to make their unit more individual. This should be avoided in any future application.

PLANNING OFFICER RESPONSE:

These comments are noted. The drawings have been amended to demonstrate the fascia sign

being of one base colour. This intent is welcomed. Nevertheless, fascia signage would be subject to separate advertisement consent. Therefore, an informative has been included in Section 2 of this report, advising of the need for advertisement consent.

HIGHWAYS OFFICER:

There are no objections from the Highways Authority for this application.

PLANNING OFFICER RESPONSE:

Noted.

## **7. MAIN PLANNING ISSUES**

### **7.01 The principle of the development**

Policy DMTC 3 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states:

A) The Council will protect and enhance the function of local centres and local shopping parades by retaining uses that support their continued viability and attractiveness to the locality they serve. In considering applications for changes of use of shops the Council will ensure that:

- i) the local centre or shopping parade retains sufficient essential shop uses to provide a range and choice of shops appropriate to the size of the parade, and its function in the Borough shopping hierarchy;
- ii) at least of 50% of the local centre or shopping parade is retained as Use Class A1 shops; and
- iii) the surrounding residential area is not deficient in essential shop uses.

In terms of the principle of the development, the proposed site is located within Ickenham Local Centre as identified in the Hillingdon Local Plan. The proposal would retain retail use in the form of the barber shop (former A1 use class, now use class E). The introduction of a beauty salon (sui generis) would technically result in some loss of A1 floor area. However this is considered a complementary use to the premises which would improve the retail offer of the local centre.

It is noted that the site lies within a smaller section of the Local Centre comprising 12 units excluding the car showroom. Of the other 10 units, there are 3 food outlets (1 restaurant and 2 hot food takeaways), a convenience store, an estate agents, solicitors, a hairdressers, a funeral directors, a launderette and a phone repair shop. The wider local offer on Swakeleys Road is also noted to include various retail shops.

The proposal to retain the use of the site as a barber shop and a beauty salon would not result in the loss of a retail shop and would not impact adversely on the retail offer as essential shops within the local centre would be retained. Therefore the principle of the use of the premises as a barber shop and beauty salon are acceptable.

### **7.02 Density of the proposed development**

Not applicable to this application.

### **7.03 Impact on archaeology/CAs/LBs or Areas of Special Character**

See 'Impact on the character & appearance of the area' section at paragraph 7.07.

### **7.04 Airport safeguarding**

Not applicable to this application.

### **7.05 Impact on the green belt**

Not applicable to this application.

### **7.07 Impact on the character & appearance of the area**

## POLICY CONTEXT

Paragraph 126 of the National Planning Policy Framework (2021) seeks the creation of high quality, beautiful and sustainable buildings. Parts b) and c) of paragraph 130 of the NPPF (2021) states that planning policies and decisions should ensure that developments are visually attractive as a result of good architecture and are sympathetic to local character and history, including the surrounding built environment.

Paragraph 199 of the NPPF (2021) states that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be).

Paragraph 202 of the NPPF (2021) states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

Policies D1, D3 and D4 of the London Plan (2021) require development proposals to be high quality and to enhance the local context and be delivering buildings and spaces that positively respond to local distinctiveness.

Policy HC1 of the London Plan (2021) states that development proposals affecting heritage assets, and their settings, should conserve their significance, by being sympathetic to the assets' significance and appreciation within their surroundings.

Policy BE1 of the Hillingdon Local Plan: Part One - Strategic Policies (2012) states that all new developments should achieve a high quality of design in all new buildings and the public realm contributes to community cohesion and a sense of place.

Policy HE1 of the Hillingdon Local Plan: Part One Strategic Policies (2012) states that the Council will conserve and enhance Hillingdon's distinct and varied environment, its settings and the wider historic landscape, which includes the designated heritage assets such as statutorily Listed Buildings, Conservation Areas and Scheduled Ancient Monuments.

Policy DMHB 1 of the Hillingdon Local Plan: Part 2 - Development Management Policies (January 2020) states:

A) The Council will expect development proposals to avoid harm to the historic environment. Development that has an effect on heritage assets will only be supported where:

- i) it sustains and enhances the significance of the heritage asset and puts them into viable uses consistent with their conservation;
- ii) it will not lead to a loss of significance or harm to an asset, unless it can be demonstrated that it will provide public benefit that would outweigh the harm or loss, in accordance with the NPPF;
- iii) it makes a positive contribution to the local character and distinctiveness of the area;
- iv) any extensions or alterations are designed in sympathy, without detracting from or competing with the heritage asset;
- v) the proposal would relate appropriately in terms of siting, style, scale, massing, height, design and materials;
- vi) buildings and structures within the curtilage of a heritage asset, or in close proximity to it, do not compromise its setting; and
- vii) opportunities are taken to conserve or enhance the setting, so that the significance of

the asset can be appreciated more readily.

Policy DMHB 4 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) states that new development, including alterations and extensions to existing buildings, within a Conservation Area or on its fringes, will be expected to preserve or enhance the character or appearance of the area. It should sustain and enhance its significance and make a positive contribution to local character and distinctiveness.

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) states that all development will be required to be designed to the highest standards and incorporate principles of good design.

Policy DMHB 12 of the Hillingdon Local Plan: Part 2 - Development Management Policies (2020) advises that development should be well integrated with the surrounding area.

## ASSESSMENT

The site falls within the Ickenham Village Conservation Area. The subject terrace sits adjacent to an interwar terrace of shops that create a rhythm of shop fronts / fascia signs that are separated by brick pilaster surrounds.

The external alterations carried out involve the installation of an additional door to enable access into each of the shops, and a canopy/awning extending across the whole of the shopfront.

While the premises would be subdivided, the shop front alterations would retain an appearance of a unified shopfront. This is largely due to the grey framing and windows which are consistent between the two shops, and a break line between the two shops which is very minimal. The installed awning also unites the two shop fronts into one. This allows for the brick shopfront pilasters to dominate, and the rhythm and proportions of the shopfronts across the terrace is therefore not significantly affected.

Therefore, while the proposal would result in less than substantial harm to the conservation area (on the lower end of the scale), this would be outweighed by the social and economic benefits that an enhanced commercial offering would bring to this local shopping area. It is considered that the external alterations to the shopfront would preserve the appearance of the Ickenham Village Conservation Area, and the visual amenity of the street scene, in accordance with the relevant planning policies.

It is acknowledged that the Council's Conservation Officer has recommended adjusting the fascia signage to be one colour scheme. The applicant has accepted this, and revised plans have been submitted. These drawings demonstrate that the white colour fascia sign would be removed and a dark grey colour scheme would be carried through both fascia signs. This would ensure a more harmonious signage scheme and removes the contrasting colour schemes that may visually detract from the appearance of the shop front. Nevertheless, while this change is welcomed and encouraged, it is also recognised that fascia signage is subject to separate advertisement consent and not planning permission. Therefore, an informative has been included in Section 2 of this report advising the applicant of the need to apply for advertisement consent.

### **7.08 Impact on neighbours**

The proposed uses would not result in amenity impacts above and beyond the pre-existing use as a sandwich shop. Nevertheless, conditions have been included in Section 2 of this report concerning hours of operation and servicing to mitigate any potential noise

and disturbance from out-of-hours operations.

Given the nature and scale of the proposed shop front alterations, these would not have any impact on neighbouring residential amenity.

**7.09 Living conditions for future occupiers**

Not applicable to this application.

**7.10 Traffic impact, Car/cycle parking, pedestrian safety**

Policies DMT 1 and DMT 2 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) require the Council to consider whether the traffic generated by proposed developments is acceptable in terms of the local highway and junction capacity, traffic flows and conditions of general highway or pedestrian safety.

Policy DMT 6 of the Hillingdon Local Plan: Part Two - Development Management Policies (2020) seeks to ensure that all development is in accordance with the car parking standards set out in Appendix C, Table 1 unless it can be demonstrated that a deviation from the standard would not result in a deleterious impact on the surrounding road network.

Given the site's location in a Local Centre, it is considered the development proposal would not affect the current parking provision. Refuse arrangements would continue as existing at the rear in the service yard. The Highway Authority do not raise any objections.

**7.11 Urban design, access and security**

Not applicable to this application.

**7.12 Disabled access**

Policy D5 of the London Plan states:

Development proposal should achieve the highest standards of accessible and inclusive design. They should:

- 1) be designed taking into account London's diverse population
- 2) provide high quality people focused spaces that are designed to facilitate social interaction and inclusion
- 3) be convenient and welcoming with no disabling barriers, providing independent access without additional undue effort, separation or special treatment
- 4) be able to be entered, used and exited safely, easily and with dignity for all
- 5) be designed to incorporate safe and dignified emergency evacuation for all building users. In all developments where lifts are installed, as a minimum at least one lift per core (or more subject to capacity assessments) should be a suitably sized fire evacuation lift suitable to be used to evacuate people who require level access from the building.

The plans currently show a stepped access which is not considered to be accessible and inclusive. As recommended by the access officer, a condition is attached to secure a step-free access into these shops thereby improving accessibility.

**7.13 Provision of affordable & special needs housing**

Not applicable to this application.

**7.14 Trees, landscaping and Ecology**

Not applicable to this application.

**7.15 Sustainable waste management**

The existing and pre-existing refuse storage arrangements to the rear of the properties will be retained. This is considered acceptable.

**7.16 Renewable energy / Sustainability**

Not applicable to this application.

**7.17 Flooding or Drainage Issues**

Not applicable to this application.

**7.18 Noise or Air Quality Issues**

Not applicable to this application.

**7.19 Comments on Public Consultations**

Addressed within Section 6 of this report.

**7.20 Planning obligations**

Not applicable to this application.

**7.21 Expediency of enforcement action**

Not applicable to this application.

**7.22 Other Issues**

No other issues are identified.

**8. Observations of the Borough Solicitor**

**General**

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

**Planning Conditions**

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

**Planning Obligations**

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

**Equalities and Human Rights**

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should

consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

#### **9. Observations of the Director of Finance**

Not applicable.

#### **10. CONCLUSION**

For the reasons set out in this report, it is considered that the proposal would comply with the objectives of national, regional and local planning policies and guidance. It is therefore recommended that the application be approved, subject to the imposition of the conditions set out in Section 2 of the Committee Report.

#### **11. Reference Documents**

Hillingdon Local Plan: Part One - Strategic Policies (November 2012)  
Hillingdon Local Plan: Part 2 - Development Management Policies (2020)  
London Plan (2021)  
National Planning Policy Framework (2021)

**Contact Officer:** Christos Chrysanthou

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

For identification purposes only.  
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Site Address:

**6 Glebe Avenue**

**LONDON BOROUGH OF HILLINGDON**  
**Residents Services Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:  
**2015/APP/2022/1894**

Scale:  
**1:1,250**

Planning Committee:  
**BoroughPage 160**

Date:  
**December 2022**





A

**Item No.**            **Report of the Interim Director of Planning, Regeneration & Public Realm**

**Address**            57 TUDOR WAY HILLINGDON

**Development:**    Erection of a single storey side extension and a part single, part double storey rear extension.

**LBH Ref Nos:**     **20951/APP/2022/1228**

**Drawing Nos:**    Location Plan  
2022/07-02  
2022/07-03 REV F  
2022/07-04 REV F  
2022/07-05 REV F  
2022/07-06 REV F  
2022/07-07  
2022/07-01  
2022/07-08 REV F  
2022/07-09 REV F

**Date Plans Received:** 13/04/2022                      **Date(s) of Amendment(s):** 31/10/2022

**Date Application Valid:** 13/04/2022

Recommendation: APPROVAL, subject to conditions.

## 1. CONSIDERATIONS

### 1.1 Site and Locality

The application relates to a two storey, well-proportioned, detached dwelling house located on the south side of Tudor Way with a mock tudor design and projecting front gable.

The dwelling has an existing single storey rear extension and attached garage to the side. The front drive is paved with vehicle access.

Tudor Way is a wide linear road with detached and semi-detached properties of a similar age and of a similar design language.

The property lies within a critical drainage area and is subject to a blanket tree preservation order (TPO no. 89).

### 1.2 Proposed Scheme

The proposal involves the erection of a single storey side extension and a part single, part double storey rear extension.

During the course of the application process, the plans have been amended to change the proposed roof form of the rear extension in order to break up the bulk and massing from the originally proposed crown roof design. The first floor of the rear extension has also been reduced from 4m to 3m.

### 1.3 Relevant Planning History

20951/APP/2022/288 57 Tudor Way Hillingdon

Erection of outbuilding to rear garden to use as a game room/gym/domestic storage room (Application for a Certificate of Lawful Development for a Proposed Development)

**Decision Date:** 27-03-2022 Approved **Appeal:**

20951/APP/2022/290 57 Tudor Way Hillingdon

Erection of a single storey side extension; part two storey, part single storey rear extension; and conversion of roof space into habitable use to include roof extension, rear dormer and 3 x side roof lights

**Decision Date:** 28-03-2022 Refused **Appeal:** 31-AUG-22 Dismissed

#### Comment on Planning History

The relevant history is detailed above.

Of greatest relevance is application 20951/APP/2022/290. Planning permission was refused for 'Erection of a single storey side extension; part two storey, part single storey rear extension; and conversion of roof space into habitable use to include roof extension, rear dormer and 3 x side roof lights'. This was refused for the following reason:

'The proposed two storey rear extension by reason of its bulk, mass, depth, scale, resultant roof form and overall height, would fail to read as a subordinate addition to the house and would fail to respect its architectural integrity, representing a harmful addition to the character of the local area, harming the streetscene of Tudor Way, all contrary to Policies DMHB 11 and DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (January 2020).'

The decision was subsequently appealed and dismissed under APP/R5510/D/22/3297373, with the Planning Inspector agreeing that the extension would harm the character and appearance of the host dwelling and wider area.

This current application seeks to overcome the Inspector's concerns and has altered the roof form of the rear extension, reduced the depth of the rear extension at first floor and included a set down from the main roof ridge. The side extension remains unchanged, as this did not form part of the reason for refusal.

### 2. Advertisement and Site Notice

2.1 Advertisement Expiry Date:- Not applicable

2.2 Site Notice Expiry Date:- Not applicable

### 3. Comments on Public Consultations

7 neighbouring properties were consulted on 29-4-22. Further rounds of consultation were carried out following the receipt of amended drawings (on 22-09-22 and 31-10-22). The consultation period closed on 14-11-22.

Objections have been received from one neighbouring property and can be summarised as:

- The double storey part of this extension plan extends into the garden somewhat beyond the double storey building line of the immediate neighbours. We request that this be

reduced to match existing buildings.

Officer response: The design of the proposal has been discussed within the main body of the report.

This application has also been called in to committee by a local Councillor.

#### **4. Local Plan Designation and London Plan**

The following Local Plan Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

Part 2 Policies:

DMHB 11 Design of New Development

DMHB 12 Streets and Public Realm

DMHB 14 Trees and Landscaping

DMEI 9 Management of Flood Risk

DMEI 10 Water Management, Efficiency and Quality

DMHD 1 Alterations and Extensions to Residential Dwellings

LPP D3 (2021) Optimising site capacity through the design-led approach

LPP D4 (2021) Delivering good design

LPP D6 (2021) Housing quality and standards

LPP D8 (2021) Public realm

LPP G7 (2021) Trees and woodlands

LPP SI12 (2021) Flood risk management

LPP SI13 (2021) Sustainable drainage

#### **5. MAIN PLANNING ISSUES**

The main consideration in this case is the design of the proposed rear extension and its subsequent impact on the character of the property, streetscene and general locality. Linked to this is whether or not the proposal overcomes the previous reason for refusal and the Inspector's subsequent reasons for dismissing the appeal.

Other considerations include the design of the side extension, the impact of the proposal on the amenities of adjoining occupiers, the reduction in size of the rear garden, flooding and drainage and car parking provision.

POLICY CONTEXT:

Policy DMHB 11 of the Hillingdon Local Plan: Part 2 - Development Management Policies (January 2020) states that:

A) All development, including extensions, alterations and new buildings, will be required to be designed to the highest standards and, incorporate principles of good design including:

- i) harmonising with the local context;
- ii) ensuring the use of high quality building materials and finishes;
- iii) ensuring that the internal design and layout of development maximises sustainability and is adaptable to different activities;
- iv) protecting features of positive value within and adjacent to the site, including the safeguarding of heritage assets, designated and undesignated, and their settings; and
- v) landscaping and tree planting to protect and enhance amenity, biodiversity and green infrastructure.

Policy DMHB 11 also seeks to ensure that development proposals do not adversely impact on the amenity, daylight and sunlight of adjacent properties and open space.

Policy DMHD 1 requires that alterations and extensions to dwellings do not have an adverse cumulative impact on the character and appearance of the street scene, and they should appear subordinate to the main dwelling. It also requires that there is no unacceptable loss of outlook to neighbouring occupiers.

With specific regard to rear extensions the policy states under criterion B):

- i) single storey rear extensions on terraced or semi-detached houses with a plot width of 5 metres or less should not exceed 3.3 metres in depth or 3.6 metres where the plot width is 5 metres or more;
- ii) single storey rear extensions to detached houses with a plot width of 5 metres or more should not exceed 4.0 metres in depth;
- iii) flat roofed single storey extensions should not exceed 3.0 metres in height and any pitched or sloping roofs should not exceed 3.4 metres in height, measured from ground level;
- iv) in Conservation Areas and Areas of Special Local Character, flat roofed single storey extensions will be expected to be finished with a parapet;
- v) balconies or access to flat roofs which result in loss of privacy to nearby dwellings or gardens will not be permitted;
- vi) two storey extensions should not extend into an area provided by a 45-degree line of sight drawn from the centre of the nearest ground or first floor habitable room window of an adjacent property and should not contain windows or other openings that overlook other houses at a distance of less than 21 metres;
- vii) flat roofed two storey extensions will not be acceptable unless the design is in keeping with the particular character of the existing house;
- viii) pitched roofs on extensions should be of a similar pitch and materials to that of the original roof and subordinate to it in design. Large crown roofs on detached houses will not be supported; and
- ix) full width two storey rear extensions are not considered acceptable in designated areas or as extensions to Listed Buildings or Locally Listed Buildings.

The previous application was not refused on the side extension and subsequently the appeal Inspector raised no concerns with this. Nevertheless, Policy DMHD 1 states for Side Extensions:

- i) side extensions should not exceed half the width of the original property;
- ii) extensions to corner plots should ensure that the openness of the area is maintained and the return building line is not exceeded;
- iii) garages should reflect the size guidelines set out in Appendix C Parking standards;
- iv) two storey side extensions should be set in a minimum of 1 metre from the side

boundary or in the case of properties in the Copse Wood and Gatehill Estates, at least 1.5 metres, but more if on a wider than average plot, in order to maintain adequate visual separation and views between houses;

v) two storey side extensions to detached and semi-detached properties should be set back a minimum of 1 metre behind the main front elevation;

vi) where hip to gable roof extensions exist, a two storey side extension will not be supported; and

vii) in Conservation Areas, single storey side extensions may be required to be set back.

#### CHARACTER AND APPEARANCE:

The proposed single storey side extension with pitched roof would replace an existing side garage. Given its width and roof design, it is considered that the proposed side extension would not harm the character of the host dwelling or wider streetscene. The extension would read as subordinate and have an appropriate mass, design and bulk. This was the conclusion reached previously and there has been no change in policy or circumstance to warrant a different conclusion on this part of the proposal.

The ground floor element of the rear addition would have a depth of 4 metres and be characterised by a flat roof, with a height of 3 metres. It is not considered that this would appear as an incongruous addition to either the host building or the wider area, given the context of surrounding properties and that similar rear extensions are common in the area.

The proposed first floor rear element has been significantly altered from the previous proposal. This was essential as the reason for refusal stated:

'The proposed two storey rear extension by reason of its bulk, mass, depth, scale, resultant roof form and overall height, would fail to read as a subordinate addition to the house and would fail to respect its architectural integrity, representing a harmful addition to the character of the local area, harming the streetscene of Tudor Way, all contrary to Policies DMHB 11 and DMHD 1 of the Hillingdon Local Plan: Part Two - Development Management Policies (January 2020).'

This was agreed by the Inspector where the Inspector noted:

'The proposal would significantly elongate the main ridge. The formation of a crown roof would not be particularly obvious, and there is no policy requirement for the ridge to be lower than that of the original property. However, the additional depth of the proposed extension, at the same height as the existing ridge and extending across the full width of the rear elevation, would create significant additional bulk and mass to the rear of the property. Consequently, it would not respect the proportions of the original property and would not appear to be subordinate to the main dwelling in terms of width, depth and height. Furthermore, the original modest hipped roof form would be significantly altered, further eroding the proportions of the original property.'

The Inspector's key concerns related to the width, depth and height and its subsequent impact. The Inspector (nor the previous planning officer) did not dismiss the principle of a rear extension entirely. The Inspector noted that 'extensions of a similar footprint have been achieved in neighbouring properties to the appeal site, which appear as subservient additions to the host dwelling and do not have a significant impact on the streetscene.'

The rear extension now proposed responds to the Inspector's concerns, as well as the established pattern, character and appearance of this part of Tudor Way. The extension would continue to extend the full width of the rear elevation, but the first floor would be limited to a reduced depth of 3m. The roof would also be set down from the main roof ridge line by 1.3m and characterised by a dual hipped roof form to break up the bulk. The result is a form that significantly reduces the massing and bulk of the first floor rear extension, in all views.

The new design is considered to fully address the concerns raised in the previous application. The extension no longer reads as a bulky addition. The extension is now subordinate and responds to the architectural integrity of the host dwelling and its neighbours.

As such, it is considered that the proposal complies with Policies DMHD 1 and DMHB 11 of the Hillingdon Local Plan Part Two (2020). The impact on the character and appearance of the building, streetscene and wider area is acceptable.

#### IMPACT ON NEIGHBOURING AMENITIES:

The proposed single storey rear extension and single storey side extension would have a depth of 4 metres beyond the existing rear wall. At first floor level the depth would be limited to 3m.

In terms of impact on No. 55 Tudor Way, the proposed two storey rear extension would not break a 45 degree line from the middle of No. 55's closest first floor rear window and the impact of its bulk and mass would be screened by No. 55's existing single storey side garage and extension. Given the screening of the existing built form and the distance of separation between the properties, it is considered that the proposed extensions would not harm the amenities of occupiers of No. 55 Tudor Way. This conclusion has been reached previously by the Council, as well as the Inspector.

In terms of impact on No. 59 Tudor Way, the proposed single storey side extension would replace the existing garage along this boundary. The proposed two storey rear extension would be separated from the shared boundary with No. 59 by the proposed single storey side extension. Given this separation, the proposed two storey rear extension would again not break a 45 degree line from the middle of No. 59's closest first floor rear window. The relationship to this near neighbour also remains acceptable, as previously concluded.

A planning condition has been recommended to ensure that the proposed new first floor side facing windows in the development are obscurely glazed, in the interests of protecting neighbouring residential amenity at numbers 55 and 59 Tudor Way.

It is considered that all other neighbouring properties are far enough separated from the proposed development to remain unaffected by the proposal. There has not been any change in policy or circumstance to warrant a different conclusion on this key issue.

#### OTHER MATTERS:

Sufficient external amenity space would be maintained for existing and future occupiers of 57 Tudor Way. Further, all the proposed habitable rooms, and those altered by the extension, would maintain an adequate outlook and source of natural light.

Given that the property lies within a Critical Drainage Area, an informative has been recommended in the event of an approval. This informative reminds the Applicant of this important constraint and provides advice on how to manage drainage on site if the proposal is implemented.

Car parking was not a concern in the previous proposal. The existing property would remain in use as a single family dwelling and there would be no notable increase in parking demand that would cause undue pressure on parking or impact highway safety.

As noted, the area is subject to a blanket tree preservation order, however it is not considered that any significant trees would be impacted by the proposal.

#### CONCLUSION:

The application is recommended for approval, subject to conditions. It is considered the proposal has fully overcome the previous reason for refusal and the reasons for the Inspector dismissing the appeal. The proposal complies with the Development Plan and has an acceptable impact on the character and appearance of the property, streetscene and wider area in its revised form.

## 6. RECOMMENDATION

### APPROVAL subject to the following:

#### 1 HO1 Time Limit

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

#### REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

#### 2 HO2 Accordance with approved

The development hereby permitted shall not be carried out except in complete accordance with the details shown on Drawings: 2022/07-03 REV F, 2022/07-04 REV F, 2022/07-05 REV F, 2022/07-06 REV F, 2022/07-08 REV F and 2022/07-09 REV F.

#### REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan Part 1 (2012) and Part 2 (2020), and the London Plan (2021).

#### 3 HO4 Materials

The materials to be used in the construction of the external surfaces of the development hereby permitted shall match those used in the existing building and shall thereafter be retained as such.

#### REASON

To safeguard the visual amenities of the area and to ensure that the proposed development does not have an adverse effect upon the appearance of the existing building in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**4** HO5 No additional windows or doors

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (or any order revoking and re-enacting that Order with or without modification), no additional windows, doors or other openings shall be constructed in the walls or roof slopes of the development hereby approved.

**REASON**

To prevent overlooking to adjoining properties in accordance with Policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**5** HO7 No roof gardens

Access to the flat roof over the extension hereby approved shall be for maintenance or emergency purposes only and the flat roof shall not be used as a roof garden, terrace, balcony, patio or similar amenity area.

**REASON**

To prevent overlooking to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**6** HO6 Obscure Glazing

The first floor side elevation shower room and en-suite windows shall be glazed with permanently obscured glass to at least scale 4 on the Pilkington scale and be non-opening below a height of 1.8 metres taken from internal finished floor level for so long as the development remains in existence.

**REASON**

To prevent overlooking to adjoining properties in accordance with policy DMHB 11 of the Hillingdon Local Plan Part 2 (2020).

**INFORMATIVES**

- 1** The site lies in a Critical Drainage Area (CDA) as identified in the Surface Water Management Plan (SWMP) for Hillingdon. A CDA is the catchment area from which surface water drains and contributes to drainage problems. All developments in this area must contribute to managing the risk of flooding from surface water by reducing surface water runoff from the site. Therefore the applicant should minimise the water from your site entering the sewers. No drainage to support the extension should be connected to any existing surface water sewer, other than as an overflow. Water run off from any roof or hard paving associated with the development should be directed to a soakaway, or tank or made permeable. This includes any work to front gardens not part of the planning application, which must be permeable or be collected and directed to a permeable area, otherwise it would need an additional permission. A water butt should be incorporated.
- 2** On this decision notice policies from the Councils Local Plan: Part 1 - Strategic Policies appear first, then relevant Local Plan Part 2 (2020), then London Plan Policies (2021). Hillingdon's Full Council adopted the Hillingdon Local Plan: Part 1 - Strategic Policies on 8 November 2012 and the Hillingdon Local Plan Part 2 on 16 January 2020.
- 3** The Council will recover from the applicant the cost of highway and footway



repairs, including damage to grass verges.

Care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense.

For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

For Private Roads: Care should be taken during the building works hereby approved to ensure no damage occurs to the verge of footpaths on private roads during construction. Vehicles delivering materials to this development shall not override or cause damage to a private road and where possible alternative routes should be taken to avoid private roads. The applicant may be required to make good any damage caused.

### Standard Informatives

- 1 The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).
- 2 The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including The London Plan - The Spatial Development Strategy for London consolidated with alterations since 2011 (2016) and national guidance.

#### Part 1 Policies:

PT1.BE1 (2012) Built Environment

#### Part 2 Policies:

DMHB 11 Design of New Development  
DMHB 12 Streets and Public Realm  
DMHB 14 Trees and Landscaping  
DMEI 9 Management of Flood Risk  
DMEI 10 Water Management, Efficiency and Quality  
DMHD 1 Alterations and Extensions to Residential Dwellings  
LPP D3 (2021) Optimising site capacity through the design-led approach

LPP D4	(2021) Delivering good design
LPP D6	(2021) Housing quality and standards
LPP D8	(2021) Public realm
LPP G7	(2021) Trees and woodlands
LPP SI12	(2021) Flood risk management
LPP SI13	(2021) Sustainable drainage

- 3 You are advised this permission is based on the dimensions provided on the approved drawings as numbered above. The development hereby approved must be constructed precisely in accordance with the approved drawings. Any deviation from these drawings requires the written consent of the Local Planning Authority.
- 4 You are advised that if any part of the development hereby permitted encroaches by either its roof, walls, eaves, gutters, or foundations, then a new planning application may have to be submitted. The validity of this planning permission may be challengeable by third parties if the development results in any form of encroachment onto land outside the applicant's control that is considered to cause harm to local amenity.
- 5 Your attention is drawn to the need to comply with the relevant provisions of the Building Regulations, the Building Acts and other related legislation. These cover such works as - the demolition of existing buildings, the erection of a new building or structure, the extension or alteration to a building, change of use of buildings, installation of services, underpinning works, and fire safety/means of escape works. Notice of intention to demolish existing buildings must be given to the Council's Building Control Service at least 6 weeks before work starts. A completed application form together with detailed plans must be submitted for approval before any building work is commenced. For further information and advice, contact - Building Control, 3N/01 Civic Centre, Uxbridge (Telephone 01895 558170).
- 6 You have been granted planning permission to build a residential extension. When undertaking demolition and/or building work, please be considerate to your

neighbours and do not undertake work in the early morning or late at night or at any time on Sundays or Bank Holidays. Furthermore, please ensure that all vehicles associated with the construction of the development hereby approved are properly washed and cleaned to prevent the passage of mud and dirt onto the adjoining highway. You are advised that the Council does have formal powers to control noise and nuisance under The Control of Pollution Act 1974, the Clean Air Acts and other relevant legislation. For further information and advice, please contact - Environmental Protection Unit, 4W/04, Civic Centre, High Street, Uxbridge, UB8 1UW (Tel. 01895 250190).

- 7 The Party Wall Act 1996 requires a building owner to notify, and obtain formal agreement from, any adjoining owner, where the building owner proposes to:
- carry out work to an existing party wall;
  - build on the boundary with a neighbouring property;
  - in some circumstances, carry out groundworks within 6 metres of an adjoining building.

Notification and agreements under this Act are the responsibility of the building owner and are quite separate from Building Regulations, or Planning Controls. The Building Control Service will assume that an applicant has obtained any necessary agreements with the adjoining owner, and nothing said or implied by the Council should be taken as removing the necessity for the building owner to comply fully with the Party Wall Act. Further information and advice is to be found in "the Party Walls etc. Act 1996 - explanatory booklet" published by the ODPM, available free of charge from the Planning Services Civic Centre, Uxbridge, UB8 1UW.

- 8 Your attention is drawn to the fact that the planning permission does not override  
does property rights and any ancient rights of light that may exist. This permission

not empower you to enter onto land not in your ownership without the specific consent of the owner. If you require further information or advice, you should consult a solicitor.

- 9 Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with: -

A) Demolition and construction works should only be carried out between the  
of hours of 08.00 hours and 18.00 hours Monday to Friday and between the hours

08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays Bank and Public Holidays.

B) All noise generated during such works should be controlled in compliance with British Standard Code of Practice BS 5228: 1984.

C) The elimination of the release of dust or odours that could create a public health nuisance.

D) No bonfires that create dark smoke or nuisance to local residents.

prior You are advised to consult the Council's Environmental Protection Unit, 3S/02, Civic Centre, High Street, Uxbridge, UB8 1UW (Tel.01895 277401) or to seek approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

10 You are advised that care should be taken during the building works hereby approved to avoid spillage of mud, soil or related building materials onto the pavement or public highway. You are further advised that failure to take appropriate steps to avoid spillage or adequately clear it away could result in action being taken under the Highways Act.

11 To promote the development of sustainable building design and construction methods, you are encouraged to investigate the use of renewable energy resources which do not produce any extra carbon dioxide (CO2) emissions, including solar, geothermal and fuel cell systems, and use of high quality insulation.

12 You are advised that care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made

good to the satisfaction of the Council and at the applicant's expense. For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

**Contact Officer:** Hardev Gill

**Telephone No:** 01895 250230



**Notes:**

 Site boundary

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Site Address:

**57 Tudor Way**

**LONDON BOROUGH OF HILLINGDON**  
 Residents Services  
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:  
**20951/APP/2022/1228**

Scale:  
**1:1,250**

Planning Committee:  
**BoroughPage 173**

Date:  
**December 2022**



**HILLINGDON**  
 LONDON

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# Plans for Borough Planning Committee

Thursday 8th December 2022



**HILLINGDON**  
LONDON

[www.hillingdon.gov.uk](http://www.hillingdon.gov.uk)

**Report of the Interim Director of Planning, Regeneration & Public Realm**

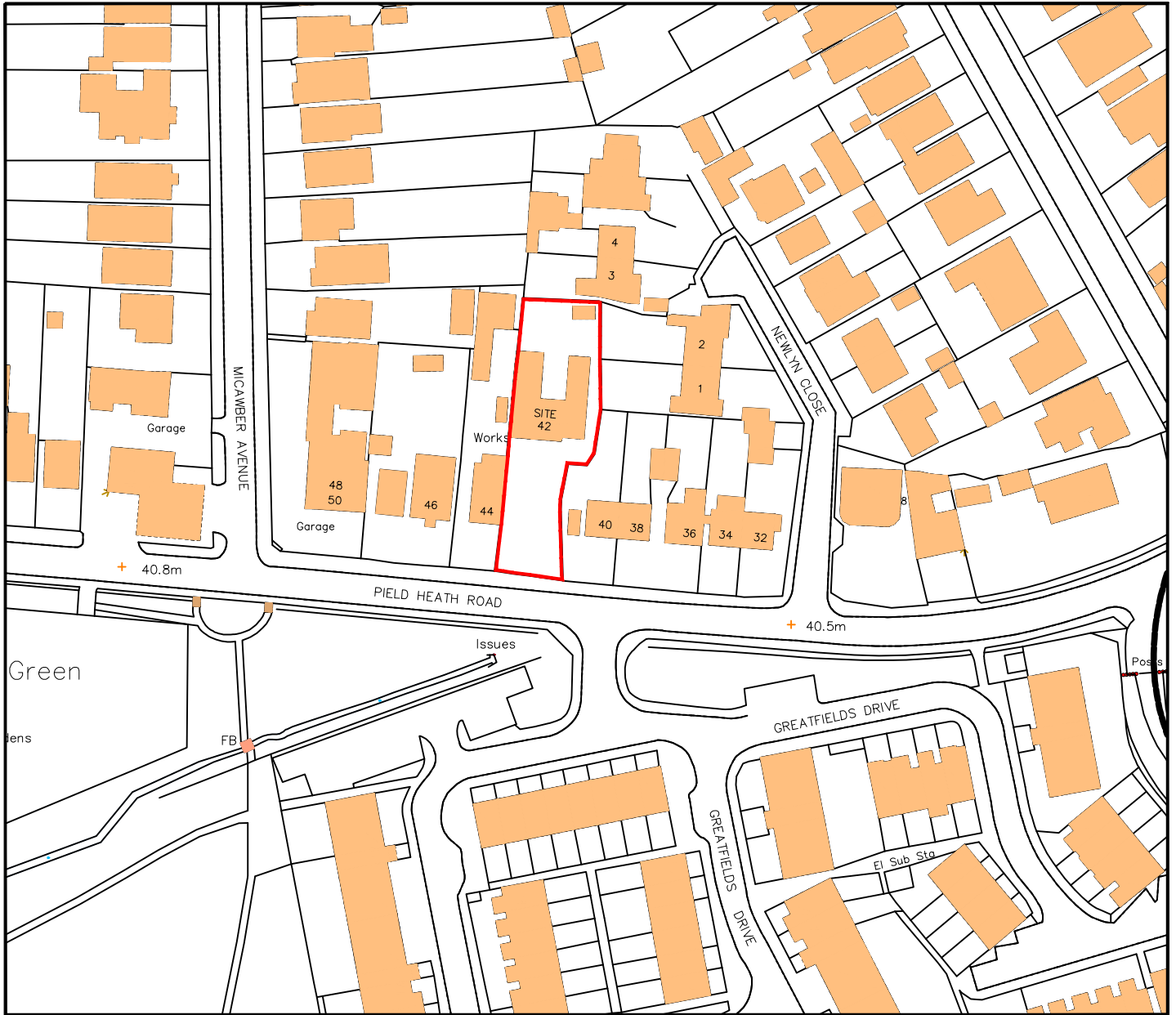
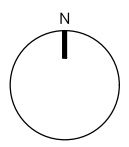
**Address** 42 PIELD HEATH ROAD HILLINGDON

**Development:** Demolition of a two-storey bed and breakfast, and the erection of a three-storey residential development comprising of seven flats, alterations to existing drop kerb, all associated external works and landscaping.

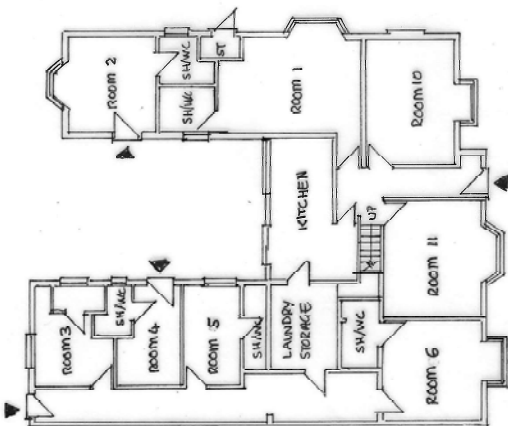
**LBH Ref Nos:** 17611/APP/2022/993



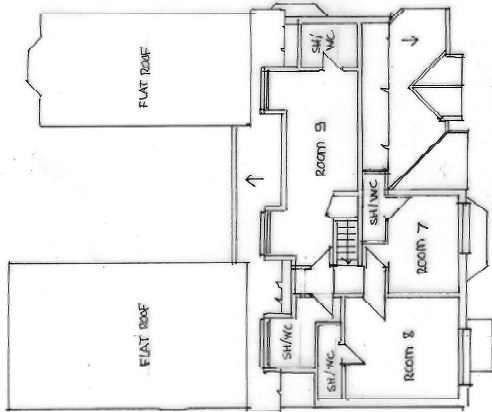
		Trafalgar Properties Ltd	
SITE:	42 Field Heath Road, Uxbridge, UB8 3NP	555/B/01	10.09.22
		DRAWING NO.	PROJECT NO.
TITLE:	Location Plan	1:1250	rev
		SCALE AT A4.	REVISION.
		DRAWN.	CHECKED.



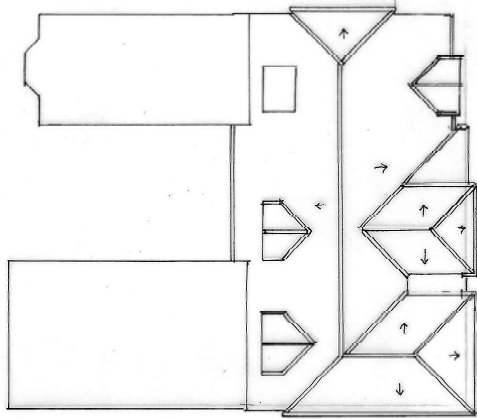
site area = 829.1m<sup>2</sup>, 0.08291ha



GROUND FLOOR PLAN



FIRST FLOOR PLAN



ROOF PLAN

SITE: <b>42 FIELD HEATH ROAD          UXBRIDGE          MIDDLESEX UB8 3NG</b>	
SCALE BAR: 	DATE: <b>SEPT 2012</b>
DESCRIPTION: <b>EXISTING FLOORS &amp; ROOF PLANS</b>	
SCALE: <b>1:200 @ A3</b>	REV: <b>555 /B /04</b>
DRAWING No: <b>555 /B /04</b>	



**GEA OF EXISTING BUILDINGS**

- Area A = 50.3m<sup>2</sup>
- Area B = 32.9m<sup>2</sup>
- Area C\* = 175.6m<sup>2</sup>
- Shed = 10.7m<sup>2</sup>

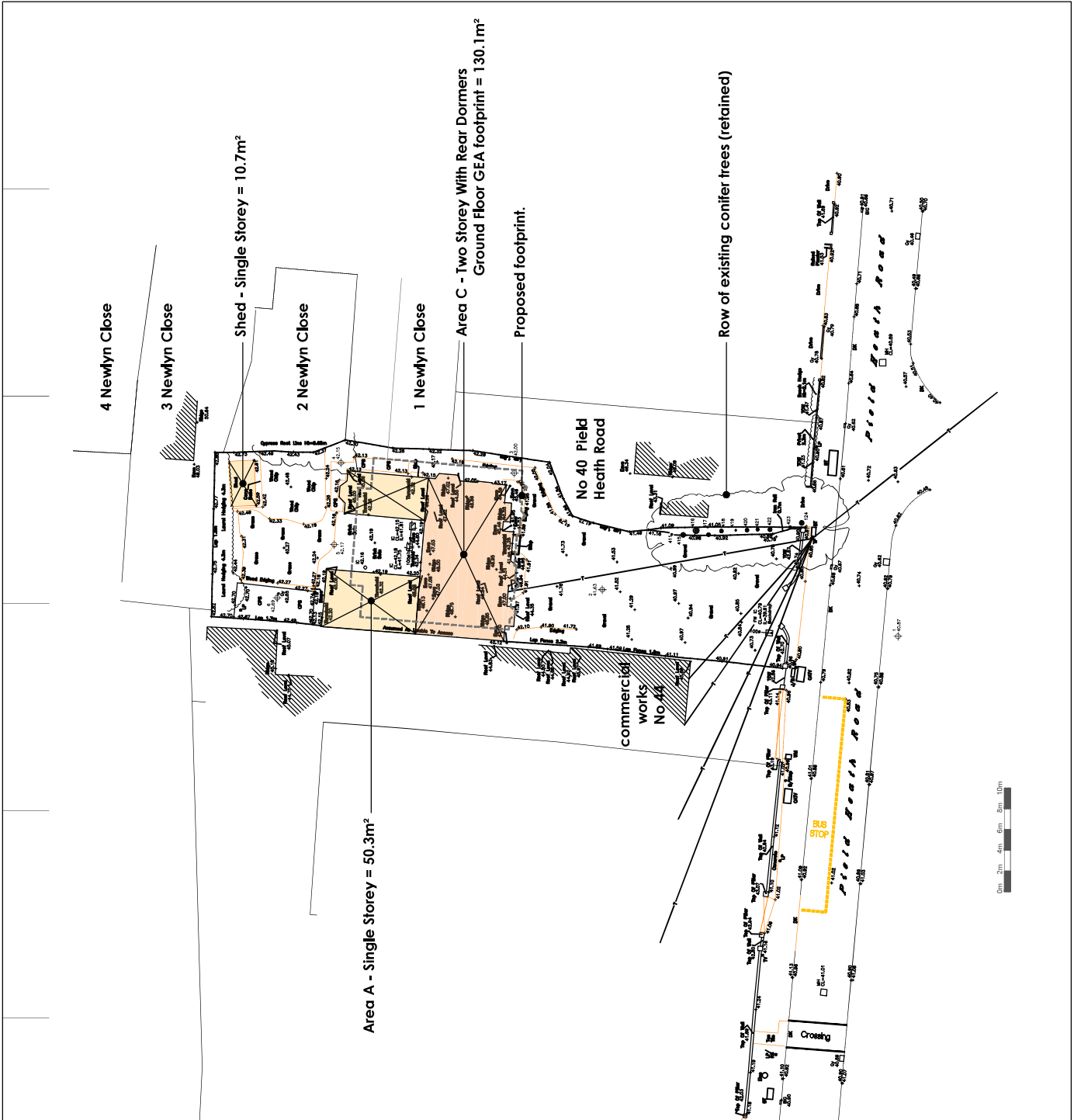
**Estimated Total Existing Footprint = 269.5m<sup>2</sup>**

\* Area C Ground floor GEA footprint = 130.1m<sup>2</sup> with rear dormers estimates of 35% of GEA footprint @ 45.5m<sup>2</sup> summed = 175.6m<sup>2</sup>

**GIA (ESTIMATED USING 300mm EXTERNAL WALL OFFSET) DORMER AREA RETAINED AS 45.5m<sup>2</sup>**

- Area A = 41.9m<sup>2</sup>
- Area B = 26.0m<sup>2</sup>
- Area C = 114.9 m<sup>2</sup>x 2
- Shed = 7.0m<sup>2</sup>
- Dormer = 45.5m<sup>2</sup>

**Estimated GIA Total of Existing Footprint = 350.2m**



REV	DESCRIPTION	DATE
01/2020	PLANNING	01/2020

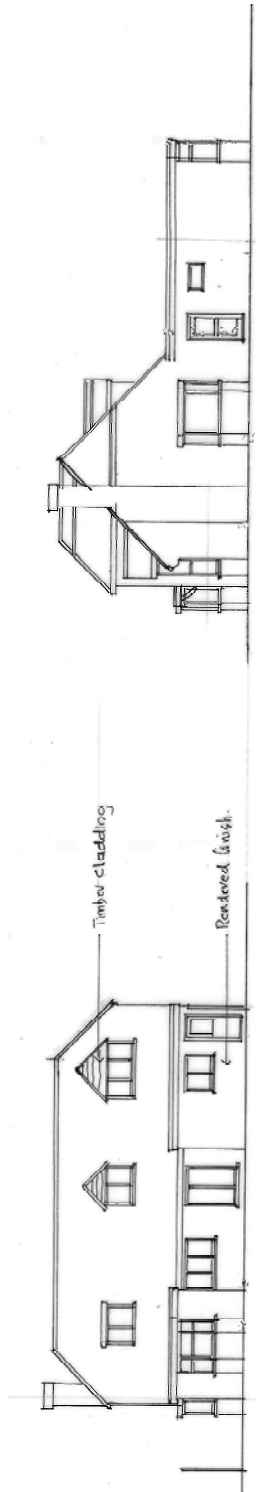
CLIENT: Tratalgar Properties Ltd

CONTRACT NO: Tratalgar Properties Ltd

TITLE: 42 Field Heath Road, Uxbridge, UB8 3NP

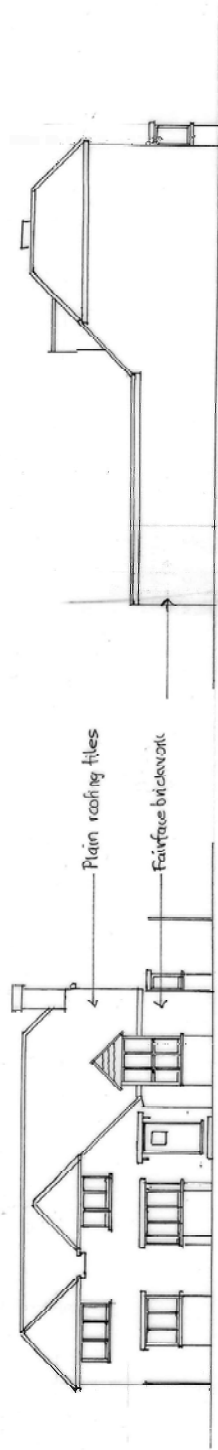
TYPE: Existing and Topographical Plan

DATE PLOTTED	SCALE	CHECKED	DATE
10/09/22	down	checked	10/09/22
PROJECT NO	DATE	REV	
555/6/02		rev	



REAR ELEVATION

SIDE ELEVATION



FRONT ELEVATION

SIDE ELEVATION (WEST)

SITE: 42 FIELD HEATH ROAD UXBRIDGE MIDDLESEX UB8 3NG	
SCALE: 800 0 2 4 6 8 10 (m)	DATE: 1 - 2008 A3
DESCRIPTION: EXISTING ELEVATIONS	
DRAWING No 555 / B / 05	REV



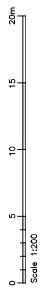
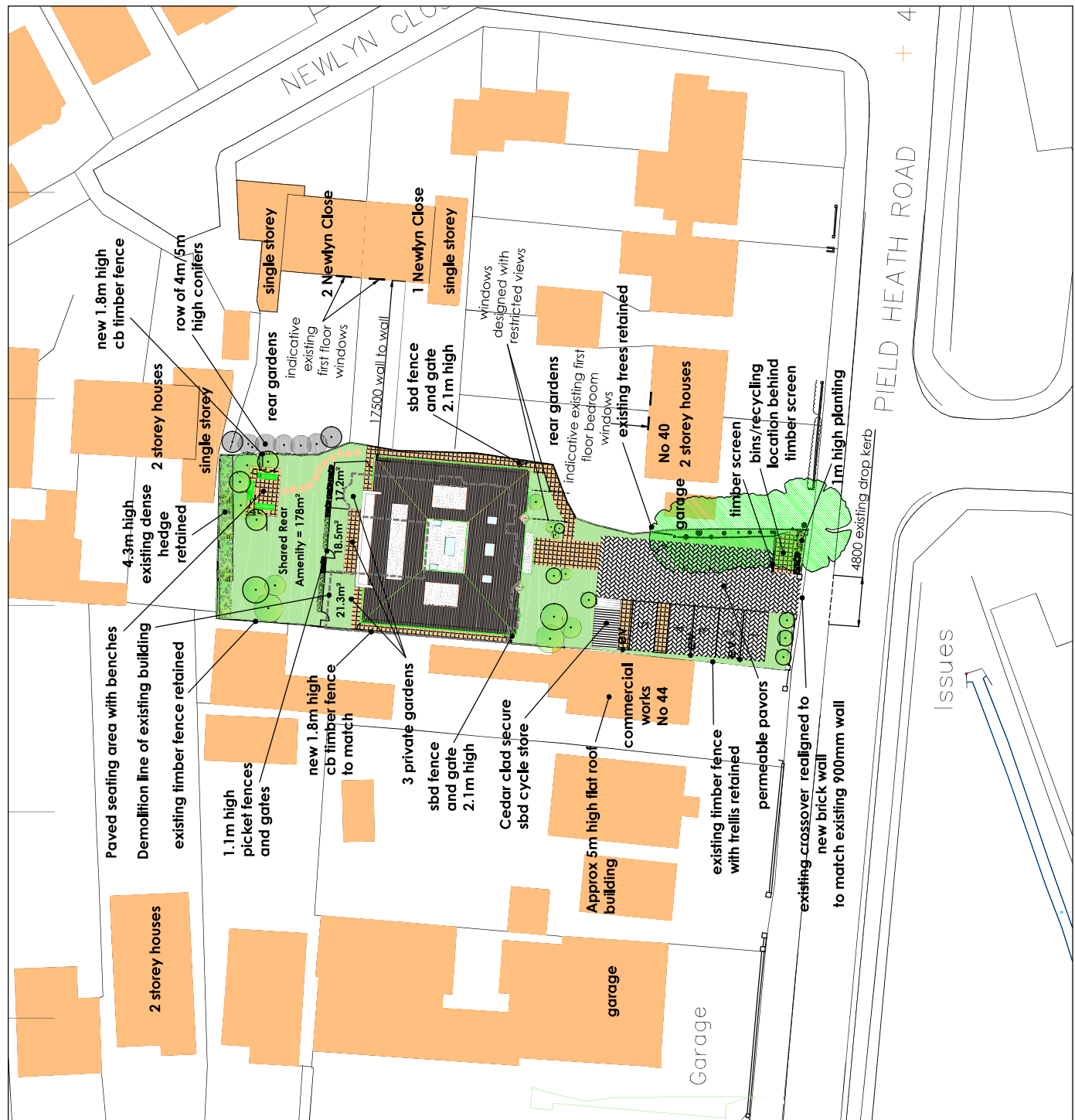
**SCHEDULE OF ACCOMMODATION**

6 x 2 Beds, 3 Person, Flats @ 61m<sup>2</sup>  
 1 x 2 Beds, 4 Person Flat @ 74m<sup>2</sup>

5 x car parking spaces to comply with London Plan, of which 2 could be adapted to add car parking spaces  
 3 x electrical paired active car charging points (i.e 1 charger per car)

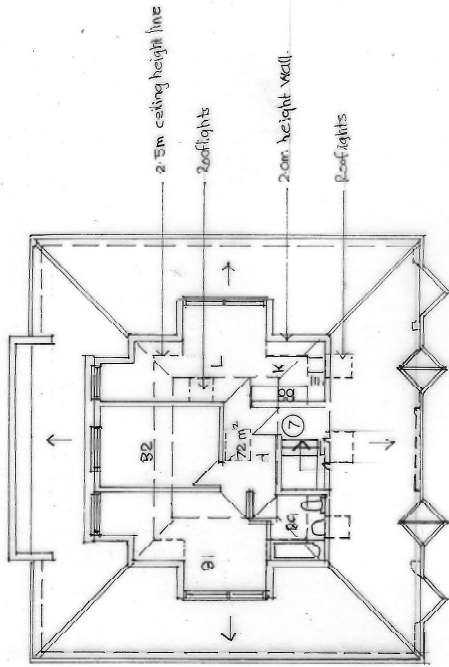
**REAR AMENITY:**

Shared Garden Amenity = 178m<sup>2</sup>  
 Total Required Amenity = 125m<sup>2</sup>  
 3 Private gardens @ 21.3m<sup>2</sup>, 18.5m<sup>2</sup> and 17.2m<sup>2</sup>, Total = 57m<sup>2</sup>  
**TOTAL ACTUAL PROVISION = 235m<sup>2</sup>**  
 (178m<sup>2</sup>+57m<sup>2</sup>)

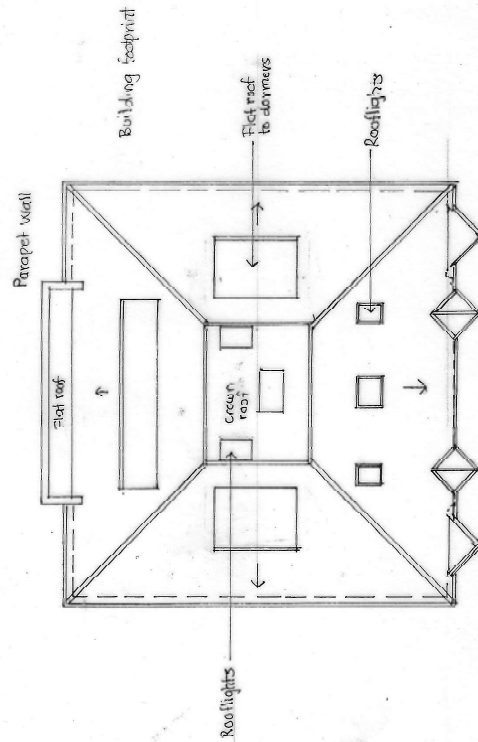


Proposed Site Plan

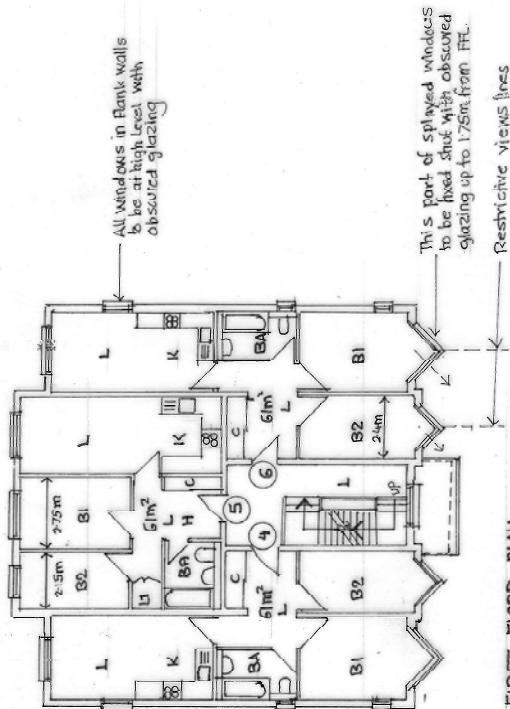
REF:	10250418/2024	DATE:	31/01/2024
<b>PLANNING</b>			
CLIENT:	Trotalgar Properties Ltd		
CONTRACT NO.:	Trotalgar Properties Ltd		
PROJECT:	42 Field Heath Road, Uxbridge, UB8 3HP.		
DATE:	10/09/22	DRAWN BY:	checked
PROJECT NO.:	555/B/03	CHECKED BY:	11/22



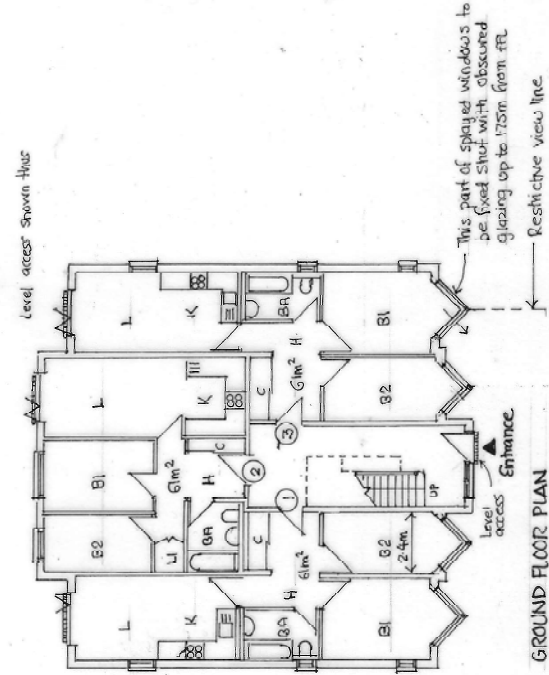
LOFT FLOOR PLAN



ROOF PLAN



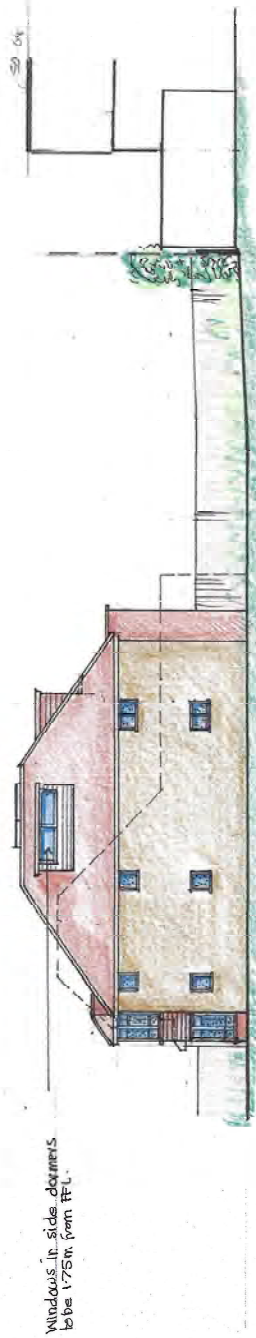
FIRST FLOOR PLAN



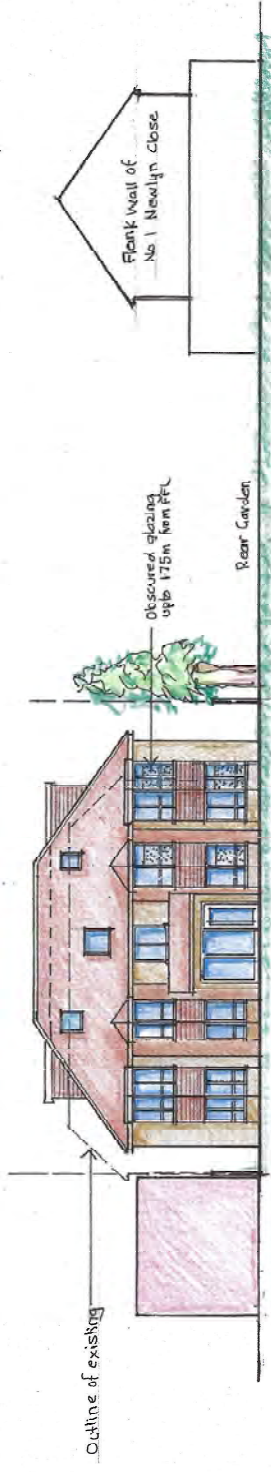
GROUND FLOOR PLAN

SITE: PIELD HEATH ROAD UXBRIDGE TRIDDLESEX UB8 3NP	
SCALE BAR:	0 1 2 4 6 8 10m
DESCRIPTION: PROPOSED FLOOR PLANS & ROOF PLAN	
SCALE:	DATE: SEPT 2002
1:2000 A3	REV
DRAWING NO: 555/B/06	



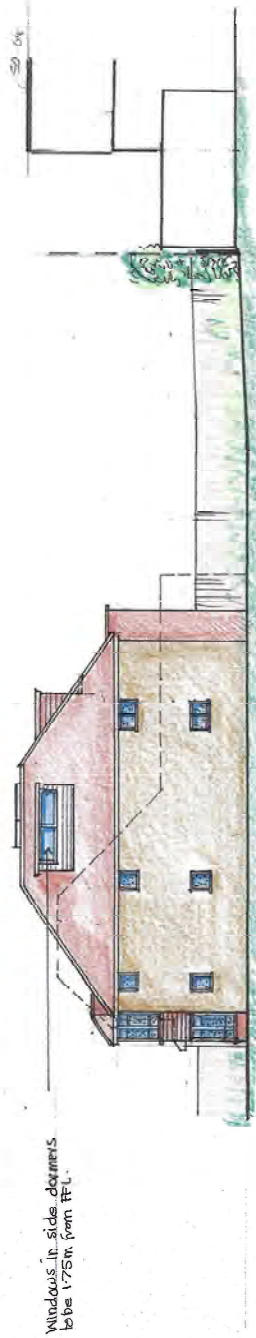


EAST ELEVATION

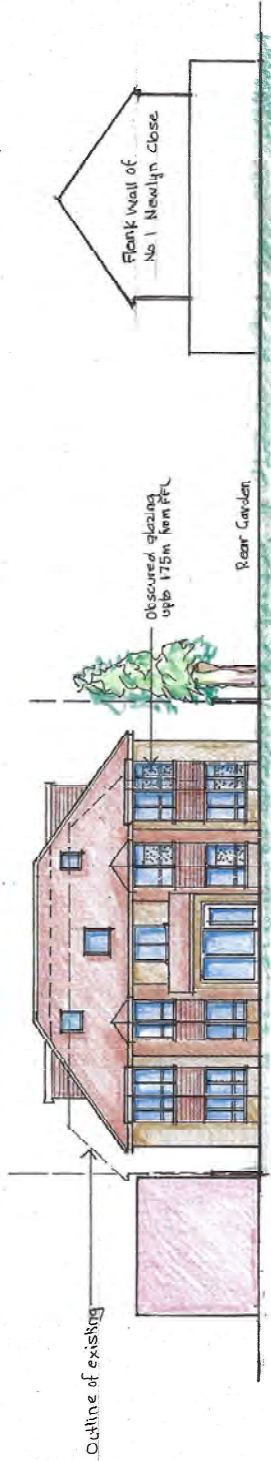


SOUTH ELEVATION

SITE: 42 FIELD HEATH ROAD UXBRIDGE MIDDLESEX UB8 3NG	
SCALE BAR: 0 1 2 3 4 5 6 8 10m	DESCRIPTION: PROPOSED EAST & SOUTH ELEVATIONS
SCALE: 1:200 @ A3	DATE: SEPT 2022
DRAWING No: 555 / A / 07	REV:



EAST ELEVATION



SOUTH ELEVATION

SITE: 42 FIELD HEATH ROAD UXBRIDGE MIDDLESEX UB8 3NG	
SCALE BAR: 0 1 2 3 4 5 6 8 10m	DESCRIPTION: PROPOSED EAST & SOUTH ELEVATIONS
SCALE: 1:200 @ A3	DATE: SEPT 2022
DRAWING No: 555 / A / 07	REV:



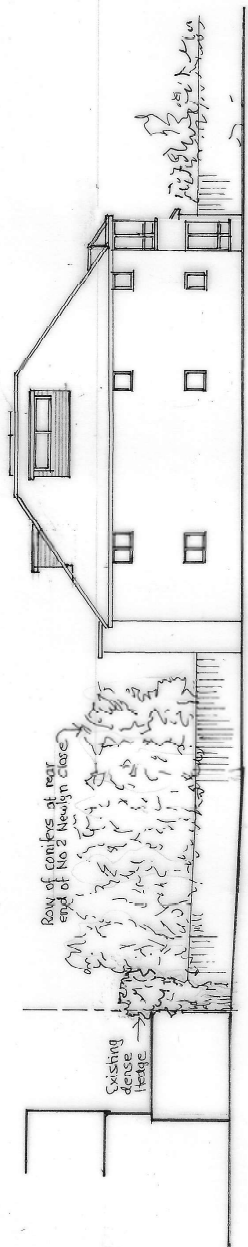


WEST ELEVATION

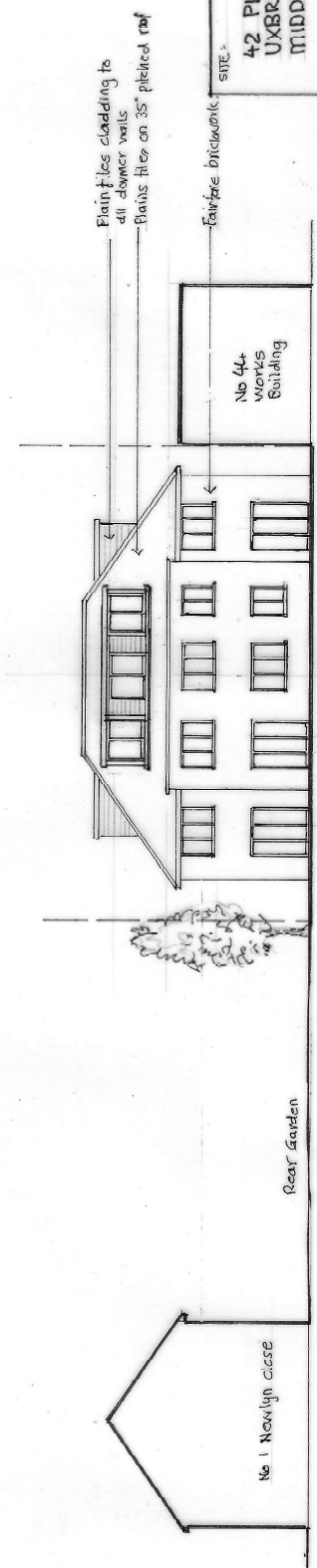


NORTH ELEVATION

SITE	
42 FIELD HEATH ROAD UXBRIDGE MIDDLESEX UB8 3NP	
SCALE BAR 0 1 2 4 6 8 10m	
PROPOSED NORTH & WEST ELEVATIONS	
SCALE:	DATE:
1:200 @ A3	SEPT 2002
DRAWING NO	REV A
555/B/08	A

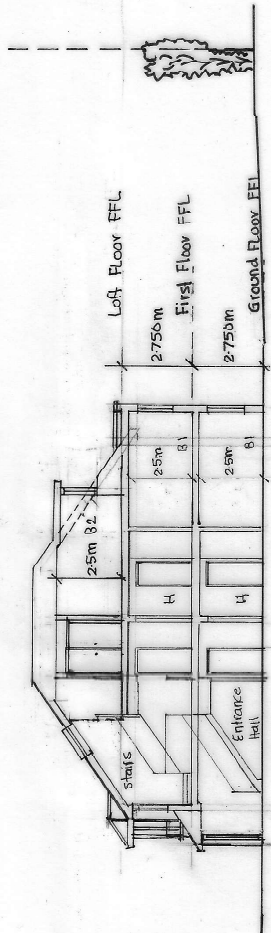


WEST ELEVATION



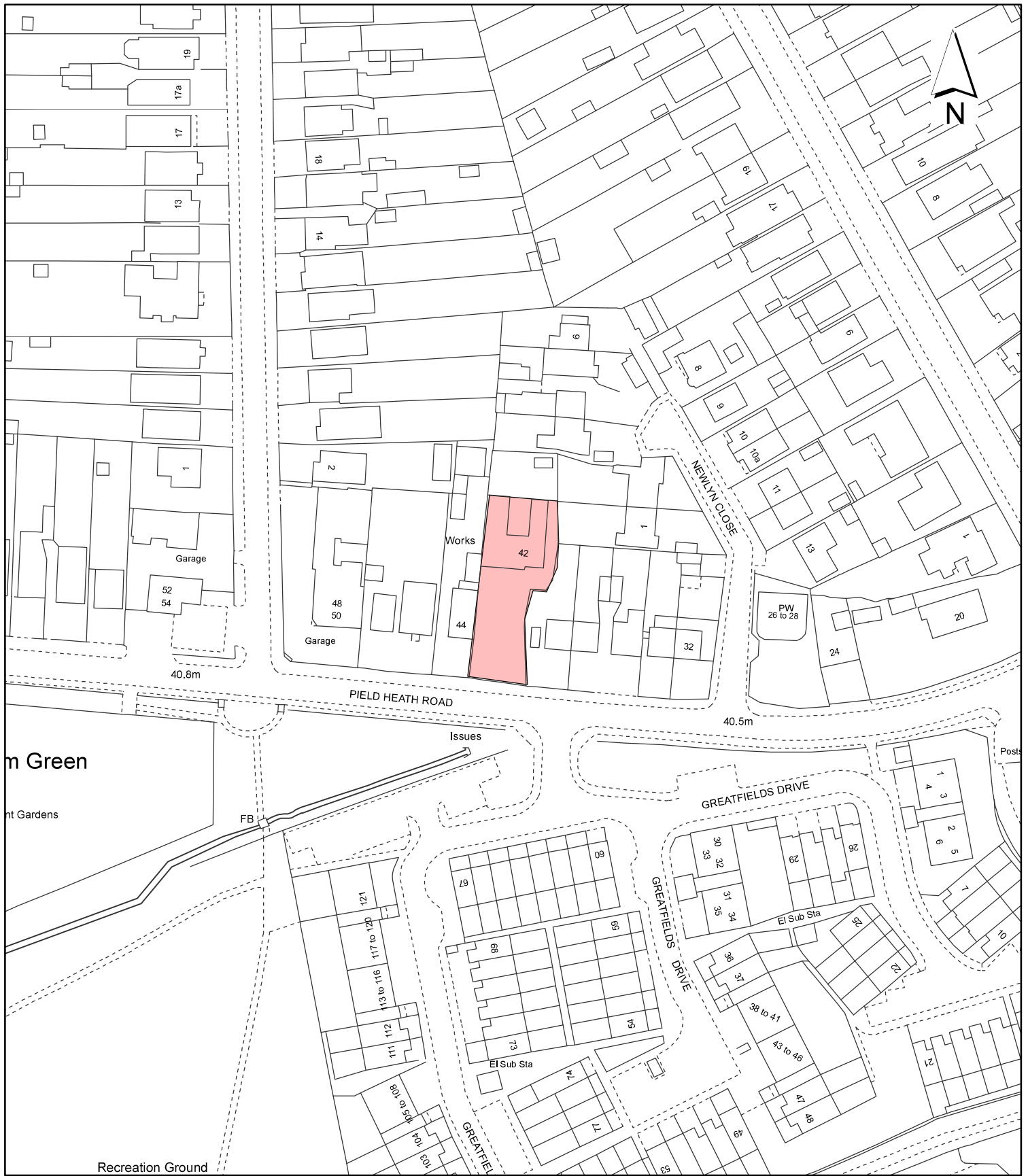
NORTH ELEVATION

SITE	
42 FIELD HEATH ROAD UXBRIDGE MIDDLESEX UB8 3NP	
SCALE BAR 0 1 2 4 6 8 MET	
PROPOSED NORTH & WEST ELEVATIONS	
SCALE:	DATE:
1:200 & A3	SEPT 2022
DRAWING NO	REV. A
555/B/08	A




SECTION A-A THRU' FRONT TO BACK

SITE:		42 FIELD HEATH ROAD UXBRIDGE MIDDLESEX UB8 3NG	
SCALE BAR:		0 1 2 3 4 5 6 7 8 9 10 cm	
DESCRIPTION:			
SECTION A-A			
SCALE:	DATE:		
1:200 @ A3	2022		
DRAWING NO.:	REV:		
555/B/09			



**Notes:**

 Site boundary

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Site Address:	
<b>42 Pield Heath Road</b>	
Planning Application Ref:	Scale:
<b>17611/APP/2022/993</b>	<b>1:1,250</b>
Planning Committee:	Date:
<b>BoroughPage 188</b>	<b>December 2022</b>

**LONDON BOROUGH OF HILLINGDON**

**Residents Services Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW  
Telephone No.: Uxbridge 01895 250111



**HILLINGDON**  
LONDON

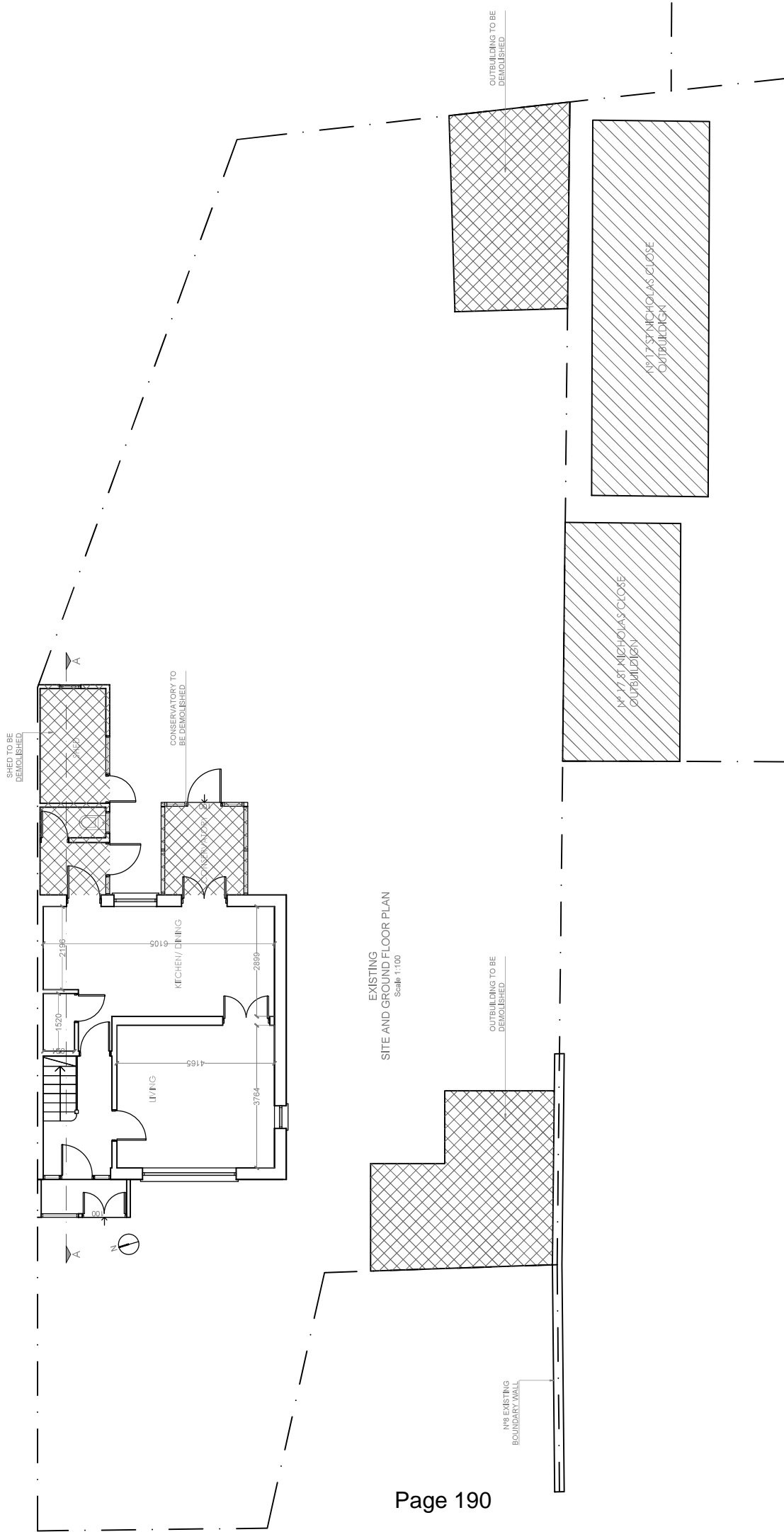
## **Report of the Interim Director of Planning, Regeneration & Public Realm**

**Address** 10 ST LUKE CLOSE COWLEY UXBRIDGE

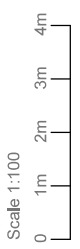
**Development:** Erection of a two storey side/rear extension and a single storey rear extension. Roof light located on rear roof slope with the demolition of outbuildings.

**LBH Ref Nos:** 36391/APP/2022/2104





EXISTING  
SITE AND GROUND FLOOR PLAN  
Scale: 1:100



NOTE: All the works must be certified by an approved building control inspector/ control inspector and Thames Water prior to works commencing  
Do not scale from this drawing

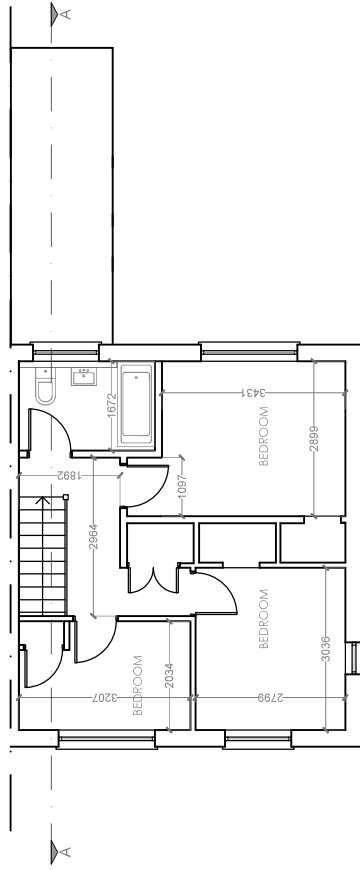
NOTE: Survey information regarding Nºs. 12 of Elpar St Lukes Close has been taken from ordinance survey maps, aerial images and photography

**10 ST LUKES CLOSE UXBRIDGE**  
LONDON

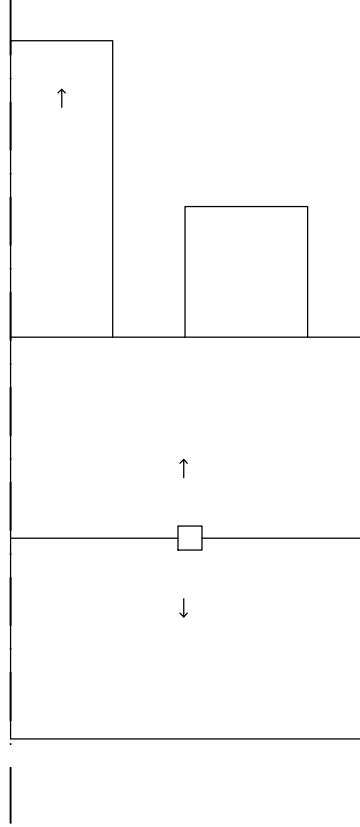
**EXISTING  
SITE AND GROUND FLOOR PLAN**

DATE	28-06-22
FILE	10 St Lukes Clos Uxbridge 28-06-22_01
LAYOUT	1
SCALE	1:100

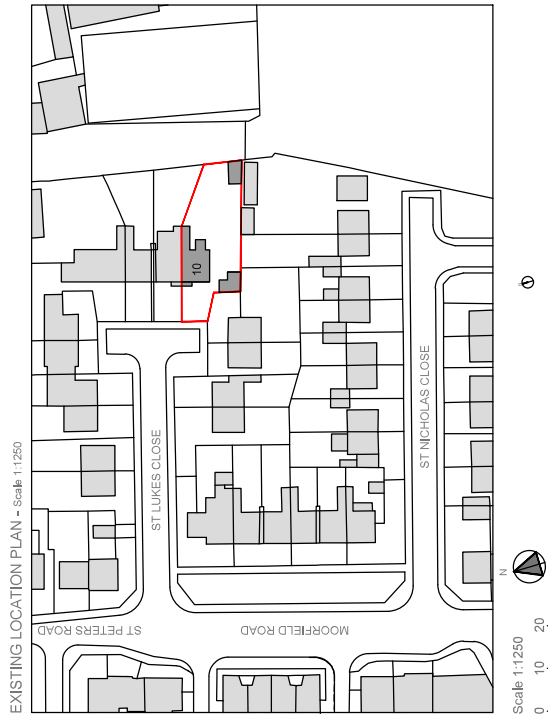
DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS  
DO NOT SCALE FROM THIS DRAWING  
CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE  
PLANT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY



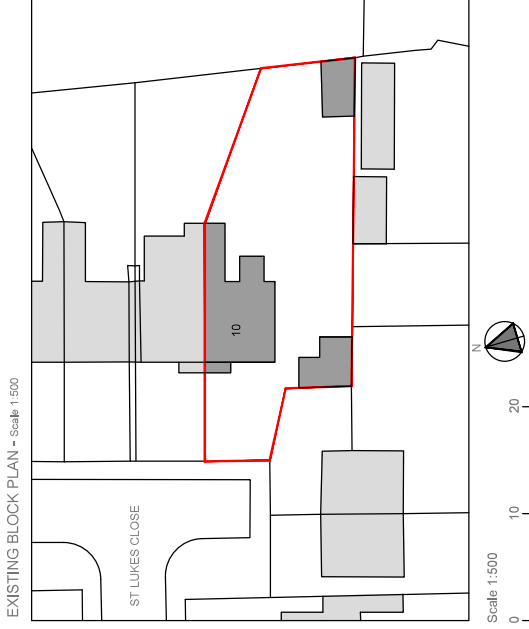
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FIRST FLOOR PLAN  
Scale 1:100



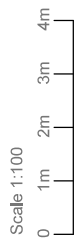
EXISTING ROOF PLAN  
Scale 1:100



EXISTING LOCATION PLAN - Scale 1:1250



EXISTING BLOCK PLAN - Scale 1:500



NOTE: All the works must be certified by an approved building control inspector and Thames Water prior to works commencing. Do not scale from this drawing.

NOTE: Survey information regarding N's 12 of Elpar St Lukes Close has been taken from ordinance survey maps, aerial images and photography.

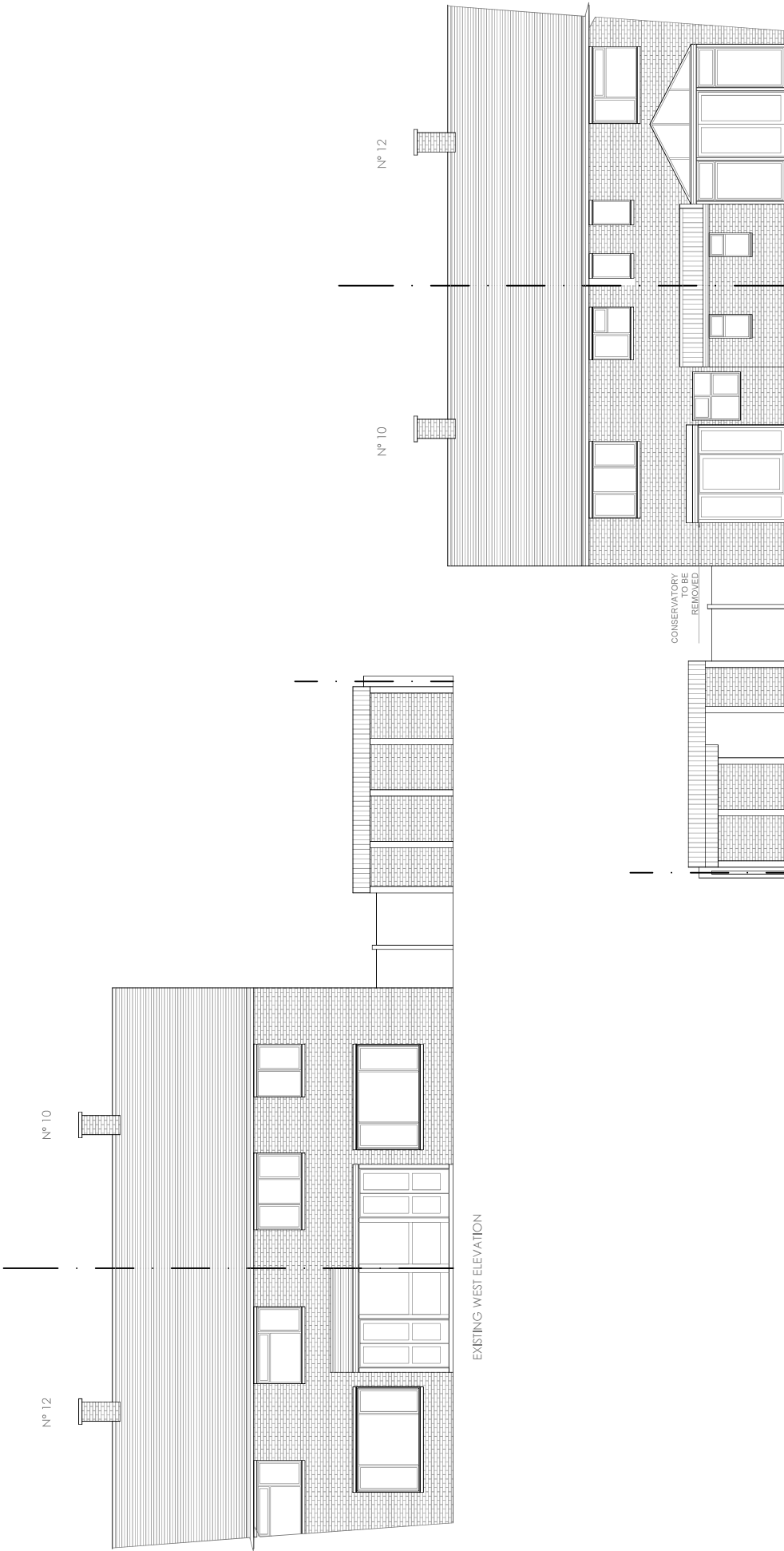
**10 ST LUKES CLOSE UXBRIDGE**

LONDON

**EXISTING  
FIRST FLOOR, ROOF, LOCATION AND BLOCK PLANS**

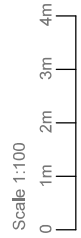
DATE	28-06-22
FILE	10 St Lukes Clos Uxbridge 28-06-22_01
LAYOUT	2
SCALE	1:110/ 1:1250/ 1:500

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS. DO NOT SCALE FROM THIS DRAWING. CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE. PLANTIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY.



NOTE: Survey information regarding N°s 12 of Elgar St Lukes Close has been taken from ordinance survey maps, aerial images and photography

NOTE: All the works must be certified by an approved building control inspector or control inspector and Thames Water prior to works commencing  
Do not scale from this drawing



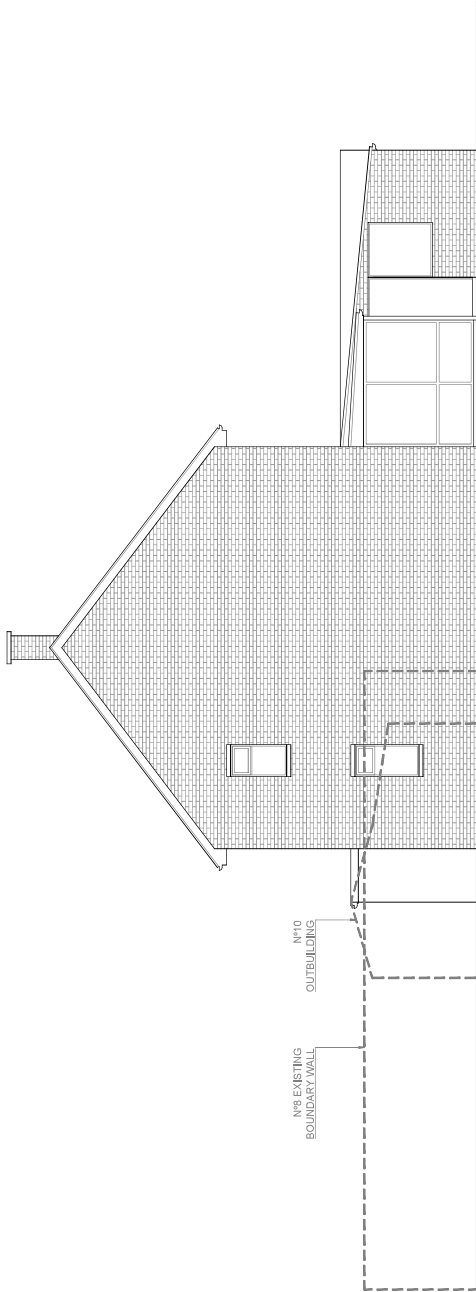
**10 ST LUKES CLOSE UXBRIDGE**  
LONDON

**EXISTING**  
**FRONT AND REAR ELEVATIONS**

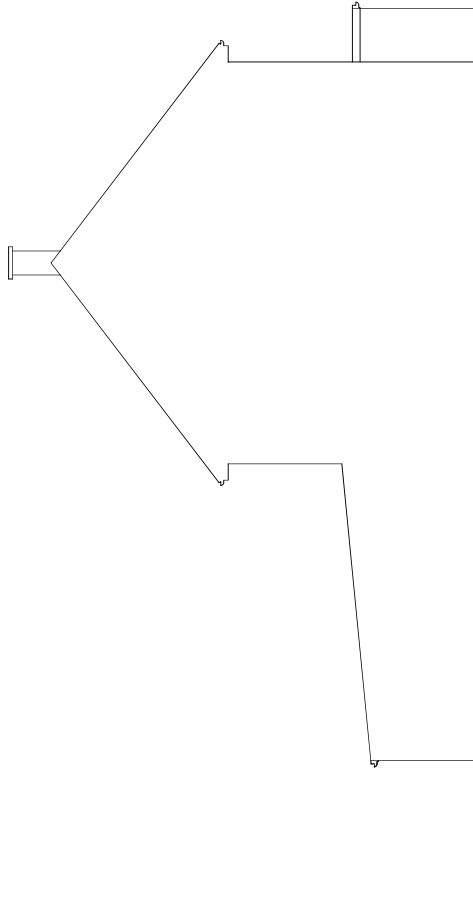
DATE 28-06-22  
FILE 10 St Lukes Close Uxbridge 28-06-22\_01  
LAYOUT 3  
SCALE 1:100

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS  
DO NOT SCALE FROM THIS DRAWING  
CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE  
PLANT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY

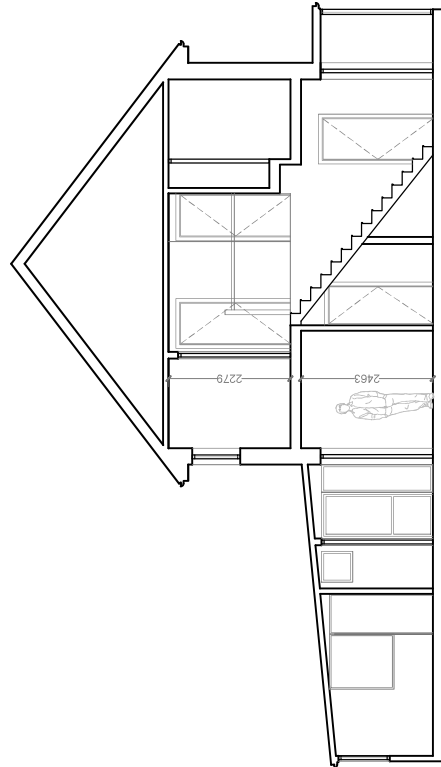




EXISTING SOUTH ELEVATION



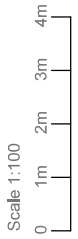
EXISTING NORTH ELEVATION



EXISTING SECTION A-A

NOTE: Survey information regarding N's 12 of Elgar St. Lukes Close has been taken from ordinance survey maps, aerial images and photography

NOTE: All the works must be certified by an approved building control inspector and Thames Water prior to works commencing  
Do not scale from this drawing



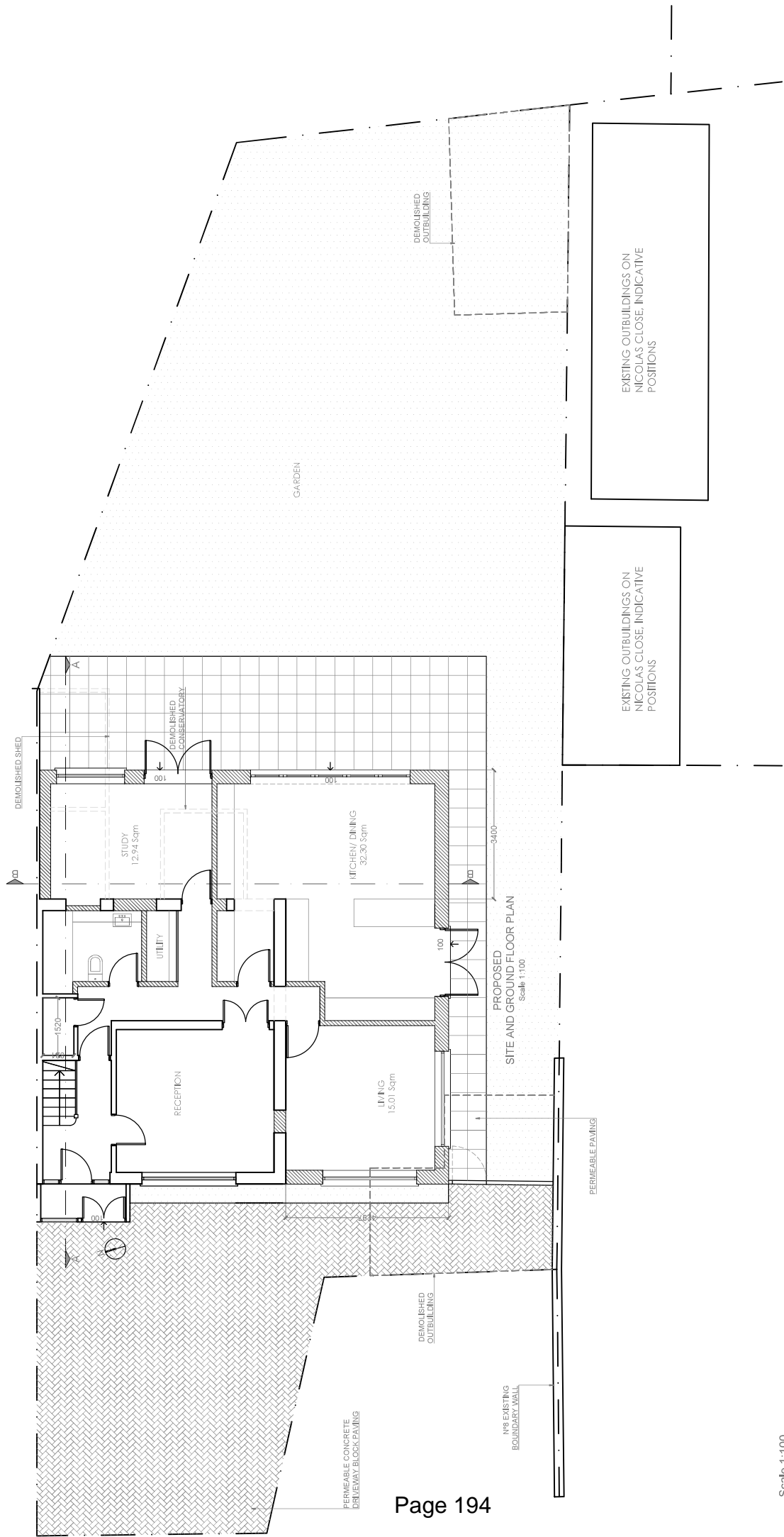
**10 ST LUKES CLOSE UXBRIDGE**

LONDON

**EXISTING  
SIDE ELEVATIONS - SECTION**

DATE	28-06-22
FILE	10 St Lukes Close Uxbridge 28-06-22_01
LAYOUT	4
SCALE	1:100

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS  
DO NOT SCALE FROM THIS DRAWING  
CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE  
PLANTIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY

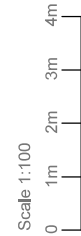


DATE	20-10-22
FILE	10 St Lukes Close Uxbridge 20-10-22_01
LAYOUT	5
SCALE	1:100

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS. DO NOT SCALE FROM THIS DRAWING. CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE. PLANTIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY.

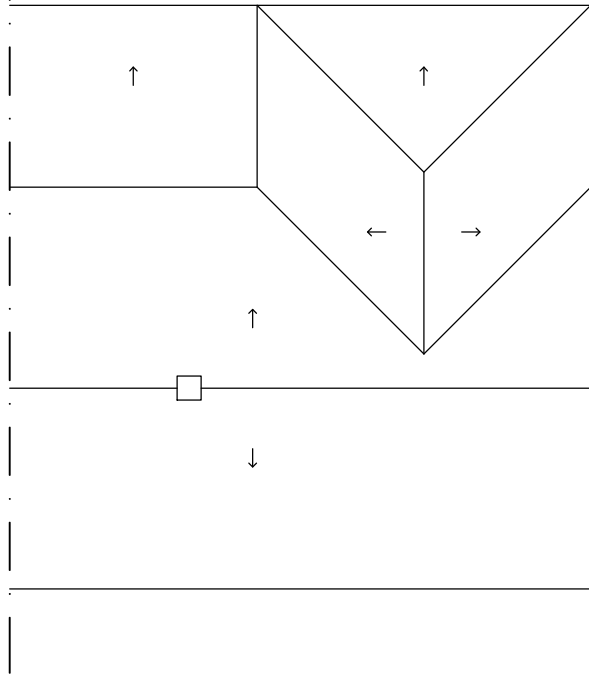
**10 ST LUKES CLOSE UXBRIDGE**  
LONDON

**PROPOSED**  
**SITE AND GROUND FLOOR PLAN**

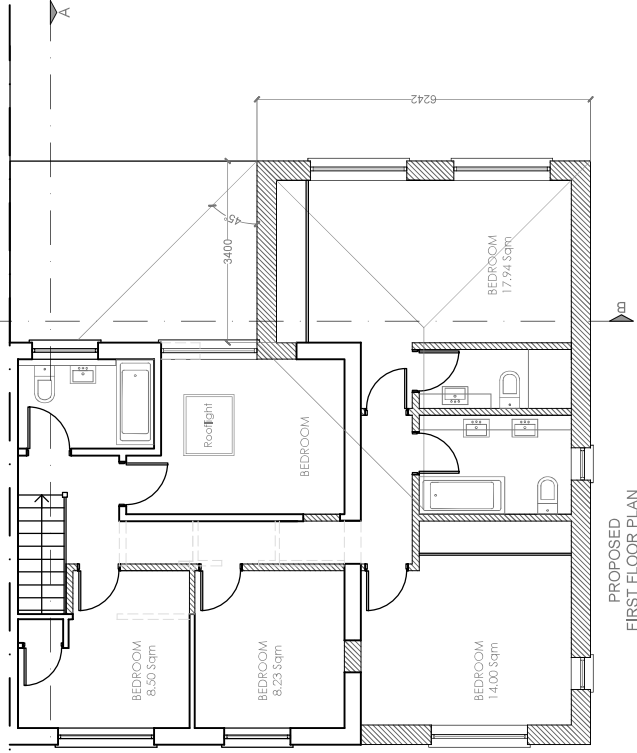


NOTE: Survey information regarding N's 12 of Elgar St Lukes Close has been taken from ordinance survey maps, aerial images and photography

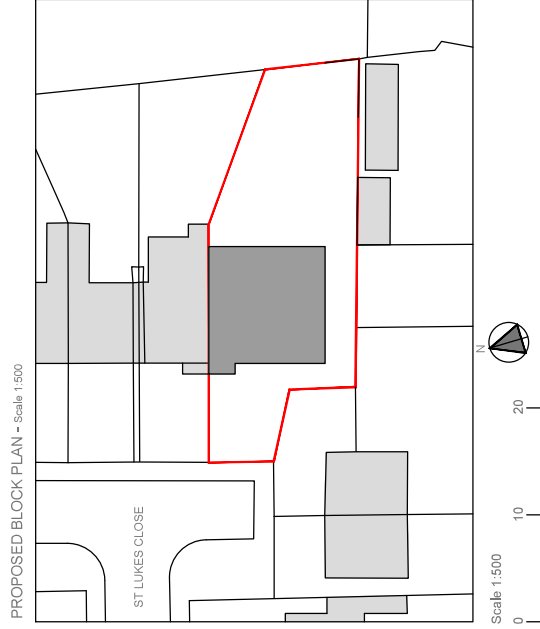
NOTE: All the works must be certified by an approved building control inspector/control inspector and Thames Water prior to works commencing. Do not scale from this drawing



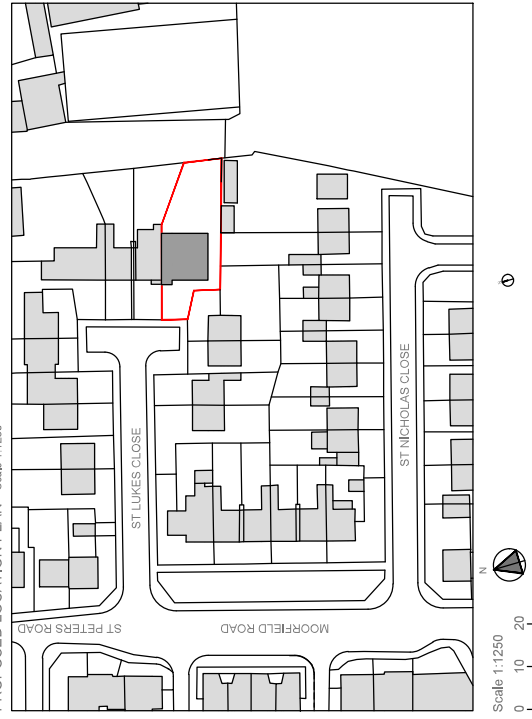
PROPOSED  
ROOF PLAN  
Scale 1:100



PROPOSED  
FIRST FLOOR PLAN  
Scale 1:100



PROPOSED BLOCK PLAN - Scale 1:500



PROPOSED LOCATION PLAN - Scale 1:1250

DATE	28-06-22
FILE	10 St Lukes Close Uxbridge 28-06-22_01
LAYOUT	6
SCALE	1:100

**10 ST LUKES CLOSE UXBRIDGE**

LONDON

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS. DO NOT SCALE FROM THIS DRAWING. CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE. PLANIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY.

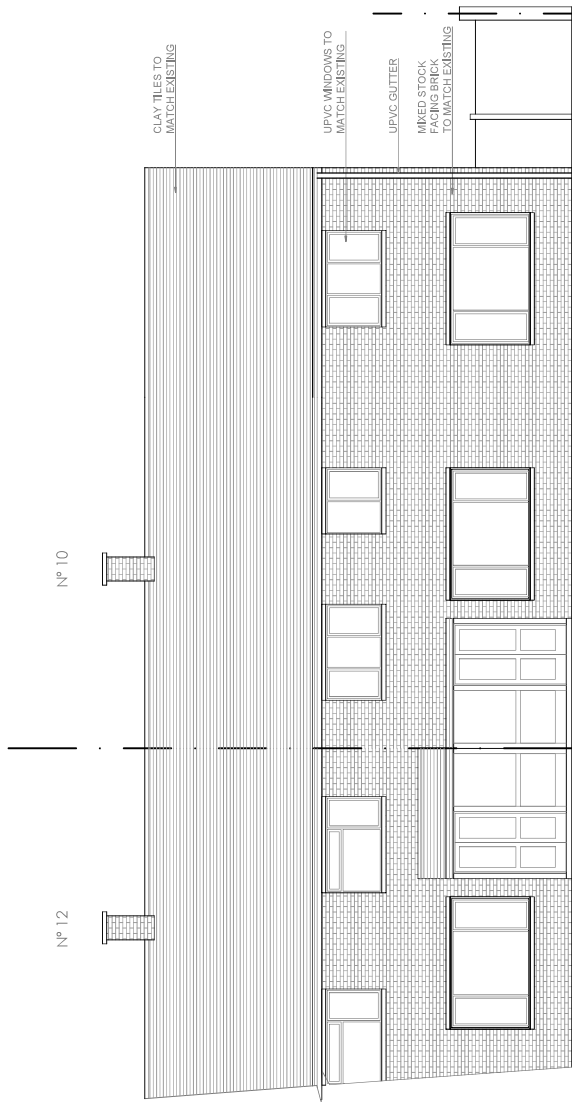
**PROPOSED  
FIRST AND ROOF PLANS**



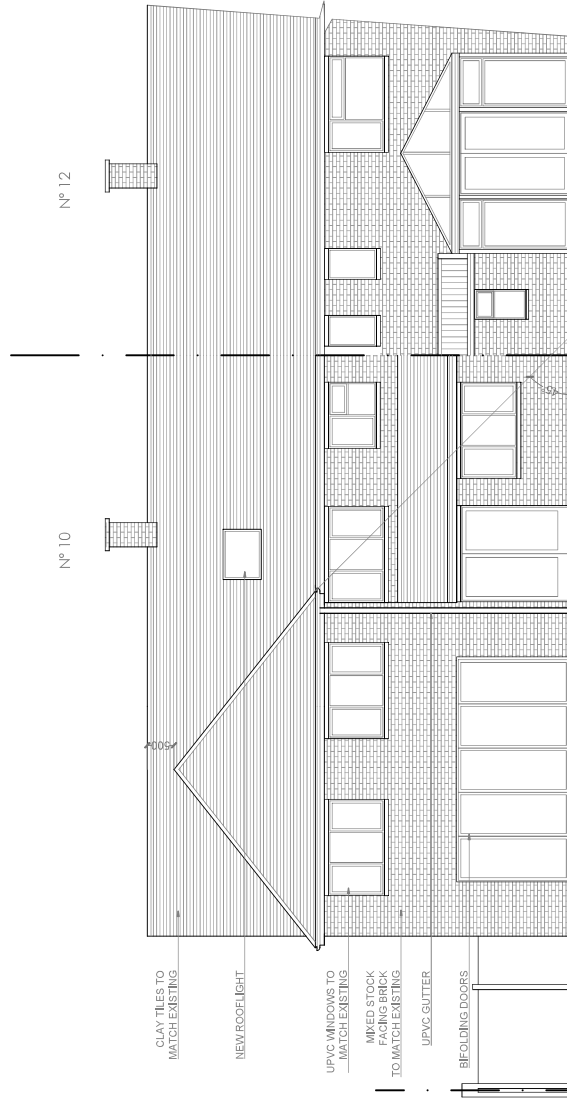
Scale 1:100

NOTE: All the works must be certified by an approved building control inspector/inspector and Thames Water prior to works commencing. Do not scale from this drawing.

NOTE: Survey information regarding N°s. 12 of Elpar St Lukes Close has been taken from ordinance survey maps, aerial images and photography.



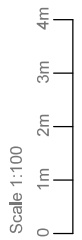
PROPOSED WEST ELEVATION



PROPOSED EAST ELEVATION

NOTE: Survey information regarding N°s 12 of Elgar St Lukes Close has been taken from ordinance survey maps, aerial images and photography

NOTE: All the works must be certified by an approved building control inspector and Thames Water prior to works commencing  
Do not scale from this drawing



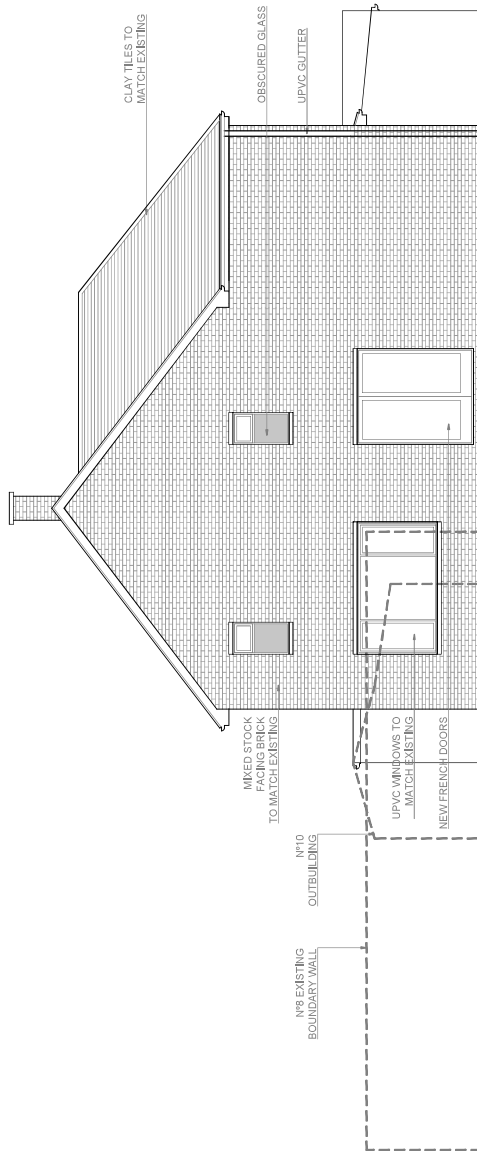
DATE 28-06-22  
FILE 10 St Lukes Close Uxbridge 28-06-22\_01  
LAYOUT 7  
SCALE 1:100

**10 ST LUKES CLOSE UXBRIDGE**

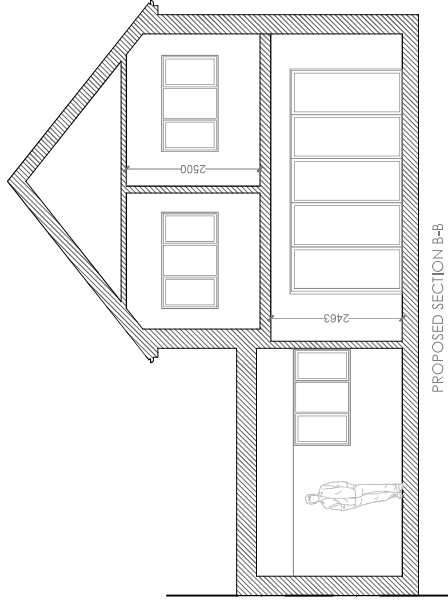
LONDON

**PROPOSED  
FRONT AND REAR ELEVATIONS**

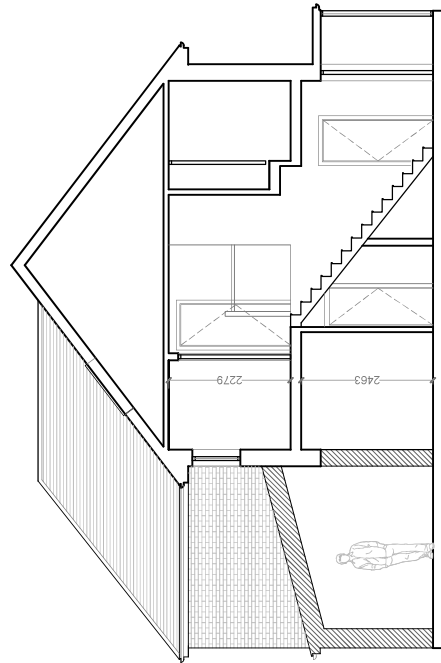
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CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE  
PLANTIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY



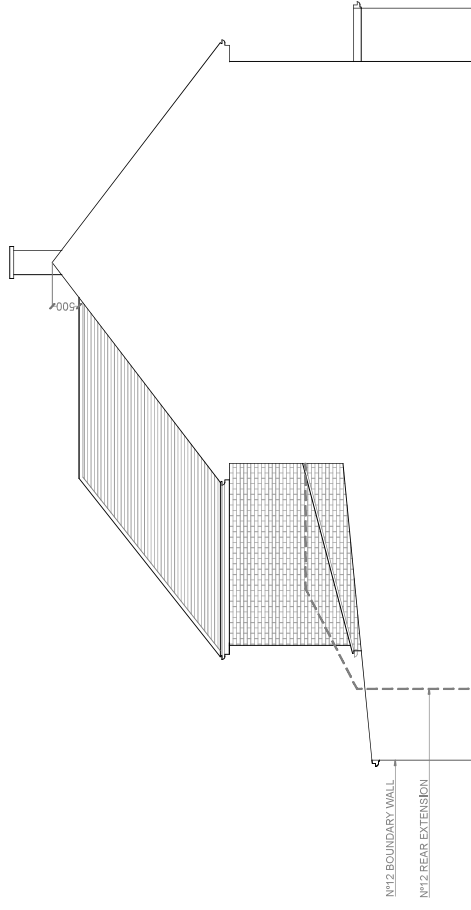
PROPOSED SOUTH ELEVATION



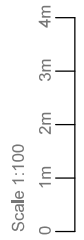
PROPOSED SECTION B-B



PROPOSED SECTION A-A



PROPOSED NORTH ELEVATION



Scale 1:100

NOTE: Survey information regarding N's 12 of Elgar St Lukes Close has been taken from ordinance survey maps, aerial images and photography

NOTE: All the works must be certified by an approved building control inspector and Thames Water prior to works commencing  
Do not scale from this drawing

**10 ST LUKES CLOSE UXBRIDGE**

LONDON

**PROPOSED  
SIDE ELEVATIONS - SECTION**

DATE 28-06-22  
FILE 10 St Lukes Close Uxbridge 28-06-22\_01  
LAYOUT 8  
SCALE 1:100

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS  
DO NOT SCALE FROM THIS DRAWING  
CONTRACTOR TO CHECK ALL DIMENSIONS ON SITE  
PLANTIT TO BE ADVISED OF ANY DISCREPANCIES BETWEEN THIS DRAWING AND SITE CONDITIONS IMMEDIATELY



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NOTE: Survey information regarding N's 12 of Elgar St Lukes Close has been taken from ordinance survey maps, aerial images and photography

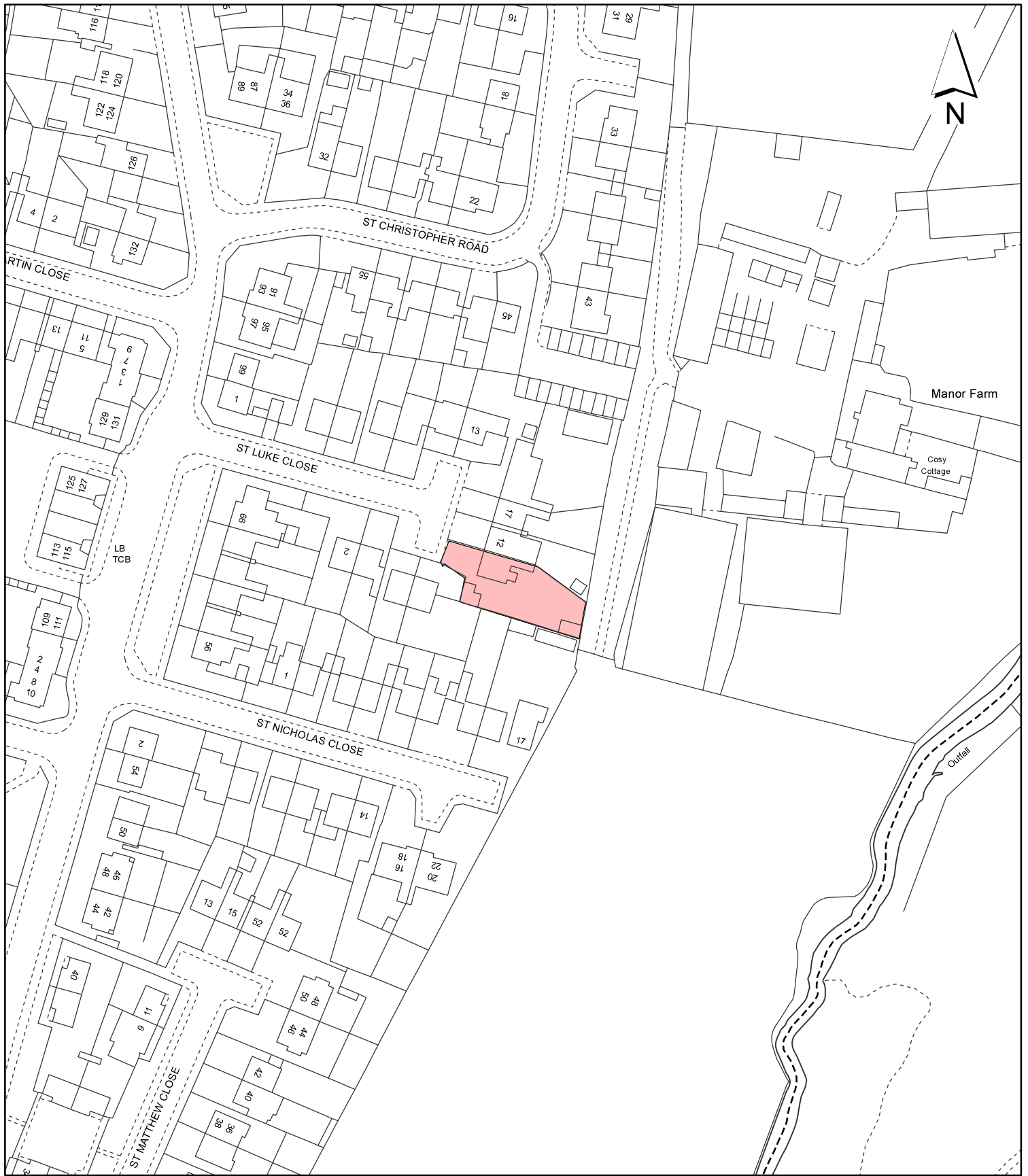
DATE 28-06-22  
FILE 10 St Lukes Close Uxbridge 28-06-22\_01  
LAYOUT  
SCALE 10

**10 ST LUKES CLOSE UXBRIDGE**  
LONDON

**PROPOSED  
VIEWS**

DRAWING TO BE READ IN CONJUNCTION WITH THE SPECIFICATION AND ALL RELEVANT DRAWINGS  
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**Notes:**

 Site boundary

For identification purposes only.

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Site Address:

**10 St Luke Close**

**LONDON BOROUGH OF HILLINGDON**  
Residents Services  
Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**36391/APP/2022/2104**

Scale:

**1:1,250**

Planning Committee:

**BoroughPage 199**

Date:

**December 2022**



**HILLINGDON**  
LONDON

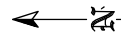
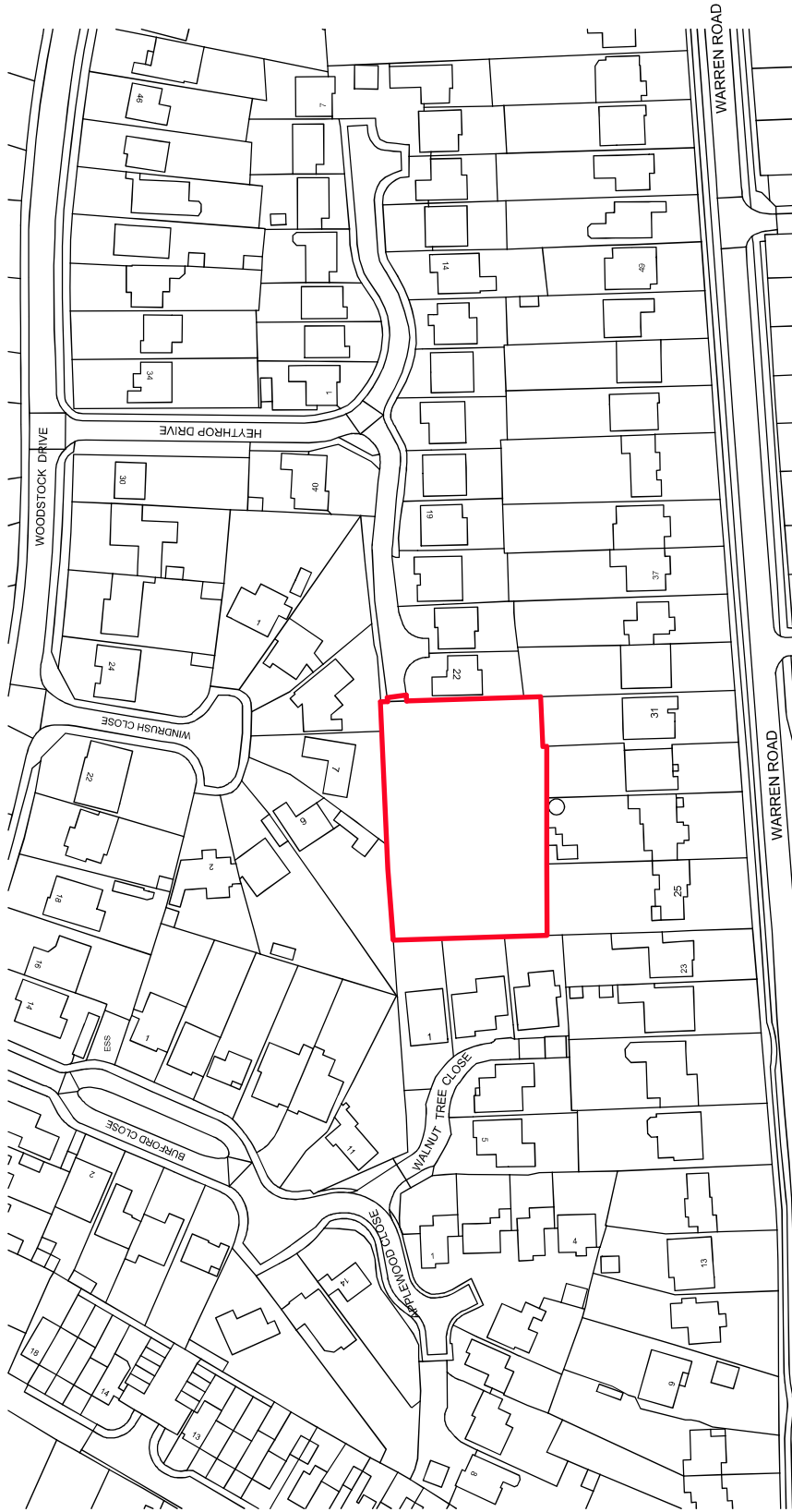
## **Report of the Interim Director of Planning, Regeneration & Public Realm**

**Address** R/O 25-31 WARREN ROAD ICKENHAM

**Development:** Erection of 4 no. two storey, detached houses, with habitable accommodation in roof space, garage and associated landscaping, parking and installation of vehicular crossover.

**LBH Ref Nos:** 77265/APP/2022/2845





11250



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LAND 1/0 25-31 WARREN ROAD,  
 ACCESS OFF HEYTHROP DRIVE, ICKENHAM.

**W J Macleod**  
 ARCHITECT  
 720 19th Street, Northwood, Middlesex, ON L4B 1B1  
 Phone: (905) 882-8200

Project No.	22 / 3521 / 18	Sheet	A
Date	11 / 8 / 22	Scale	1:500 @ A1

Block Plan



REV. A - Garage roof Plot 2 adjusted & tree information updated, 24-10-22

WINDRUSH CLOSE

HEYTHROP DRIVE

SITE PLAN

0 20m

PROJECT NUMBER	22 / 3521 / 10
DATE	11 / 8 / 22
SCALE	1:200 @ A1
PROJECT	A

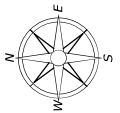
**W J Macleod**  
ARCHITECT  
750 High Street, Northwood, Middlesex HA6 1BL  
Phone: 02089 50000

LAND 1/0 25-31 WARREN ROAD,  
ACCESS OFF HEYTHROP DRIVE, TICKENHAM.




33 WARREN ROAD

25 WARREN ROAD

WALNUT TREE CLOSE



KEY

-  EXISTING TREES RETAINED.
-  EXISTING TREES REMOVED.
-  INDICATIVE PLANTING (SEE SPECIFICATIONS).



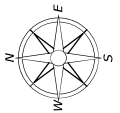
REV. A - Notes added, car charging points & rubbish collection. Tree information updated. 24-10-22

ANNOTATED SITE PLAN

**W J Macleod**  
ARCHITECT  
750 High Street, Northwood, Middlesex HA6 1RL  
Phone: 0208 8999999

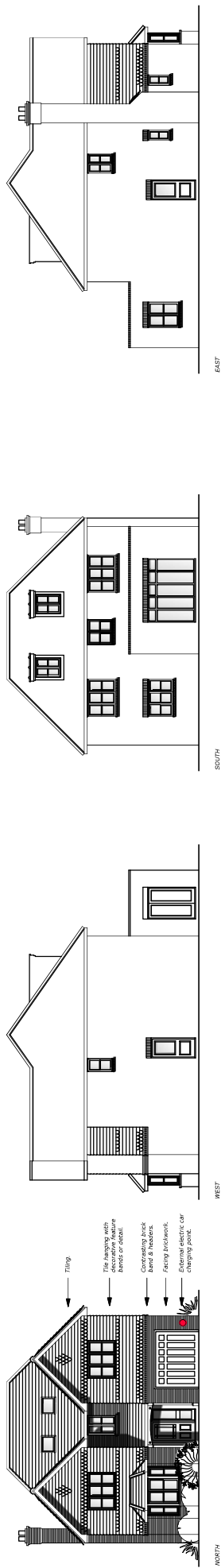
LAND 1/0 25-31 WARREN ROAD,  
ACCESS OFF HEYTHROP DRIVE, TCKENHAM.

Project No.	22 / 3521 / 11
Phase	A
Date	11 / 8 / 22
Scale	1:200 @ A1



KEY

- EXISTING TREES REMAIN.
- EXISTING TREES REMOVED.
- EXISTING CAR CHARGING POINTS.
- NEW CAR CHARGING POINTS.
- EXISTING RUBBISH COLLECTION POINTS.
- NEW RUBBISH COLLECTION POINTS.
- EXISTING TREE PROTECTION ZONES.
- NEW TREE PROTECTION ZONES.

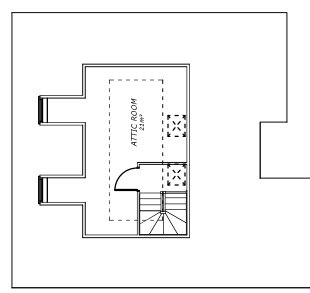


EAST

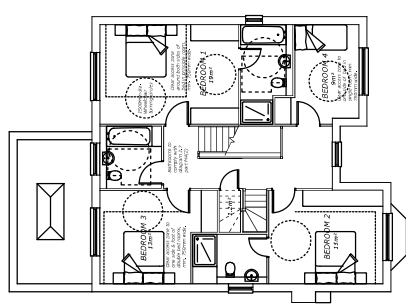
SW

WEST

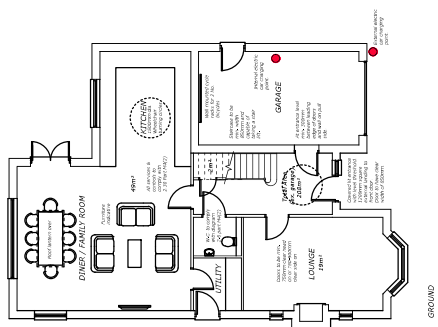
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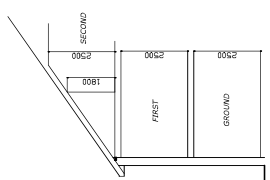
SECOND



FIRST

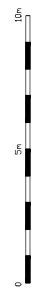


GROUND



SECTION

Floor Plans & Elevations Plot 1

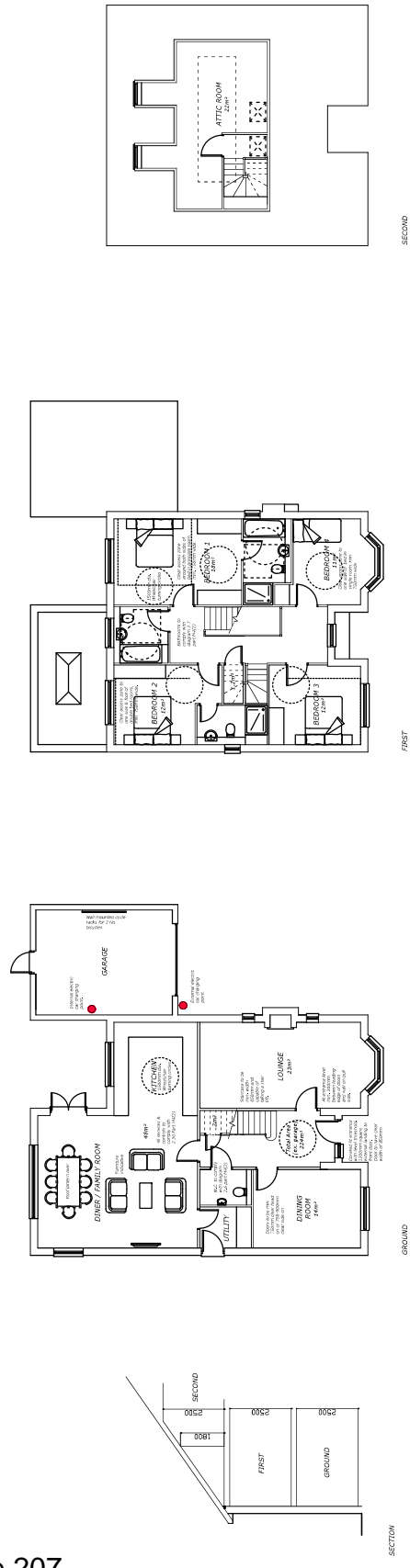
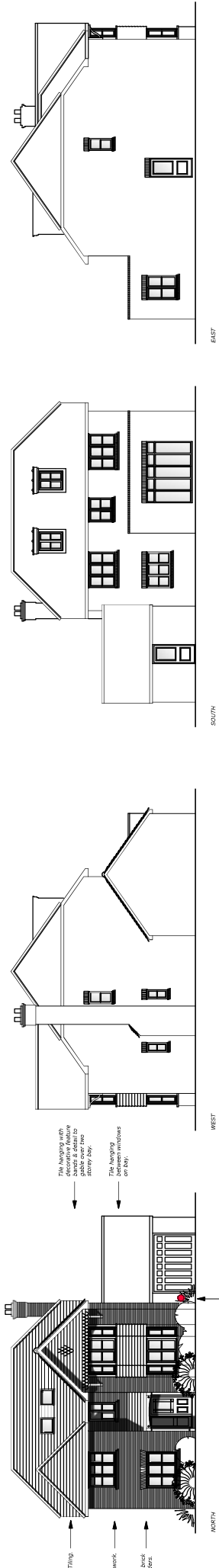


PROJECT NO.	22 / 3521 / 12
DATE	11 / 8 / 22
SCALE	1:100@A1

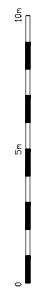
**W J Macleod**  
 ARCHITECT  
 176 High Street, Northwood/Middlesex HA6 1EL  
 phone: 01232 840600

LAND r/o 25-31 WARREN ROAD,  
 ACCESS OFF HEYTHROP DRIVE, ICKENHAM.

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 REV. A - Notes added to floor plans, section added & car charging point note to elevation.  
 Garage roof adjusted. 24-10-22



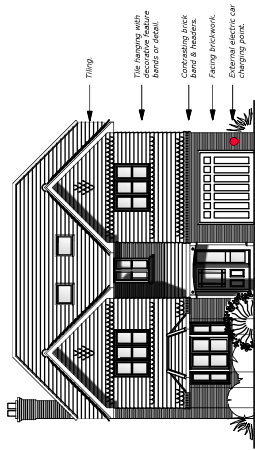
Floor Plans & Elevations Plot 2



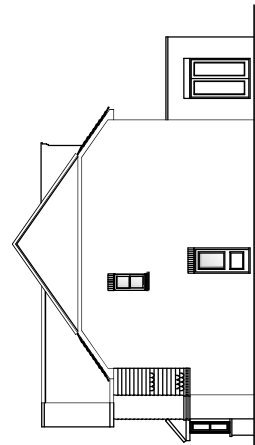
Project Number	22 / 3521 / 13
Date	11 / 8 / 22
Drawn	J:100/A1

**W J Macleod**  
 ARCHITECT  
 176 High Street, Northwood, Middlesex, HA6 1LE  
 phone: 01872 840000

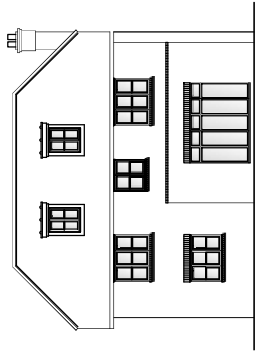
LAND r/o 25-31 WARREN ROAD,  
 ACCESS OFF HEYTHROP DRIVE, ICKENHAM.



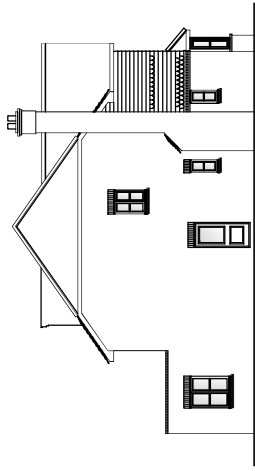
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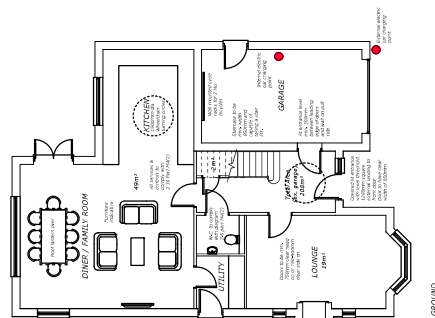
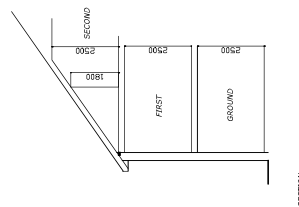
WEST



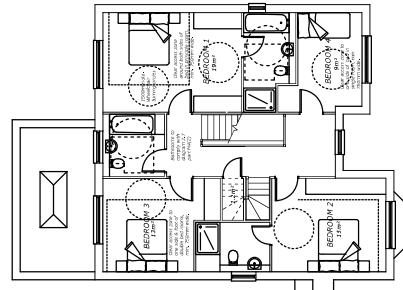
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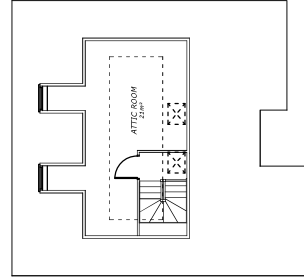
EAST



GROUND



FIRST



SECOND

Floor Plans & Elevations Plot 3

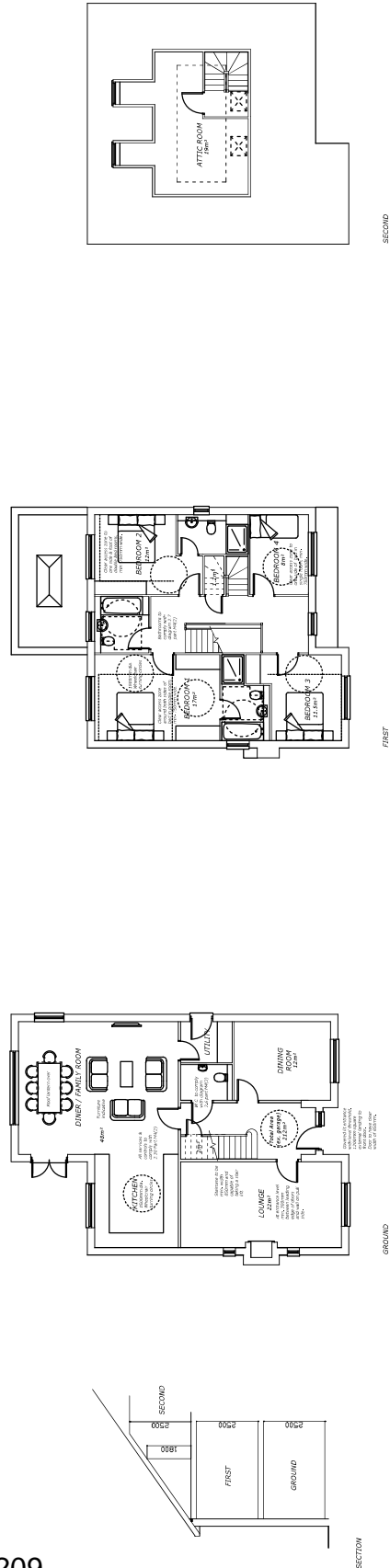
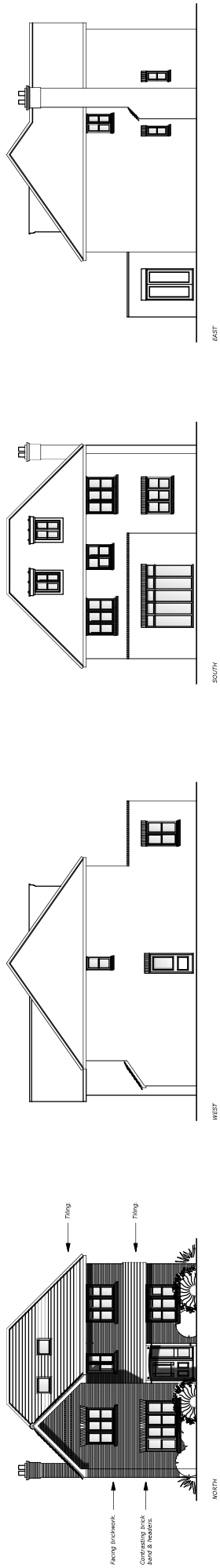


LAND r/o 25-31 WARREN ROAD,  
 ACCESS OFF HEYTHROP DRIVE, ICKENHAM.

**W J Macleod**  
 ARCHITECT  
 178 High Street, Northwood/Hatfield, MK45 1BL  
 phone: 01235 840600

Project Number	22 / 3521 / 14	Sheet	A
Date	11 / 8 / 22	Scale	1:100@A1





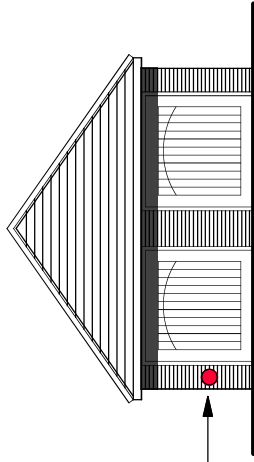
Floor Plans & Elevations Plot 4



Project Number	22 / 3521 / 15
Date	11 / 8 / 22
Scale	1:100@A1

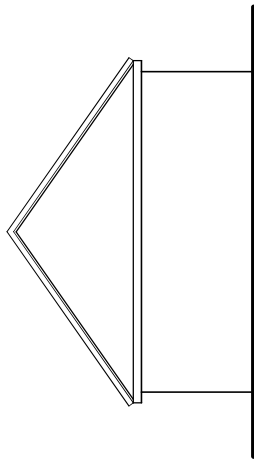
**W J Macleod**  
 ARCHITECT  
 708 High Street, Northwood/Hallam, WYO, WIL.  
 phone: 01223 840000

LAND r/o 25-31 WARREN ROAD,  
 ACCESS OFF HEYTHROP DRIVE, ICKENHAM.

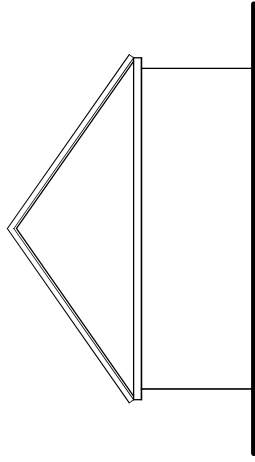


External electric car charging point.

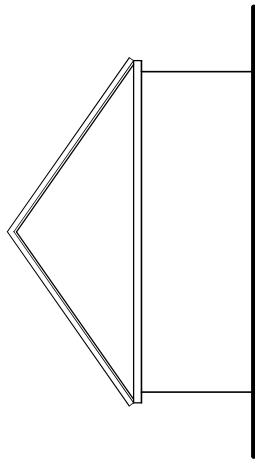
East



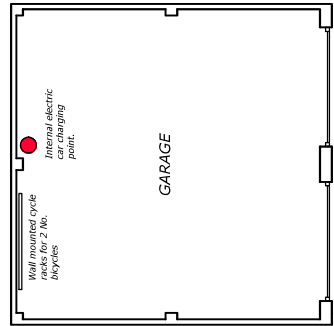
North



West



South



Wall mounted cycle racks for 2 bicycles

Internal electric car charging point.

GARAGE

External electric car charging point.

Garage Plans and Elevations Plot 4



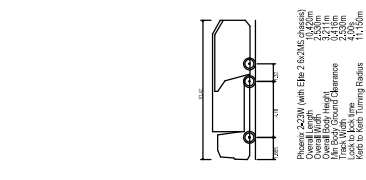
LAND r/o 25-31 WARREN ROAD,  
ACCESS OFF HEYTHROP DRIVE, ICKENHAM.

**W J Macleod**  
ARCHITECT  
70 High Street, ICKENHAM, GL53 6JH  
Tel: 01292 462001

PROJECT NO.	22 / 3521 / 16	DATE	11 / 8 / 22
SCALE	1:100 @ A3		

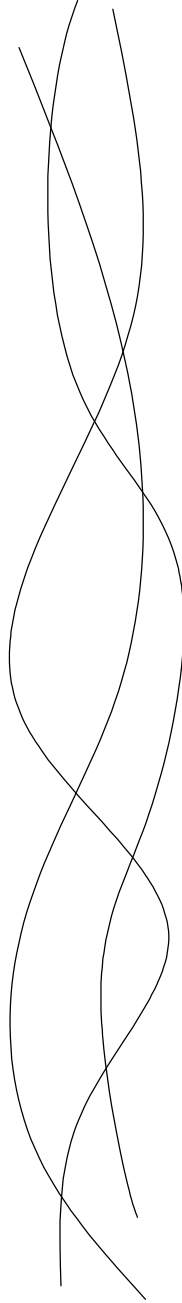


- NOTES:
- Do not scale from this drawing. This drawing shall be read in conjunction with all other relevant Architects and Engineers drawings.
  - Any discrepancies, ambiguities, or omissions in the information provided on this drawing or any of the engineering drawings package must be reported prior to work proceeding.
  - All accommodation work deemed necessary to facilitate a satisfactory link between the new works and the existing to be undertaken by the developer.
  - It is the contractor's responsibility to locate all existing services and verify their levels & locations prior to commencing any works. Checks may be affected by the nature and location of the new works. The contractor shall coordinate with any special protection requirements agreed.
  - All highway works shall be undertaken in accordance with London Borough of Hillingdon Highway Design Guide and Specification and strictly in accordance with the Specification for Highway Works. All drainage works shall be undertaken in accordance with the Design & Construction Guidance, and any other Highway Water requirements. All works to be supervised/inspected as required by the relevant Inspector.
  - Access must comply with current health and safety guidance & standards. All temporary signing to comply with Traffic Signs Manual - Chapter 10.
  - All products are to be installed and maintained strictly in accordance with manufacturer's recommendations & guidelines.



**PRELIMINARY**

Rev:	Date:	Description:
Client:	WE Black Ltd	
Project:	LAND T/O WARREN DRIVE OFF HEYTHROP DRIVE, ICKENHAM.	
Drawing Title:	Refuse Vehicle Auto Track Layout - Turn	
 Hillingdon Business Centre, Clifton House, Hillingdon, Bucks: HP17 8BY. ☎ 01844 396333 ✉ www.bealuk.com		
Drawn:	SH	Checked:
Date:	12/08/2022	Approved:
Scale:	1:200 @ A1	Revision:
Drawing Number:	567,001	



STREET SCENE / SITE SECTION

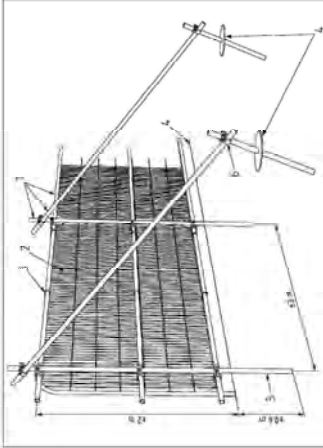
0 20m

LAND 1/0 25-31 WARREN ROAD,  
 ACCESS OFF HEYTHROP DRIVE, TCKENHAM.

**W J Macleod**  
 ARCHITECT  
 750 High Street, Northwood, Middlesex HA6 1RL  
 Phone: 01895 829699

Project No.	22 / 3521 / 17	Revision	A
Date	11 / 8 / 22	Scale	1:200 @ A1

**TPF SPECIFICATION**



- Key
1. Geotextile surface profile
  2. Heavy gauge 2 m x 100mm x 2mm steel welded mesh (with 50mm overlap)
  3. 200mm x 200mm x 10mm steel reinforcement
  4. 100mm x 100mm x 10mm steel reinforcement
  5. 100mm x 100mm x 10mm steel reinforcement
  6. 100mm x 100mm x 10mm steel reinforcement
  7. 100mm x 100mm x 10mm steel reinforcement
  8. 100mm x 100mm x 10mm steel reinforcement

**NO-DIG HARD SURFACING PRELIMINARY SPECIFICATION**

In the positions shown on this plan by cyan hatching where new hard surfacing is proposed over the RPA of retained trees it shall be constructed above ground level using a no-dig three-dimensional cellular confinement system. Construction shall comprise:

- A geotextile membrane (eg Terram) laid on the ground
- 3-dimensional cellular confinement such as Geosynthetic: celweb filled with clean, angular, 20-40mm stone containing no fines
- Edge constraints above ground to comprise, for example timber sleepers or pinned kerbstones in concrete haunching
- A 200mm wide wearing course

The ground protection must provide adequate support for access while minimising ground disturbance to an acceptable level. There shall be no vehicular access permitted over this area until the ground has been protected

**NO-DIG** means that, with the exception of turf removal, there shall be no digging during its construction

**4 DETAILED SITE-SPECIFIC CONSTRUCTION DESIGN SHALL BE PREPARED BY AN ENGINEER OR PRODUCT MANUFACTURER PRIOR TO INSTALLATION**

**ARBORCULTURAL SUPERVISION**

A qualified arboriculturist shall provide on-going site supervision during work at this site.

The critical times when site supervision is required are:

- Prior to construction work commencing, hold a pre-commencement site meeting. This meeting shall be attended by site managers and contractors. The meeting shall discuss the proposed work and the arboriculturist are providing throughout the construction process. During this meeting a site supervisor will be appointed to take responsibility for tree protection and to report any breaches.
- Following installation of TPF (solid red lines), and ground protection (dark blue hatching)
- Prior to installation of the no-dig access
- Prior to the commencement of landscaping work following completion of primary construction

**LANDSCAPING WITHIN RPAs**

After construction has been completed the tree protection fencing and ground protection shall be removed to allow landscaping to take place. Prior to landscaping commencing the project arboriculturist shall meet the landscaping contractors to discuss what is proposed and any precautions required.

The following principles shall be followed within the RPA of retained trees:

- No machinery shall pass over the ground unless protected by temporary ground protection
- If excavation is required, this shall be localised and undertaken carefully by hand tools only ensuring that roots are preserved
- No changes in levels, unless agreed by the project arboriculturist

**NOTE:**

No work shall commence on site until an arboricultural pre-commencement meeting has taken place

**SUMMARY OF PROPOSED TREE WORK**

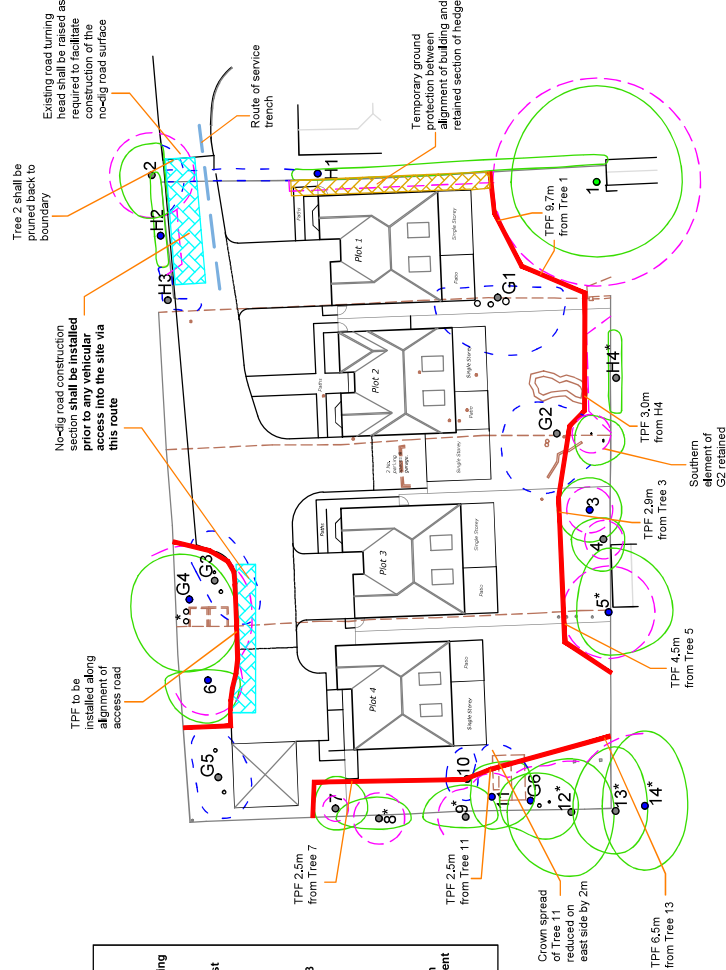
Tree	Species	Grade	Reason for removal
2	Hawthorn	C	Prune back overhanging branches to edge of proposed access
11	Cherry-plum	B2	Reduce spread on east side by 2m
G1	Leyland cypress	C	Remove to facilitate development
G2	Various species	C	Remove to facilitate trees at southern end
G3	Various species	C	Remove to facilitate development
G5	Apple, pear	C	Remove to facilitate development
H1	Lawson cypress	B2	Remove northern 12m to facilitate development
H3	Photinia laurel	C	Remove to facilitate development

**SERVICES AND DRAINAGE**

All utility and drainage trenches are to be positioned outside the RPA of retained trees. The service trench entering the site shall be positioned along the southern edge of the access road

**This drawing must be viewed in colour**

For details of trees shown on this plan, refer to the Tree Survey Report dated 19th May 2022



**ARBORCULTURAL METHOD STATEMENT**

Work on site shall proceed in the following sequential order:

1. Prior to work starting there shall be a pre-commencement meeting between the appointed project arboriculturist and the site contractors. The purpose of this is to ensure that contractors are fully briefed in, and understand the details of, this Tree Protection Plan.
2. Prior to any ground works commencing on site the following measures for the protection of trees shall be installed:
  - Tree Protection Fencing (TPF) based on a scaffold framework per the detail in the top-left corner of this plan. TPF shall be positioned as shown on this plan by solid red lines.
  - No-dig hard surfacing where the new access road is to be constructed over the RPA of retained trees, as indicated on this plan by cyan hatching.
  - This shall comprise a three-dimensional cellular-confinement system per the specification in the box on the left side.
  - Temporary ground protection between the alignment of the proposed house on Plot 1 and the retained section of the hedge H1
3. The measures installed for tree protection comprise the Construction Exclusion Zone. Refer to the box on the left side setting out prohibitions within CEZ. The TPF and temporary ground protection shall be retained in-situ for the duration of the development process until the commencement of soft landscaping.
4. Install services into the site along the southern edge of the access road to avoid the RPA of off-site Tree 2. There shall be no trenching for services or drainage through the RPA of retained trees.
5. Following the completion of the construction of the houses, the TPF and ground protection shall be removed to facilitate landscaping work. Precautions are required during the landscaping phase - refer to the detail in the bottom left corner of this plan. There shall be an arboricultural supervision visit immediately prior to the commencement of landscaping work.
6. An appointed project arboriculturist shall supervise work, with site meetings at the key times set out in the Arboricultural Supervision box on the left side of this plan, and at other times if required during the development process. Following each site visit a summary report setting out what was seen and discussed shall be prepared.

**TREE PROTECTION PLAN**

**SITE ADDRESS**  
25-31 Warren Road, Ickenham, UB10 8AA

**CLIENT**  
WE Black Ltd

**JOB REF**  
DS20042201

**DRAWING NO**  
DS20042201.03

**DATE**  
26/10/2022

Patrick Stilleman Ltd  
9 Chestnut Drive, Berkhamsted, Herts, HP4 2JL 01442 866112

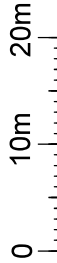
**KEY**

- Tree / tree group for retention
- Tree / tree group for removal
- Root Protection Area (RPA) for retained tree / tree group
- Existing structure or feature for removal
- Position of Tree Protection Fencing (TPF), installed prior to the commencement of work on site
- No-dig hard surfacing construction
- Temporary ground protection

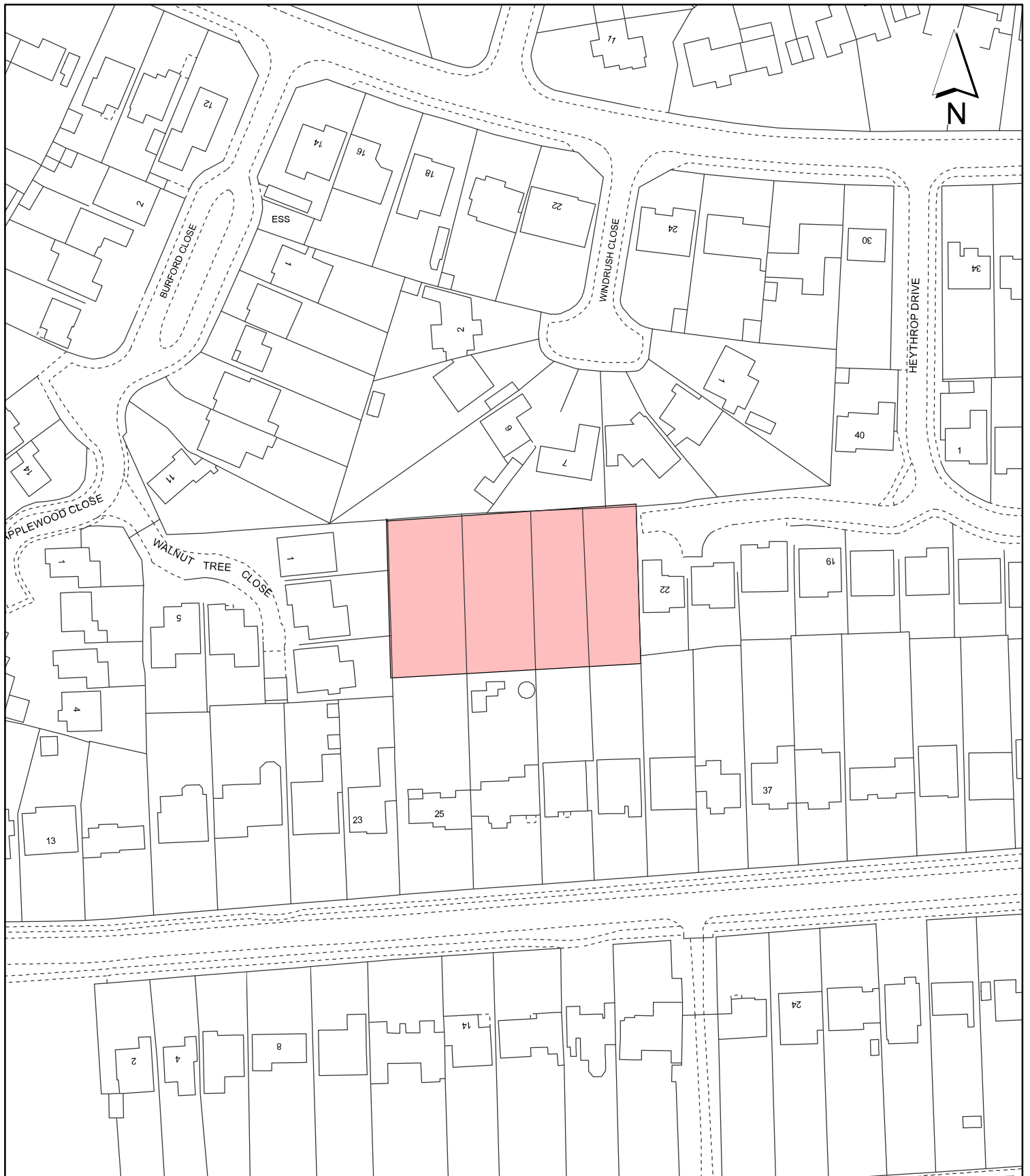
**BS 5837 category key**

- Category U tree
- Category A tree
- Category B tree
- Category C tree

**SCALE**  
1:500 @ A3







**Notes:**

Site boundary

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Site Address:

**R/O 25-31 Warren Road**

**LONDON BOROUGH OF HILLINGDON**  
Residents Services  
Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**77265/APP/2022/2845**

Scale:

**1:1,250**

Planning Committee:

**BoroughPage 215**

Date:

**December 2022**



**HILLINGDON**  
LONDON

**Report of the Interim Director of Planning, Regeneration & Public Realm**

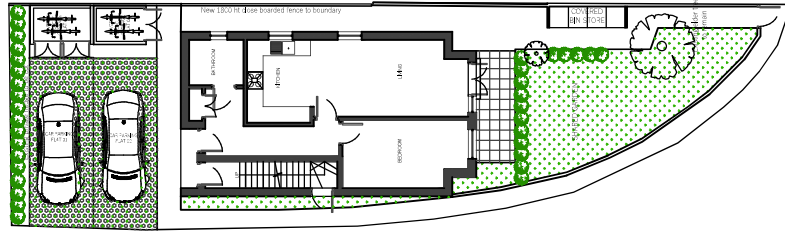
**Address** LAND AT LONGFORD CLOSE LONGFORD GARDEN HAYES  
**Development:** Erection of a two storey dwelling comprising two 1-bedroom flats  
**LBH Ref Nos:** 77102/APP/2022/647



NOTES  
Dimensions are not to be taken from this drawing.



01 - OS MAP SCALE 1:1250



02 - PROPOSED BLOCK PLAN  
SCALE 1:200

NO	REVISIONS	DESCRIPTION	DATE	BY

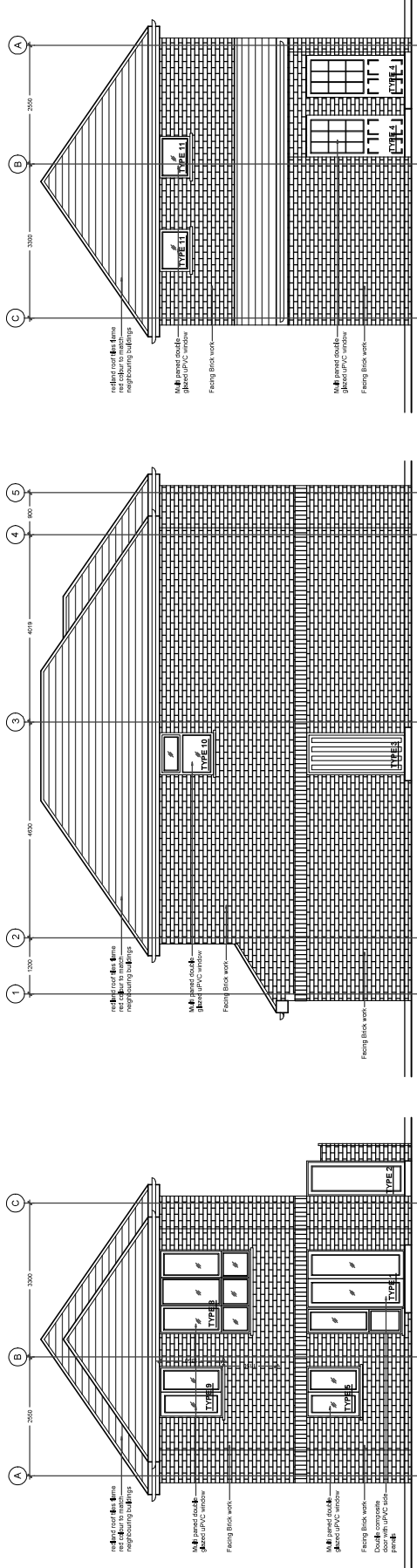
**DESIGN ENDEAVOURS LTD**  
M: 0786560774, 01984502259  
E: enquiry@designendeavors.com

**PROJECT:** LAND AT LONGFORD CLOSE LONGFORD GARDENS, HAYES  
**TITLE:** OS MAP & BLOCK PLAN

Drawn	JTS	Checked	JTS	Issue	FEB 22
Job No:	DE-625	Drawing No:	FP-LC-01	Rev:	



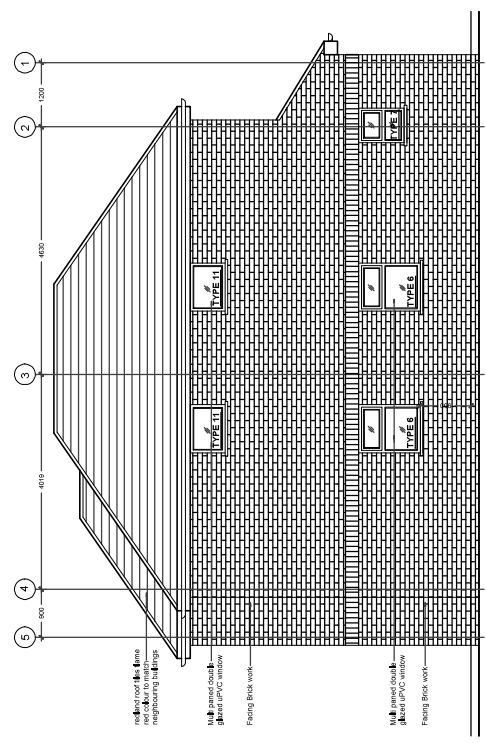
NOTES  
Dimensions are not to be read from this drawing.



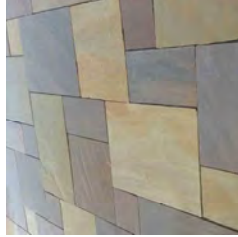
03 - REAR ELEVATION

02 - LEFT SIDE ELEVATION

01 - FRONT ELEVATION



04 - RIGHT SIDE ELEVATION



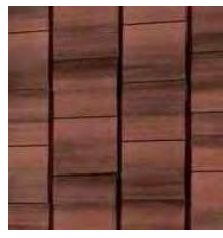
NATURAL SANDSTONE PAVING  
COLOR: BUFF BLEND  
FINISH: TEXTURED  
BRAND: -  
LOCATION: - HARD LANDSCAPE



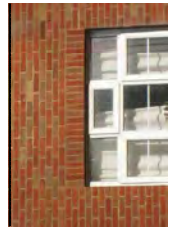
RED MULTIFACING BRICKWORK  
NAME: KASSANDRA BRICK  
FINISH: LIGHT TEXTURED  
MANUFACTURER: WIENERBERGER  
LOCATION: TO EXTERNAL WALLS WITH SOLDIERS AND UPVC WINDOW



WINDOWS  
COLOR: WHITE uPVC  
BRAND: ANGLIAN WINDOWS  
LOCATION: TO BUILDING



ROOF TILES  
COLOR: Flame red  
BRAND: REDLAND -duoplain,  
concrete roof tile  
LOCATION: TO ROOF



RED MULTIFACING BRICKWORK  
NAME: KASSANDRA BRICK  
FINISH: LIGHT TEXTURED  
MANUFACTURER: WIENERBERGER  
LOCATION: TO EXTERNAL WALLS



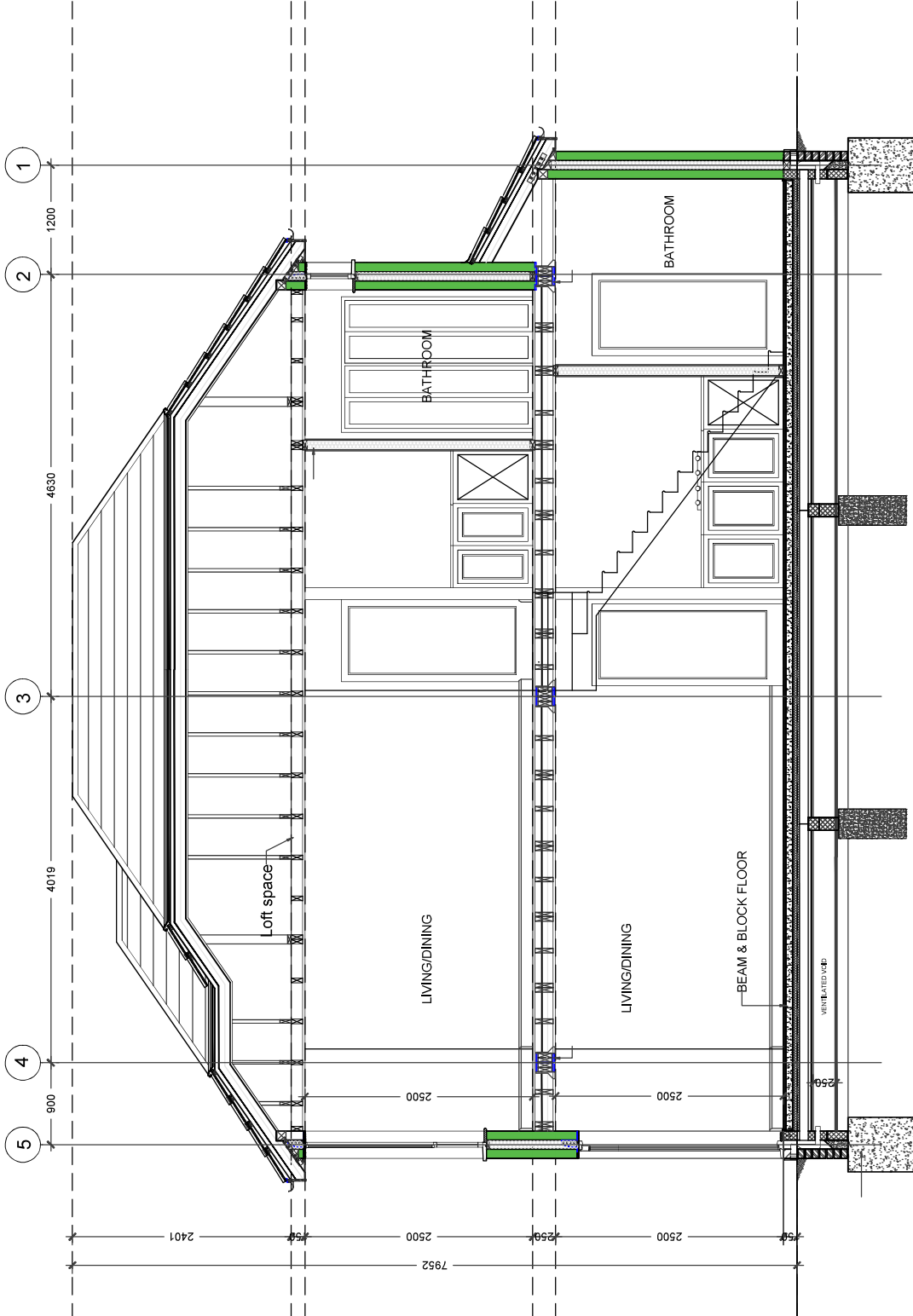
Permeable Paving  
Name: Bradstone Driveway Infilla Block Paving  
Location: CAR PARKING



NO.	DESCRIPTION	DATE	BY

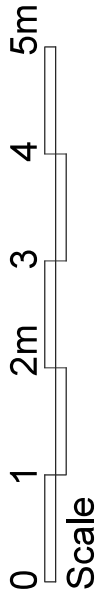
<b>DESIGN ENDEAVOURS LTD</b> M: 0786600774 0194502289 E: enquiry@designendeavoursltd.com	
Project	LAND AT LONGFORD CLOSE LONGFORD GARDENS, HAYES
TH	ELEVATIONS & SAMPLE BOARDS
Drawing No.	DE-625
Date	FEB 22
Revision No.	FP4LC03

NOTES  
Dimensions are not to be scaled from this drawing.



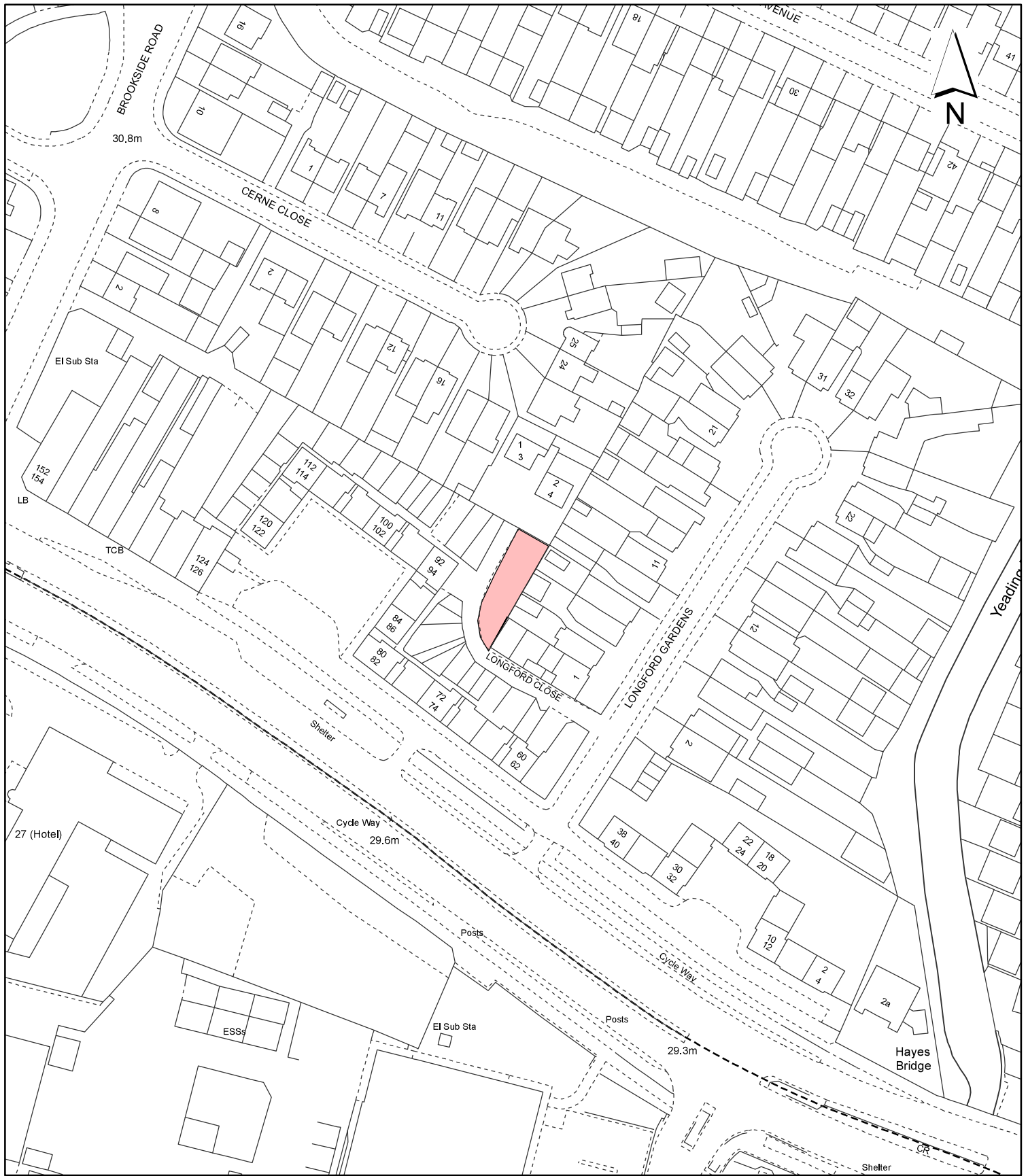
WALL LEGEND

01-SECTION AA



NO	DESCRIPTION	DATE	BY

<b>DESIGN ENDEAVOURS LTD</b> <small>M: 0788666074, 0788662259          E: enquiry@endeavoursgroup.com</small>			
<small>Project</small> LAND AT LANGFORD CLOSE LANGFORD GARDENS, HAYES		<small>Section</small> SECTION AA	
<small>Scale</small> 1:150@A3	<small>Drawn</small> FA	<small>Approved</small> Date SEP 21	<small>Rev</small> P-LG-04
<small>Drawing No</small> DE-925	<small>Drawing No</small>	<small>Section No</small>	<small>Rev</small>



**Notes:**

 Site boundary

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Site Address:

**Land at Longford Close  
 Longford Gardens**

Planning Application Ref:

**77102/APP/2022/647**

Planning Committee:

**BoroughPage 221**

Scale:

**1:1,250**

Date:

**December 2022**

**LONDON BOROUGH  
 OF HILLINGDON  
 Residents Services  
 Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111



**HILLINGDON**  
 LONDON

## **Report of the Interim Director of Planning, Regeneration & Public Realm**

**Address** 14 HIGH STREET HAREFIELD

**Development:** Application for planning permission for the permanent retention of the existing outdoor seating area at the rear of the coffee shop and replacing existing fence and floor, previously approved under planning application reference number 47647/APP/2020/2017.

**LBH Ref Nos:** 47647/APP/2022/2552





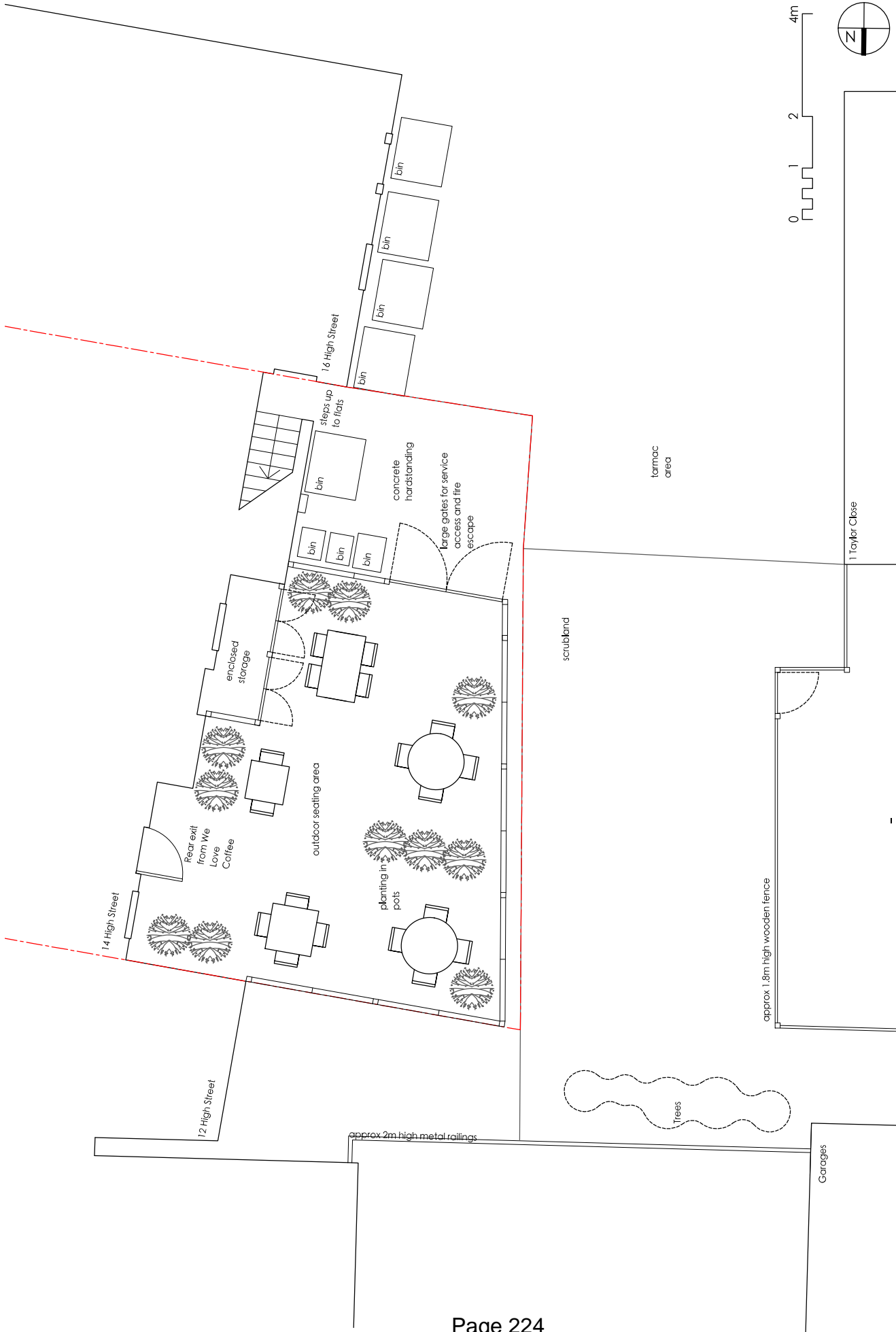
© Crown copyright and database rights [2020] Ordnance Survey 0100031673



drawing title	scale	1:1250 @ A4
Existing Site Location Plan	date	June 2020
drawing number	drawn by	AB
1570.02.03.Exg.001	checked by	AB

job title	We Love Coffee
client	Tushar Gupta

**MICHAEL JONES ARCHITECTS**  
 020 8948 1863 | 129 Kew Road, Richmond, TW9 2PN  
 www.mjarchitects.co.uk | studio@mjarchitects.co.uk



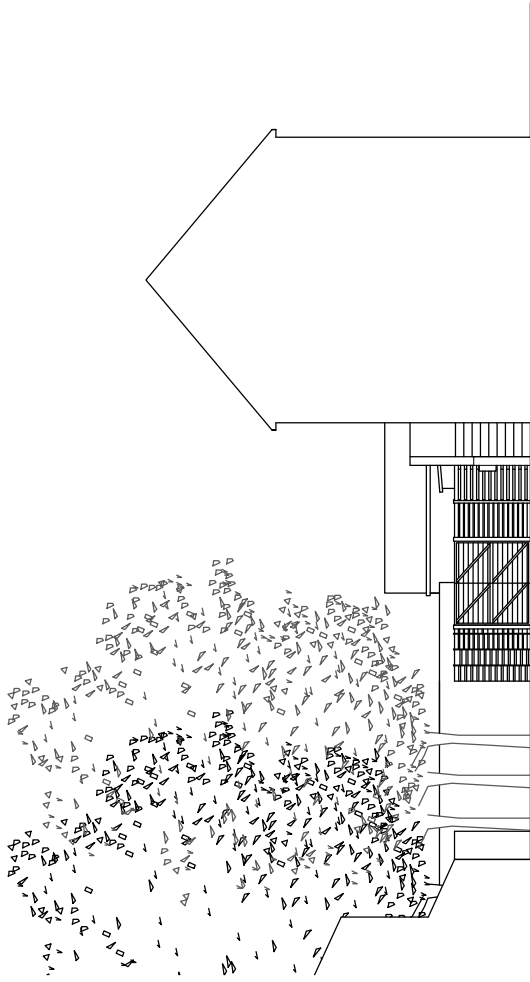
scale	1:100 @ A4
date	April 2022
drawn by	A8
checked by	A8

drawing title	Existing Site Plan
drawing number	1570.02.03.Exg02.002
rev	-
client	We Love Coffee
architect	Tushar Gupta

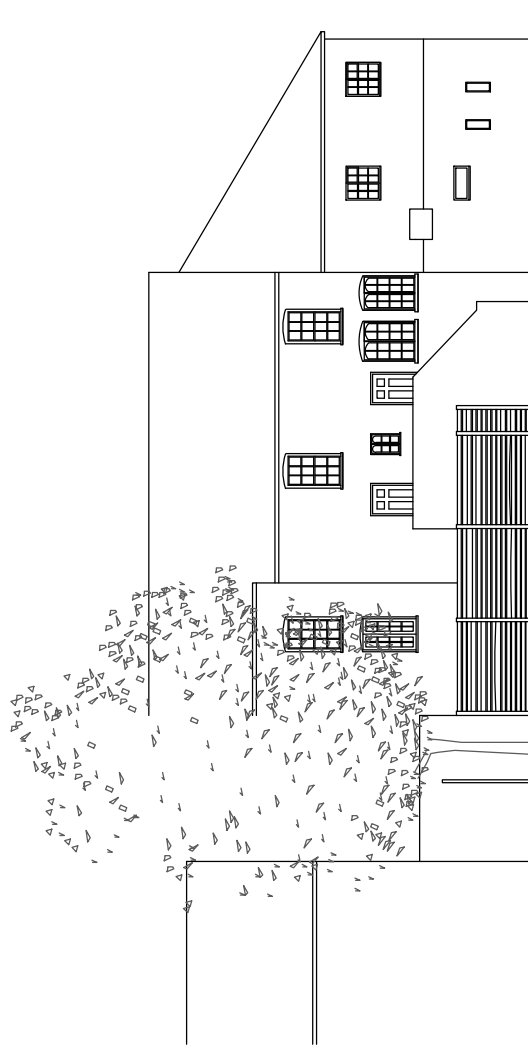
**MICHAEL JONES ARCHITECTS**  
 020 8948 1863 | 129 Kew Road, Richmond, TW9 2PN  
 www.mjarchitects.co.uk | studio@mjarchitects.co.uk

Rev B: height of fence reduced to 2m	: 18.05.2020
Rev A: correction of made referencing height of fence	: 12.08.2020
revisors	

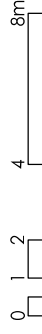




Existing Section/Elevation Facing North  
1:200



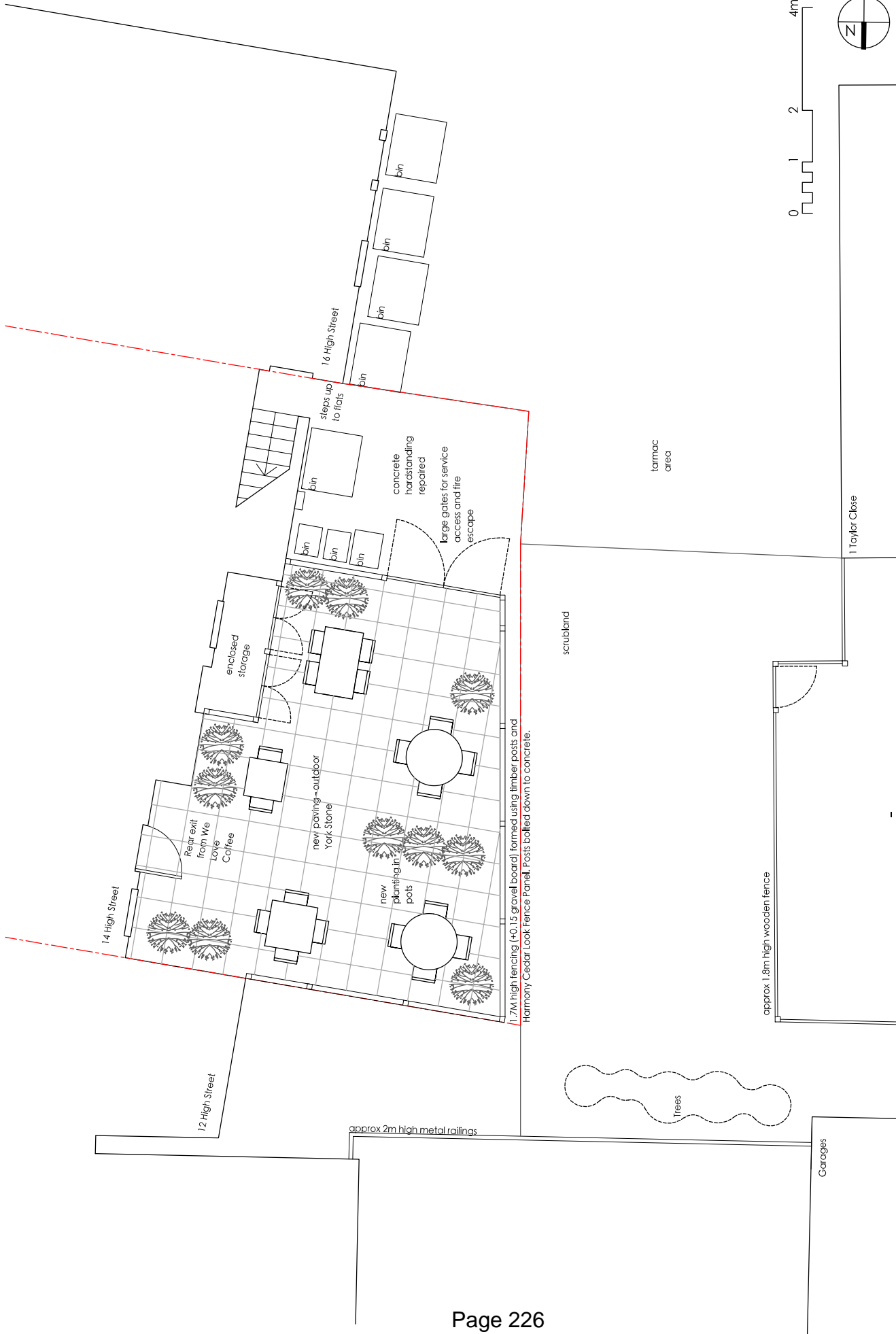
Existing Rear (West) Elevation  
1:200



drawing title	scale	1:200 @ a4
Existing Elevation/Section	date	April 2022
drawing number	rev	-
1570.02.03.Exg02.060	drawn by	A8
	checked by	A8

job title  
We Love Coffee  
client  
Tushar Gupta

**MICHAEL JONES ARCHITECTS**  
020 8948 1863 | 129 Kew Road, Richmond, TW9 2PN  
www.mjarchitects.co.uk | studio@mjarchitects.co.uk



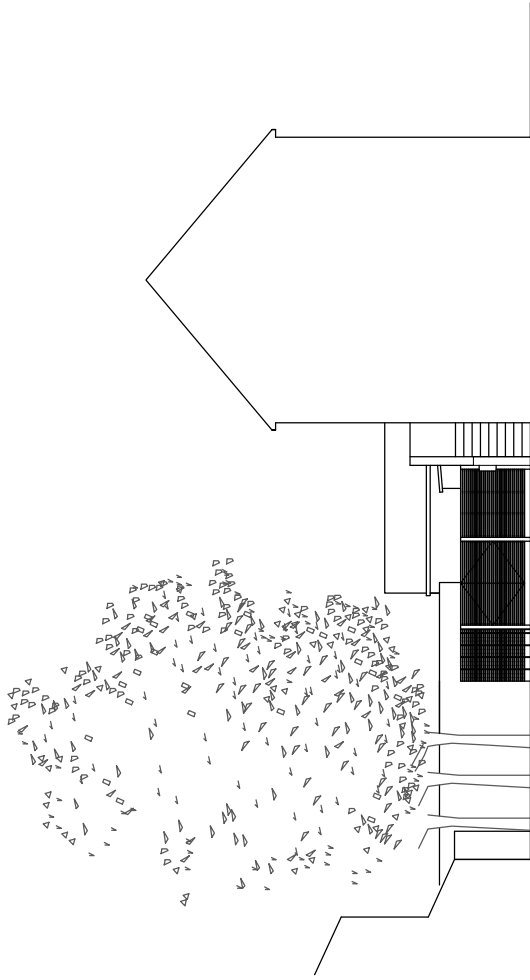
1.7m high fencing (+0.15 gravel board) formed using timber posts and Harmony Cedar Look Fence Panel. Posts bolted down to concrete.

scale	1:100 @ a4
date	June 2020
drawn by	A8
checked by	A8
job title	We Love Coffee
client	Tushar Gupta
drawing title	Proposed Site Plan
drawing number	1570.04.03.Prh02.002
rev	A

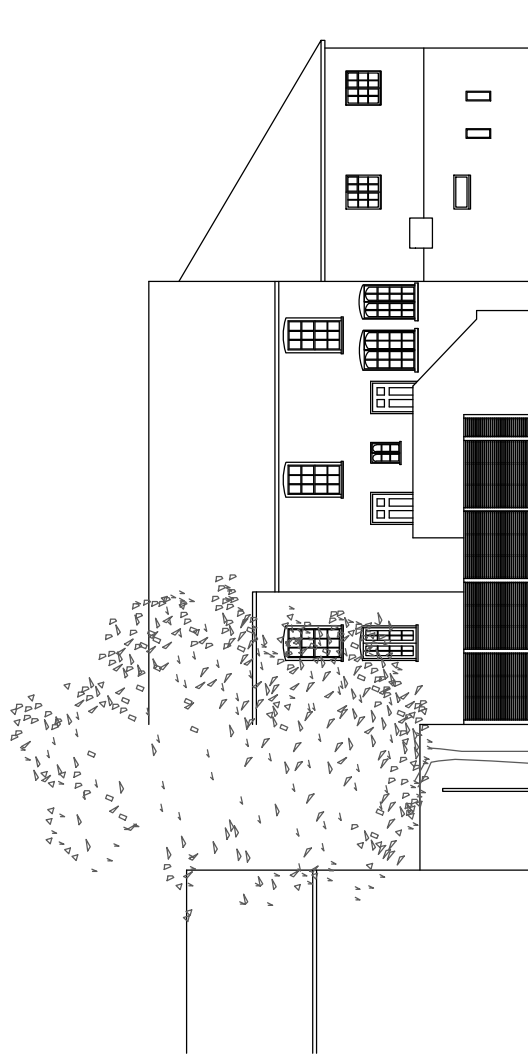
**MICHAEL JONES ARCHITECTS**  
 020 8948 1863 | 129 Kew Road, Richmond, TW9 2PN  
 www.mjarchitects.co.uk | studio@mjarchitects.co.uk

17/10/2020 + AA

Proposed paving changed  
 renders



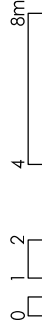
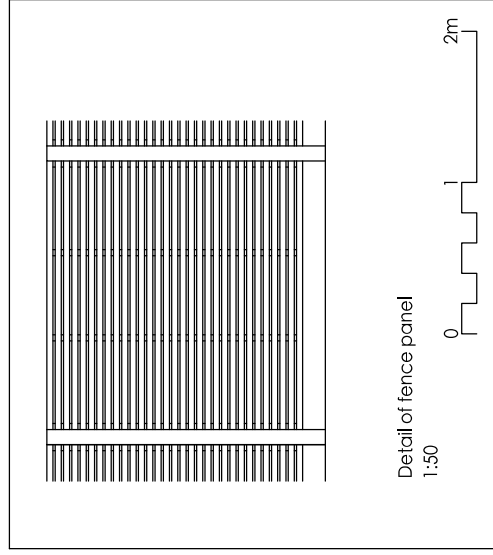
Proposed Section/Elevation Facing North  
1:200

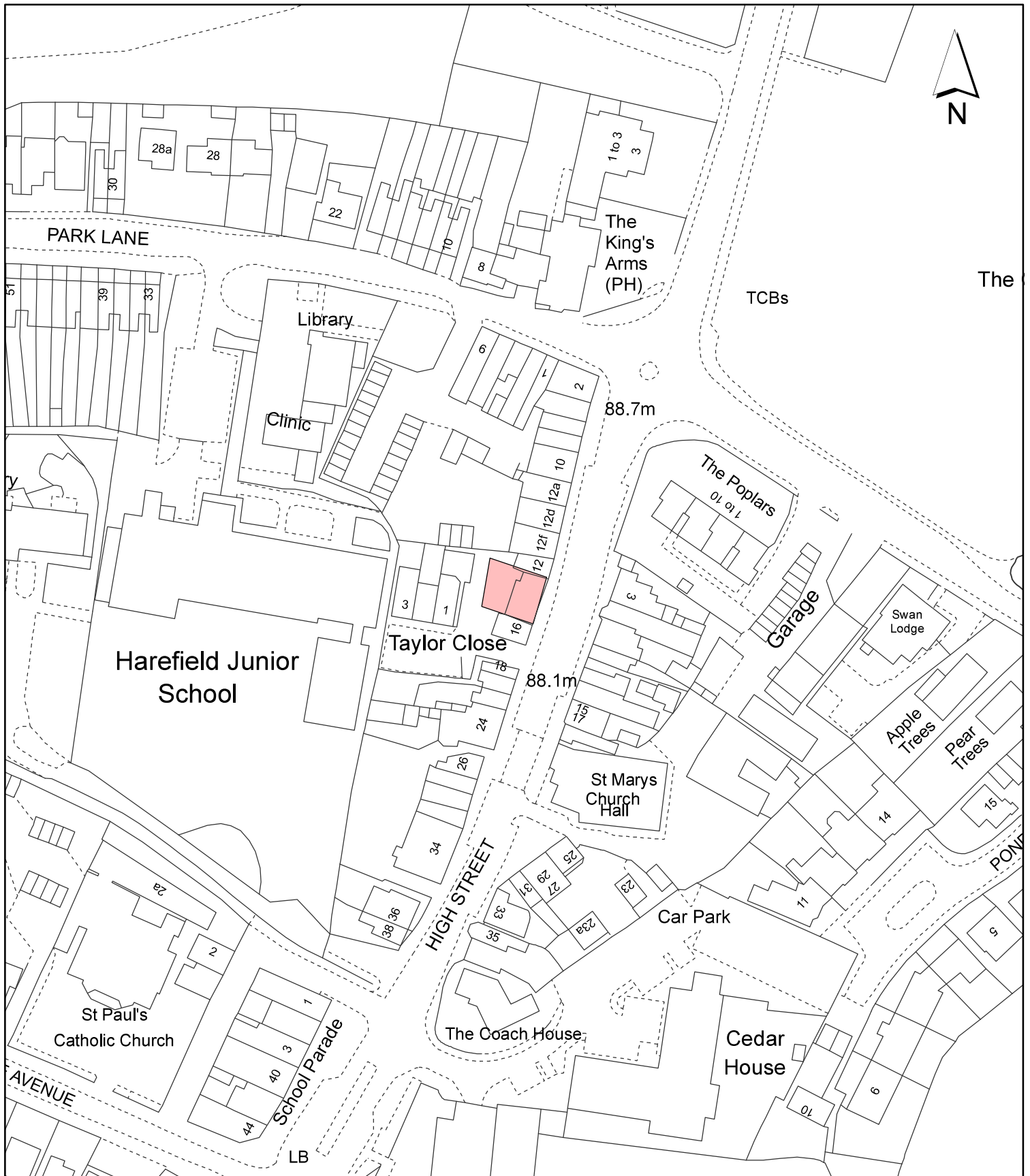


Proposed Rear (West) Elevation  
1:200



Harmony Cedar Look Fence Panel





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Site Address:

**14 High Street**

**LONDON BOROUGH OF HILLINGDON**  
**Residents Services Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:  
**47647/APP/2022/2552**

Scale:  
**1:1,250**

Planning Committee:  
**BoroughPage 228**

Date:  
**December 2022**



**Report of the Interim Director of Planning, Regeneration & Public Realm**

**Address**                    **AXIS HOUSE 242 BATH ROAD SIPSON**

**Development:**        Change of use of parking spaces from residential use to a paying public car park.

**LBH Ref Nos:**        43794/APP/2022/2500

Written dimensions to be taken in preference to scaled dimensions. The Contractor is responsible for checking all dimensions before work starts. All work is to be carried out to the requirements, and to the satisfaction of the Local Authority. These drawings are for planning purposes only. Any discrepancies to be brought to the attention of 4D Planning Consultants immediately.

Description

Rev	Date	Description	By	chk

**4D PLANNING™**  
Chartered Town Planners & Architectural Technicians

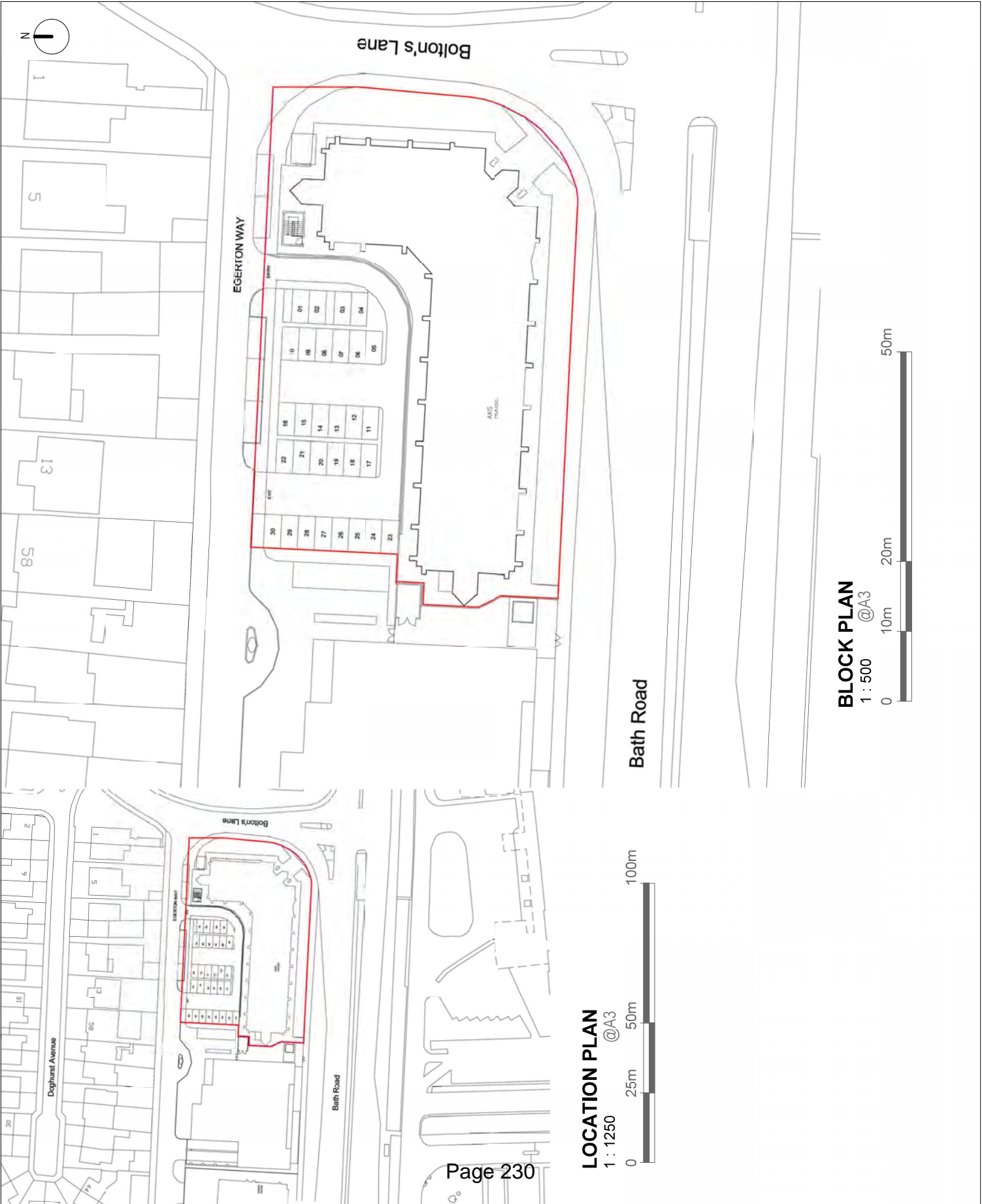
Architecture | Town Planning  
T: 0203 1500 183 www.GetMePlanning.com  
www.GetMePlanning.com

Client  
Mr Adam Naeem

Project  
AXIS HOUSE 242 BATH ROAD SIPSON, UB3 5AY

Title  
EXISTING BLOCK AND LOCATION PLANS

Date: 09.09.2022 Rev:  
Scale: AS INDICATED  
Drawing No: 4D-304 E-00  
Drawn by: IMAX





Written dimensions to be taken in preference to scaled dimensions. The Contractor is responsible for checking all dimensions before work starts. All work is to be carried out to the requirements, and to the satisfaction of the Local Authority. These drawings are for planning purposes only. Any discrepancies to be brought to the attention of 4D Planning Consultants immediately.

Description

Rev	Date	Description	By	chk

**4D PLANNING™**  
Chartered Town Planners & Architectural Technicians

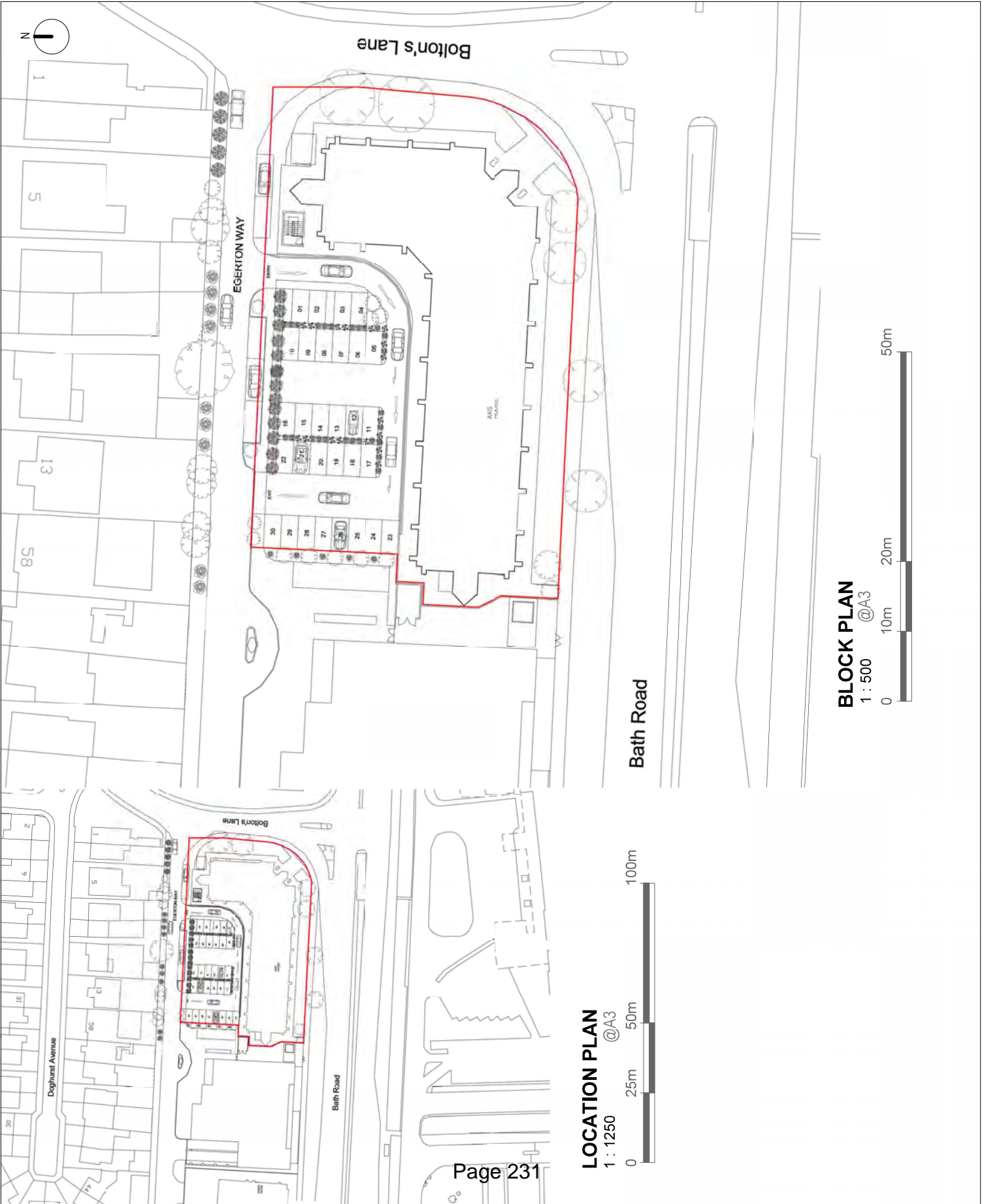
Architecture | Town Planning  
T: 0203 1500 183 www.GetMePlanning.com  
www.GetMePlanning.com

Client  
Mr Adam Naeem

Project  
AXIS HOUSE 242 BATH ROAD SIFSON, UB3 5AY

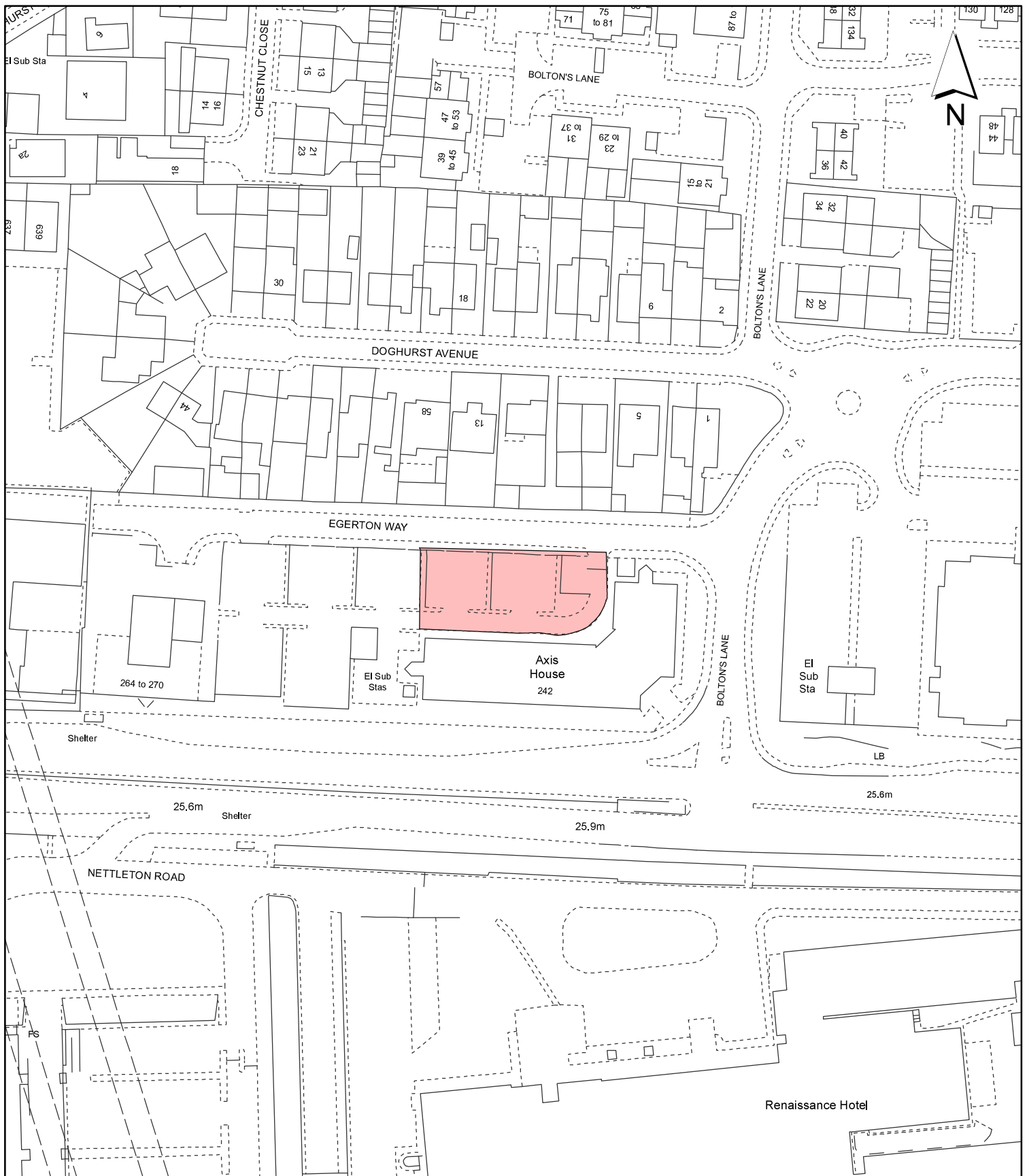
Title  
PROPOSED BLOCK AND LOCATION PLANS

Date: 09.09.2022 Rev:  
Scale: AS INDICATED  
Drawing No. 4D-304 P 00  
Drawn by: IMAX









**Notes:**

 Site boundary

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Site Address:

**Axis House  
242 Bath Road**

**LONDON BOROUGH  
OF HILLINGDON**  
Residents Services  
Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
Telephone No.: Uxbridge 01895 250111

Planning Application Ref:  
**43794/APP/2022/2500**

Scale:  
**1:1,250**

Planning Committee:  
**BoroughPage 233**

Date:  
**December 2022**



## **Report of the Interim Director of Planning, Regeneration & Public Realm**

**Address** 6 GLEBE AVENUE ICKENHAM

**Development:** Subdivision of premises into two to provide one barber shop (Class E) and one beauty salon (Sui Generis), and alterations to shopfronts including the installation of a canopy (retrospective application)

**LBH Ref Nos:** 2015/APP/2022/1894

6, Glebe Avenue, Ickenham, Hillingdon, UB10 8PB



Site Plan shows area bounded by: 507893.36, 185966.34 508034.78, 186107.76 (at a scale of 1:1250), OSGridRef: TQ 7968603. The representation of a road, track or path is no evidence of a right of way. The representation of features as lines is no evidence of a property boundary.

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**Notes**

- 1 All concrete to be 1:2:4 mix by volume
- 2 All dimensions are in millimeters
- 3 All materials used to be half hour fire resistance and used to manufacturers instructions
- 4 All new gutters to be roddable and back inlet type.
- 5 New walls bonded to existing using 'Furrifix' or similar profiles.
- 6 The contractor to check all dimensions before commencement of works and inform the Client of any discrepancies.
- 7 All works to be carried out in accordance with British Standards and British Standards, all in approval of the L.A. engineer.
- 8 All new glazing below 1000 from floor level to be toughened safety glass to BS6206.
- 9 All structural timber to be tannalised VERMIN
- 10 Any proposed works likely to be affected by landfill gas to have 0.25 ZEDCOR polymer thermoplastic with ZEDCOR DPM jointing system across the cavity at DPC level with cavity trays over, the floor slab to be vented using kerbside land drains out to street.
- 11 All dimensions to be double checked on site
- 12 All steels to be measure on site with built dimensions
- 13 Steels to have 30 min fire protection
- 14 All drawings to be approved prior to Build works, any works carried out without approval is at own risk.
- 15 Any discrepancies to be discussed with our team prior to works, any changes made on site to be submitted to and approved by us in writing

Scale 1/100



**Title / Description :**

Existing Plans

**Project Address :**

6 Glebe Avenue  
Leekham, Uxbridge UB10 8PB

**Scale of Drawing**

1/100 @ A1

**Drawing No**

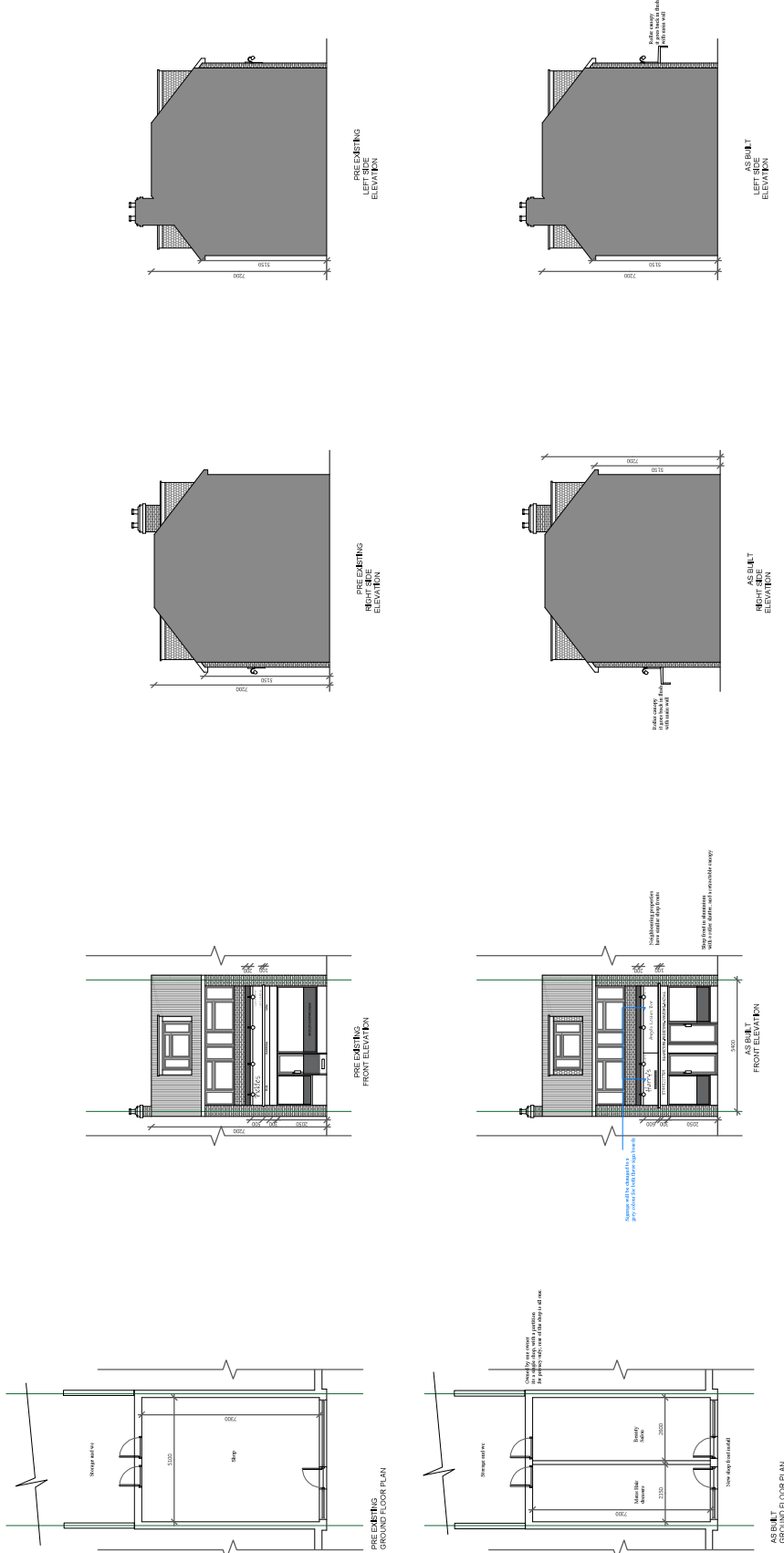
06 001 - rev. 02

**Drawn By**

Sunny Bahia

**Date of Proj**

Nov. 22





**Notes:**

 Site boundary

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Site Address:

**6 Glebe Avenue**

**LONDON BOROUGH OF HILLINGDON**  
**Residents Services Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW  
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:  
**2015/APP/2022/1894**

Scale:  
**1:1,250**

Planning Committee:  
**BoroughPage 237**

Date:  
**December 2022**

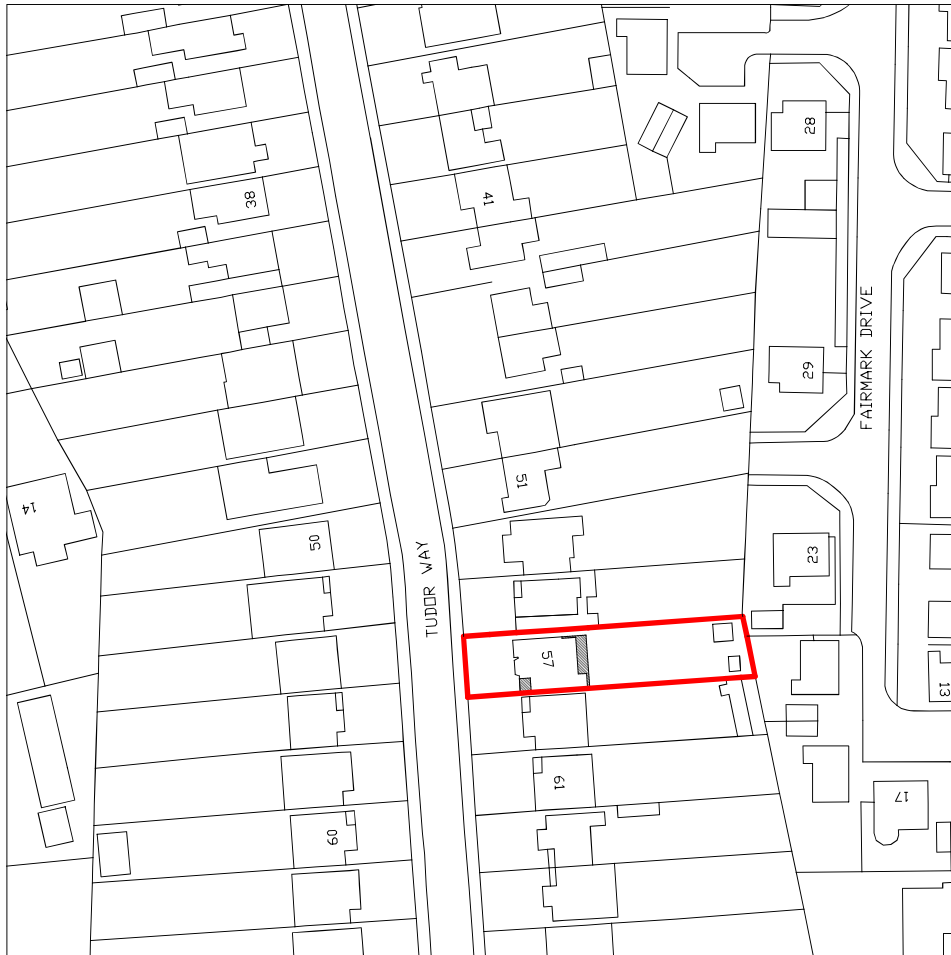


**Report of the Interim Director of Planning, Regeneration & Public Realm**

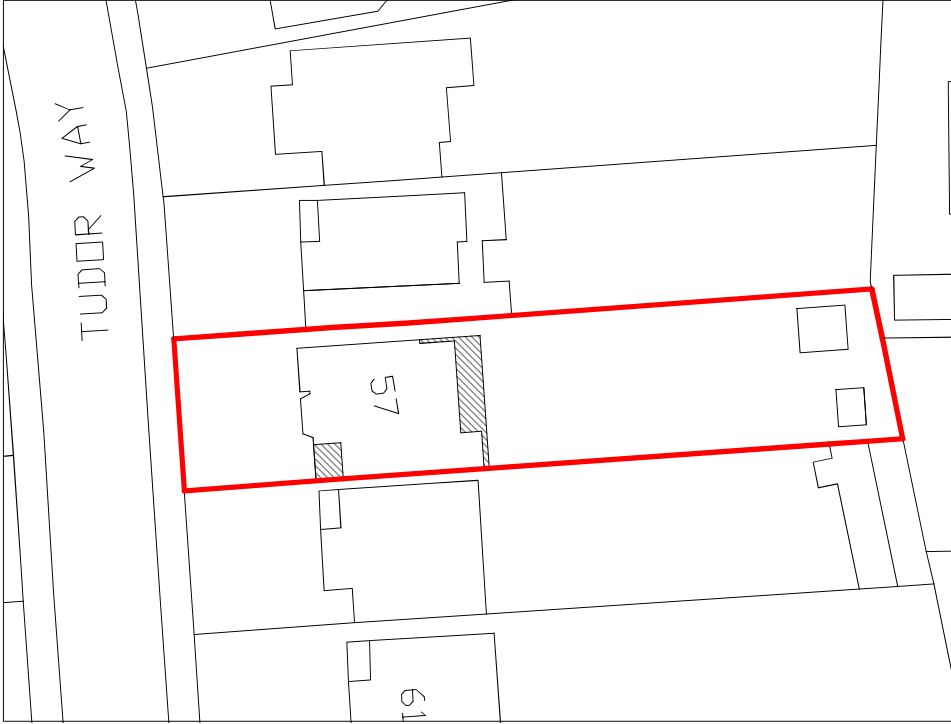
**Address** 57 TUDOR WAY HILLINGDON

**Development:** Erection of a single storey side extension and a part single, part double storey rear extension.

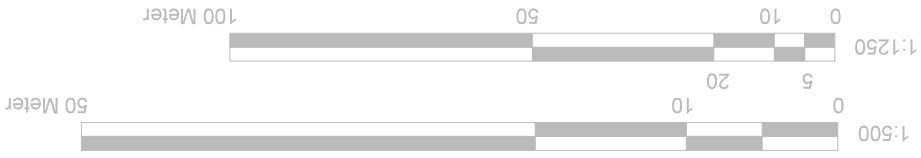
**LBH Ref Nos:** 20951/APP/2022/1228



**Location Plan**  
Scale 1:1250



**Block Plan**  
Scale 1:500



GENERAL NOTES:  
 1. ALL DIMENSIONS ARE IN MILLIMETER.  
 2. VERIFY ALL DIMENSIONS AND CONDITIONS BEFORE BUILDING OR STARTING CONSTRUCTION. NOTIFY THE DESIGNER IMMEDIATELY OF ANY DISCREPANCY OR VARIATION.  
 3. ALL WORK TO COMPLY WITH CURRENT BUILDING REGULATIONS AND CODES OF PRACTICE

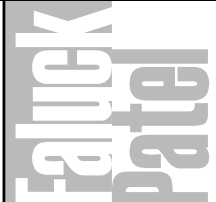
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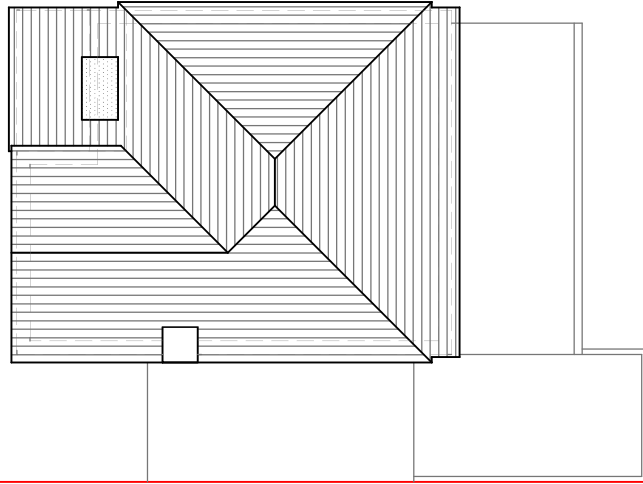


Site Address  
 57 Tudor Way  
 Uxbridge  
 UB10 9AA

Scale: 1:1250/500@A4  
 Date: 24/01/2022  
 Drawing No.: 2022/07-07  
 Drawn By: FP

Revision Date:  
 e.mail - faluckpate@yahoo.com  
 (M) +44 (0) 7871 466 254

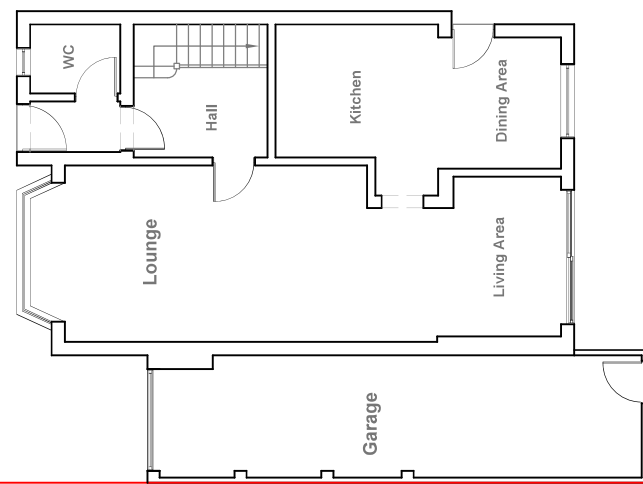




**Existing Roof Plan**  
Scale 1:100

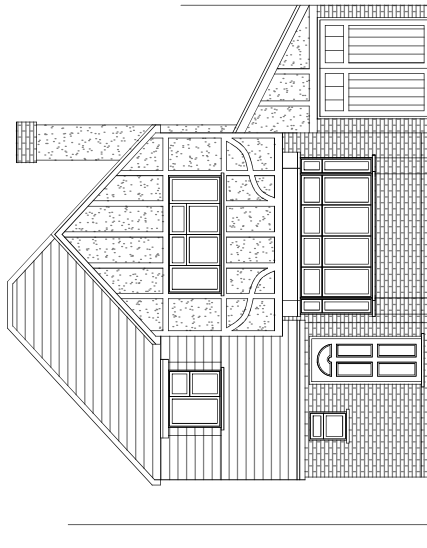


**Existing First Floor Plan**  
Scale 1:100

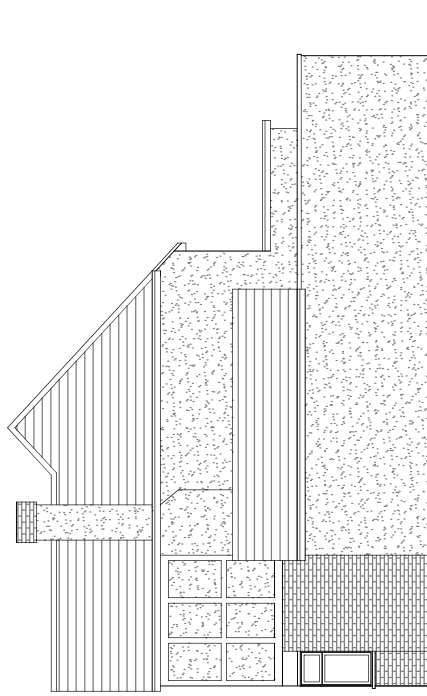


**Existing Ground Floor Plan**  
Scale 1:100

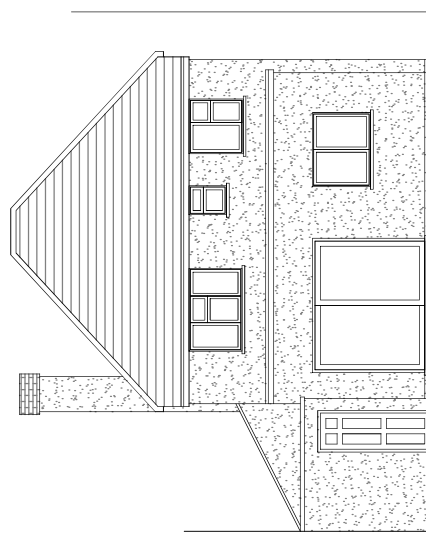




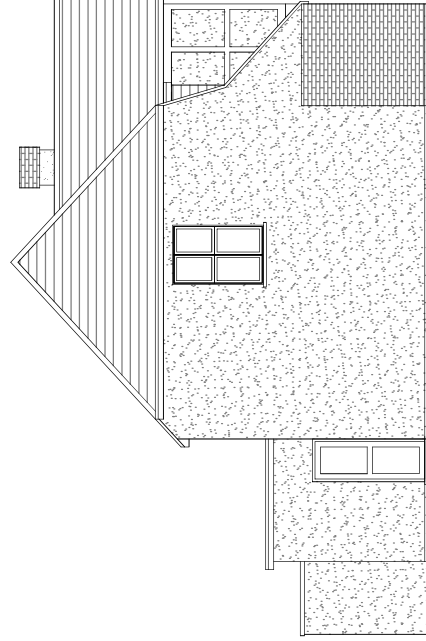
**Existing Front Elevation**  
Scale 1:100



**Existing Side Elevation**  
Scale 1:100



**Existing Rear Elevation**  
Scale 1:100



**Existing Side Elevation**  
Scale 1:100



Title:

# Existing Elevations

- GENERAL NOTES:
1. ALL DIMENSIONS ARE IN MILLIMETER.
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  3. ALL WORK TO COMPLY WITH CURRENT BUILDING REGULATIONS AND CODES OF PRACTICE

Site Address  
57 Tudor Way  
Uxbridge  
UB10 9AA

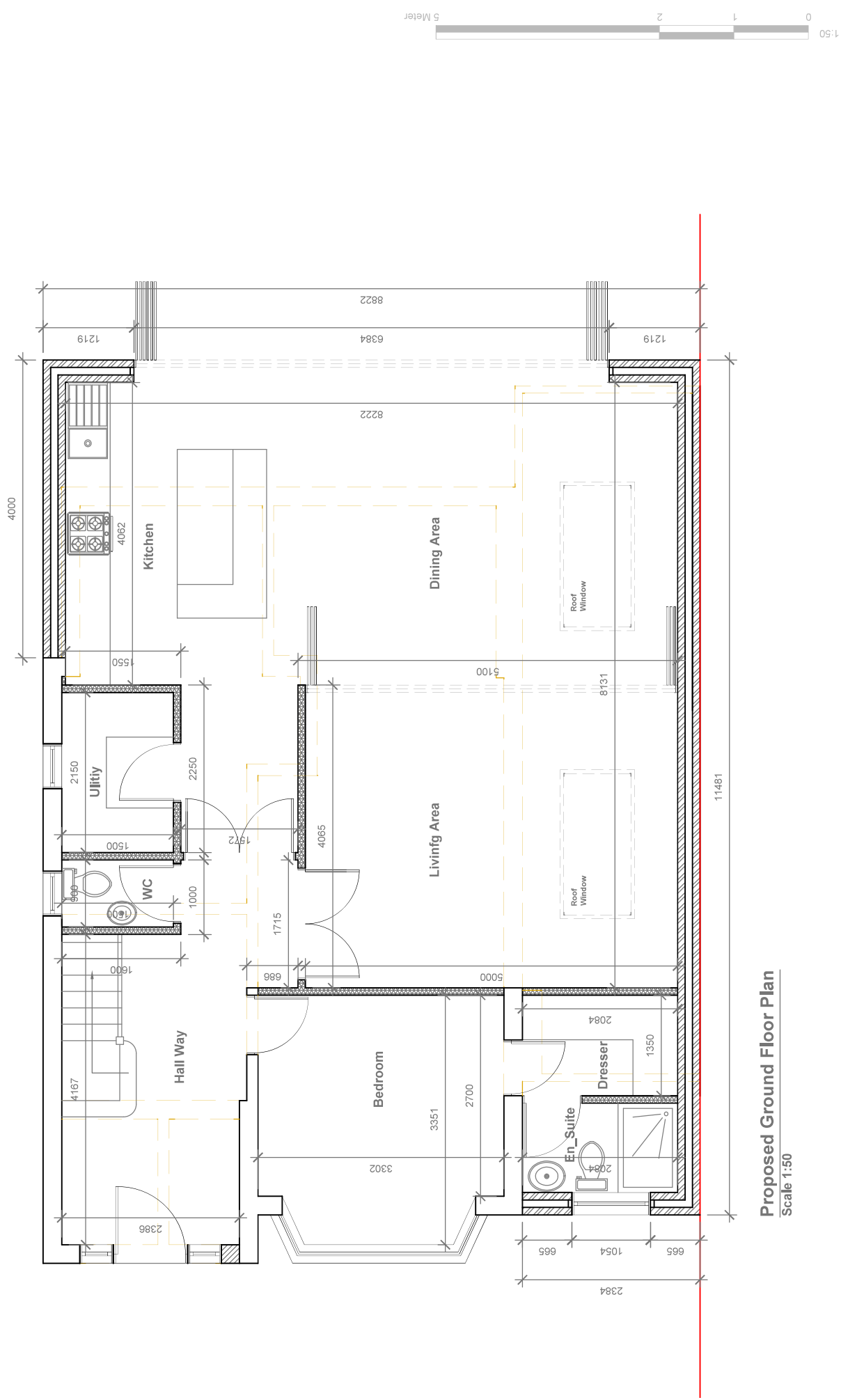
Scale: 1:100 @A3  
Date: 24/07/2022  
Drawing No.: 2022/07-02  
Drawn By: FP

Revision Date:

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falucpatal@yahoo.com  
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# Proposed Ground Floor Plan

**Proposed Ground Floor Plan**  
Scale 1:50

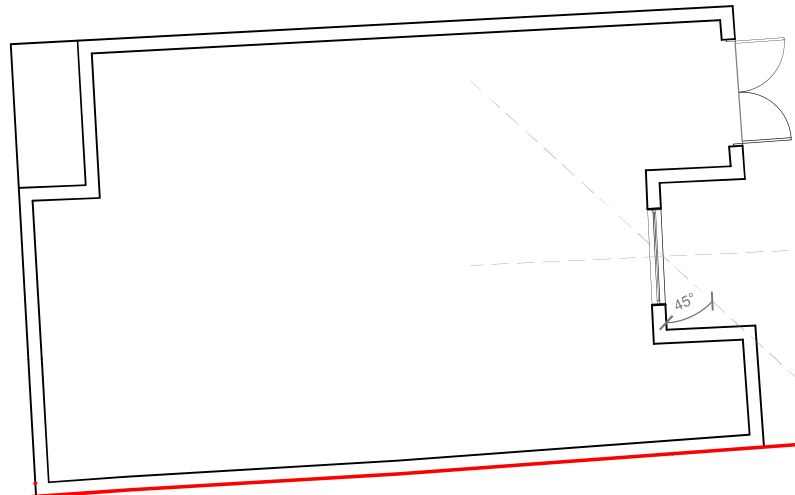


Proposed External Finish  
Materials to Match Existing  
External Finish Materials

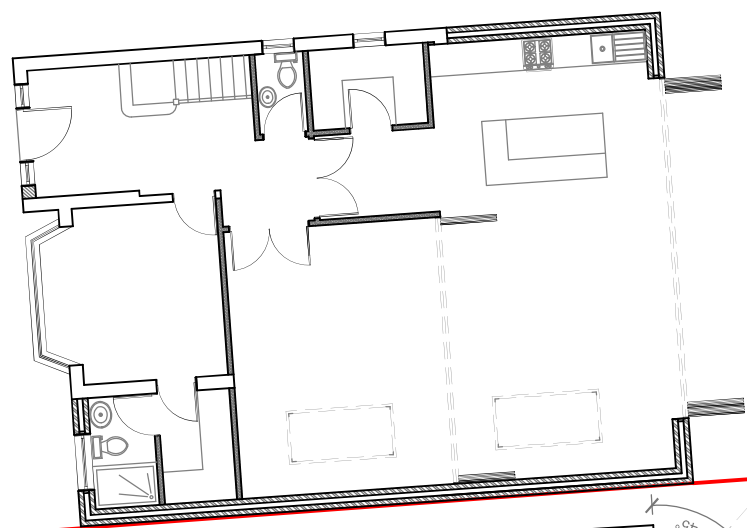
Proposed Flank Wall Window to be  
Obscure Glazed and Non Opening  
below 1.7m from FFL



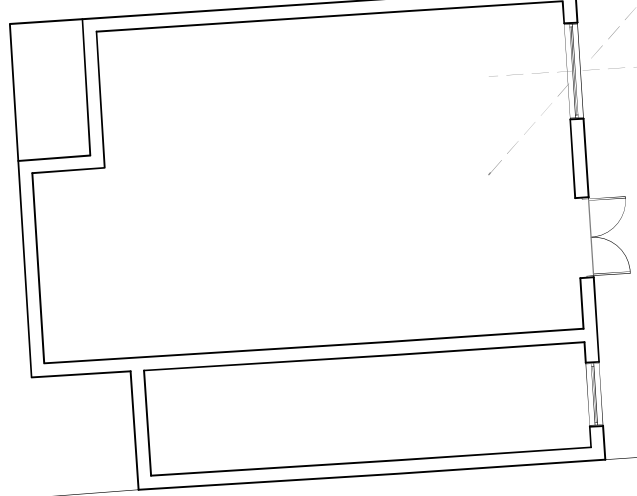
SS



LS



SS



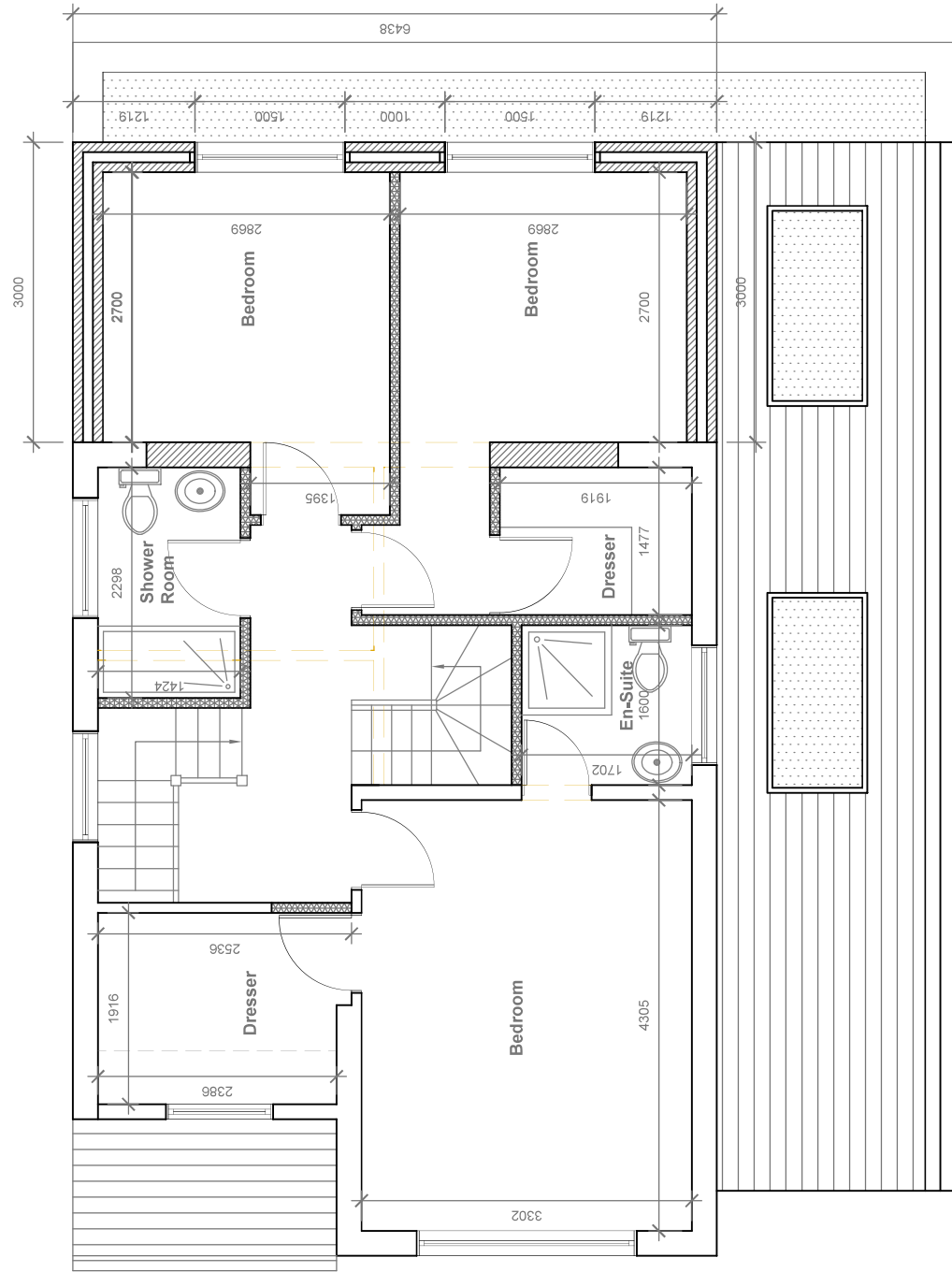
Revision Date:	D	16-06-2022
	E	24-06-2022
	F	31-10-2022
e-mail:	faulkpatel@yahoo.com	
(M) +44 (0) 7871 466 254		

Scale:	1:100 @A3
Date:	06/06/2022
Drawing No.:	2022/07-08
Drawn By:	FP

Site Address  
57 Tudor Way  
Uxbridge  
UB10 9AA

# Proposed Ground Floor Plan

GENERAL NOTES:  
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**Proposed First Floor Plan**  
 Scale 1:50

Proposed External Finish  
Materials to Match Existing  
External Finish Materials

Proposed Flank Wall Window to be  
Obscure Glazed and Non Opening  
below 1.7m from FFL



Revision Date:	D	16-06-2022
	E	24-08-2022
	F	31-10-2022
e-mail:	faulcpatal@yahoo.com	
(M) +44 (0) 7871 466 254		

Scale:	1:100 @A3
Date:	06/06/2022
Drawing No.:	2022/07-09
Drawn By:	FP

Site Address  
57 Tudor Way  
Uxbridge  
UB10 9AA

# Proposed First Floor Plan

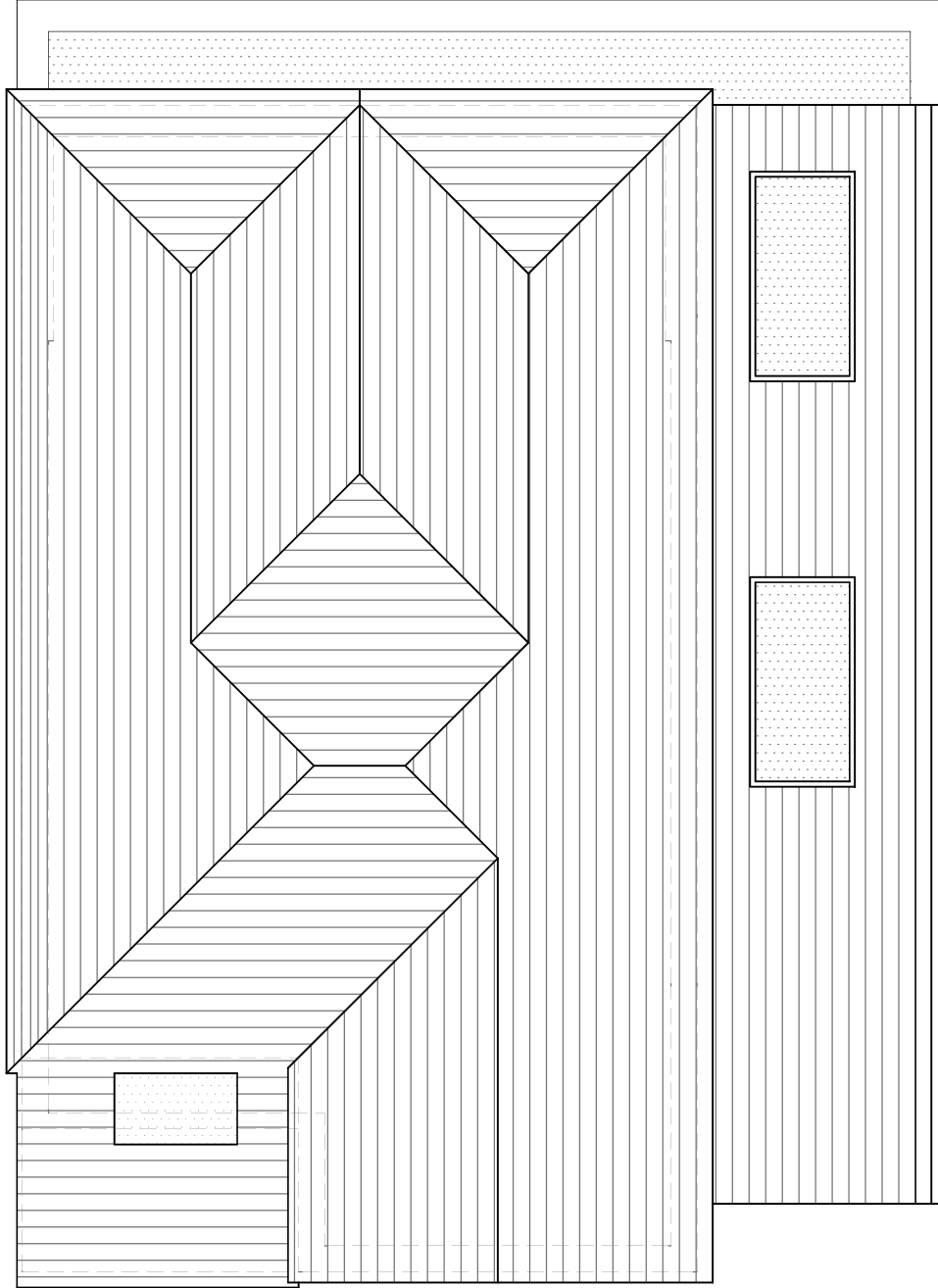
GENERAL NOTES:  
1. ALL DIMENSIONS ARE IN MILLIMETER.  
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SS

LS

6S



**Proposed Roof Plan**  
Scale 1:50

GENERAL NOTES:  
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Title:

# Proposed Roof Plan

Site Address  
 57 Tudor Way  
 Uxbridge  
 UB10 9AA

Scale: 1:50 @A3  
 Date: 24/07/2022  
 Drawing No.: 2022/07-05  
 Drawn By: FP

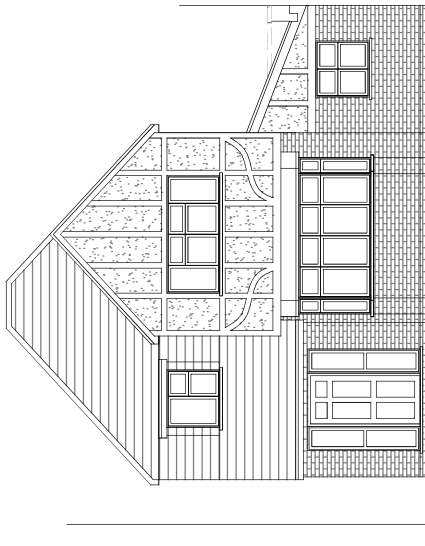
Revision Date:	
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E	24-08-2022
F	31-10-2022

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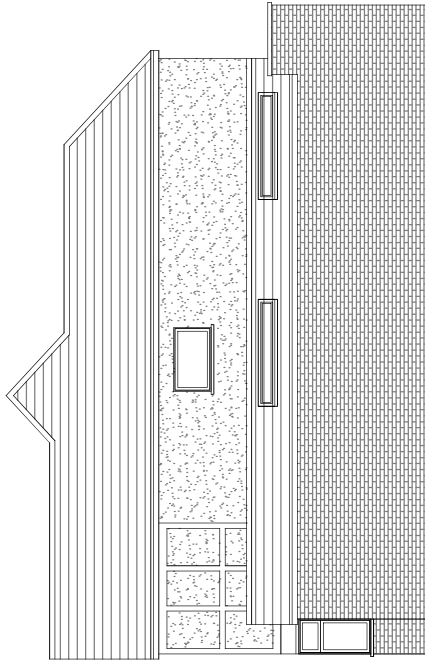


Proposed External Finish  
Materials to Match Existing  
External Finish Materials

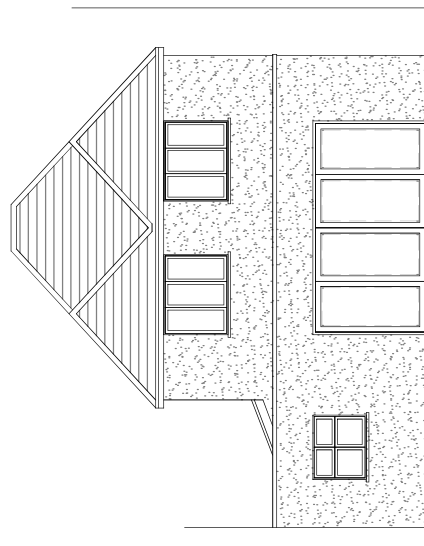
Proposed Flank Wall Window to be  
Obscure Glazed and Non Opening  
below 1.7m from FFL



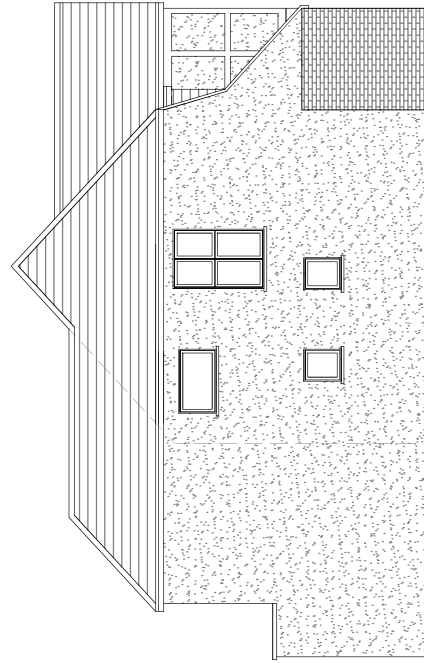
**Proposed Front Elevation**  
Scale 1:100



**Proposed Side Elevation**  
Scale 1:100



**Proposed Rear Elevation**  
Scale 1:100



**Proposed Side Elevation**  
Scale 1:100



Title:

GENERAL NOTES:  
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# Proposed Elevations

Site Address  
57 Tudor Way  
Uxbridge  
UB10 9AA

Scale: 1:100 @A3  
Date: 24/07/2022  
Drawing No.: 2022/07-06  
Drawn By: FP

Revision Date:  
D 16-06-2022  
E 24-08-2022  
F 31-10-2022  
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**Faluc Patel**



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Site Address:

**57 Tudor Way**

**LONDON BOROUGH OF HILLINGDON**  
Residents Services  
Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW  
Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

**20951/APP/2022/1228**

Scale:

**1:1,250**

Planning Committee:

**BoroughPage 248**

Date:

**December 2022**



**HILLINGDON**  
LONDON